

BURLINGTON STREET RECONSTRUCTION PROJECT

CITY OF WATERTOWN ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

12/13/2024 BID DOCUMENTS



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CITY MANAGER:

CITY ENGINEER:

CITY ATTORNEY:

CITY PURCHASING MANAGER:

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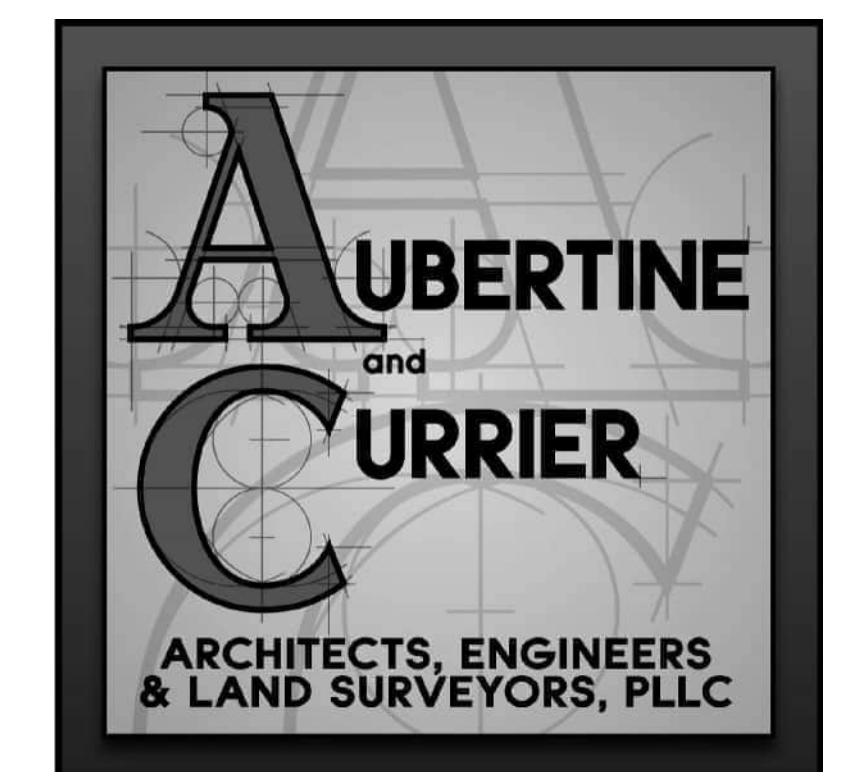
522 BRADLEY STREET

WATERTOWN, NEW YORK 13601

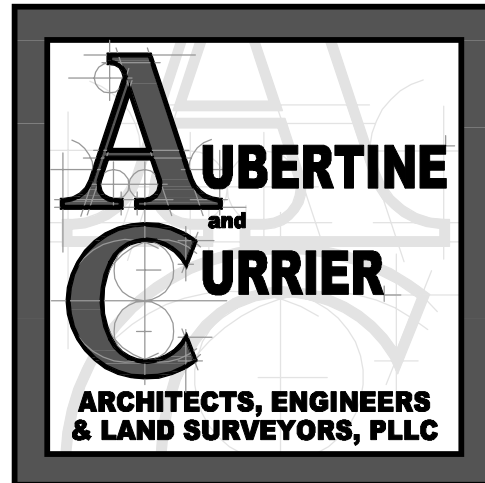
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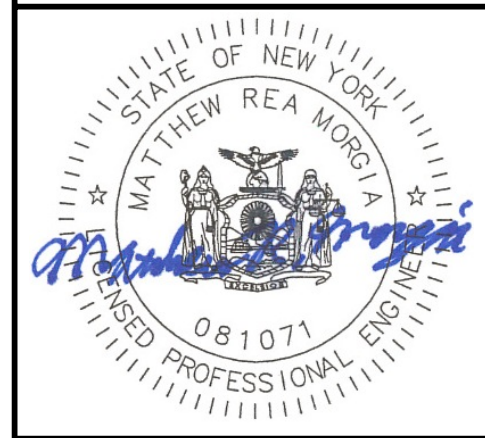


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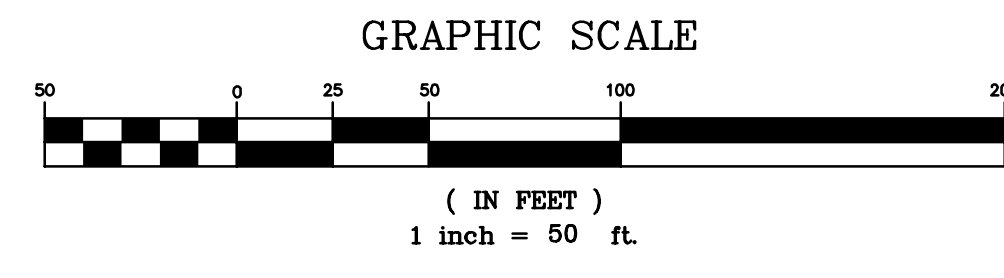
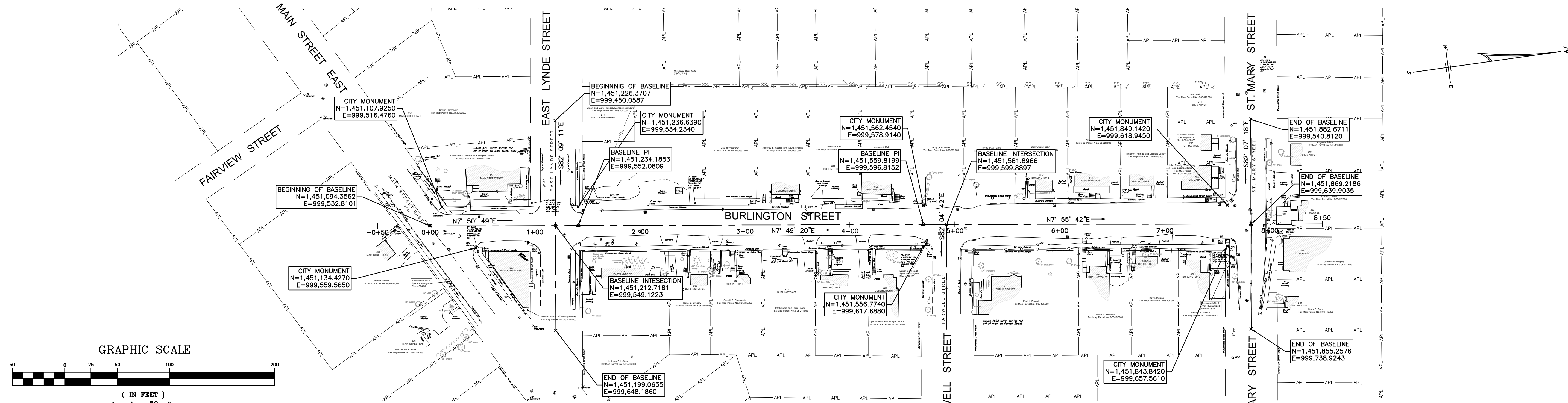
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BURLINGTON STREET RECONSTRUCTION PROJECT CITY OF WATERTOWN ENGINEERING DEPARTMENT JEFFERSON COUNTY, STATE OF NEW YORK



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GENERAL NOTES:

- UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN. PRIOR TO CONSTRUCTION CONTACT UNDERGROUND UTILITIES CALL CENTER OF NEW YORK FOR EXACT LOCATION OF ALL UNDERGROUND UTILITIES. (1-800-962-7962). CONTRACTOR IS RESPONSIBLE FOR LOCATING AND WORKING WITH THE APPROPRIATE UTILITY COMPANIES PRIOR TO CONSTRUCTION.
- THE ON-SITE TOPOGRAPHIC, UTILITY, AND PLANNING SURVEY FOR THE PROJECT AREA WAS CONDUCTED BY AUBERTINE AND CURRIER, PLLC, IN APRIL AND MAY OF 2023. UTILITY LOCATIONS WERE PLOTTED FROM DRAWINGS OF MULTIPLE PROJECTS THAT ARE ON FILE IN THE CITY ENGINEERING DEPARTMENT. VERTICAL DATUM IS BASED ON NAVD83 DATUM AND THE HORIZONTAL DATUM IS BASED ON NAD83 (2011).
- ALL OUT-OF-SCOPE AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS WILL BE RESTORED TO CONDITIONS EQUAL TO OR BETTER THAN THAT PRIOR TO CONSTRUCTION. OUTSIDE OF PROPERTY BOUNDARIES AND EASEMENT AREAS THE CONTRACTOR IS REMINDED THAT HE MUST OBTAIN WRITTEN AUTHORIZATION TO USE PRIVATE PROPERTY AND ASSUMES ALL LIABILITY HIMSELF.
- THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE CHARACTERISTICS AND EXTENT OF SUBSURFACE SOILS, ROCK, WATER TABLE LEVELS, ETC., PRIOR TO BIDDING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND BONDS NECESSARY TO OBTAIN SAID PERMITS WHERE APPLICABLE.
- SITE CONTRACTOR TO PROVIDE EROSION AND DUST CONTROL AS REQUIRED.
- A LICENSED LAND SURVEYOR SHALL BE RETAINED FOR ALL UTILITY AND FIELD STAKEOUT AT THE CONTRACTORS EXPENSE.
- PAVED AREAS WILL BE SAWCUT PRIOR TO EXCAVATION AND PAVING OPERATIONS. SAW CUT AREAS WILL BE TACK COATED PRIOR TO PAVING. TACK COAT SHALL MEET THE REQUIREMENTS OF ASPHALT OF ASPHALT EMULSION FOR TACK COAT, NYS DOT TABLE 702-9.
- CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES THROUGHOUT CONSTRUCTION UNTIL ESTABLISHMENT OF VEGETATIVE COVER. RUN-OFF CONTAINING SEDIMENTS FROM DISTURBED AREAS OF THE SITE SHALL NOT BE ALLOWED DIRECTLY INTO NATURAL STREAM CHANNELS.
- ALL TREES AND WETLANDS TO REMAIN SHALL BE PROTECTED BY THE CONTRACTOR. CONSTRUCTION ACTIVITIES ADJACENT TO TREES SHALL BE CONDUCTED TO REDUCE THE IMPACT TO TREES TO THE MAXIMUM EXTENT PRACTICAL. ANY DAMAGE TO EXISTING TREES SHALL BE REPAIRED OR THE TREE REPLACED, AS DIRECTED BY THE OWNER AT THE CONTRACTORS EXPENSE.
- CONTRACTOR SHALL PERFORM ALL ROADWAY CONNECTION WORK IN ACCORDANCE WITH NYS DOT SPECIFICATIONS. ALL ROADWAY WORK SHALL BE IN ACCORDANCE WITH NYS DOT MAINTENANCE AND PROTECTION OF TRAFFIC REGULATIONS, INCLUDING FLAGMEN, BARRICADES, WARNING SIGNSLIGHTS, ETC., WHERE WARRANTED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL AND PROPER DISPOSAL, AT A NYS DEC ACCEPTABLE LOCATION, OF ALL MATERIALS NOT REUSED AS TRENCH BACKFILL.
- EXCAVATIONS SHALL BE TO DEPTHS SHOWN ON DRAWINGS. ALL UNSTABLE OR UNSUITABLE MATERIAL SHALL BE EXCAVATED AND REMOVED TO SUCH DEPTH AS REQUIRED TO PROVIDE SUFFICIENT BEARING CAPACITY. OVEREXCAVATED AREAS SHALL BE BACKFILLED WITH SUITABLE MATERIAL.
- COMPACTION OF PIPE BEDDING AND BACKFILL MATERIAL SHALL BE BY MEANS OF HAND-GUIDED POWER DRIVEN OR DRUM-TYPE OR PLATE TAMPERS. BACKFILLING SHOULD PROCEED IN ACCORDANCE WITH LIFT THICKNESS AND COMPACTION REQUIREMENTS AS SHOWN ON THE DRAWINGS. UNLESS OTHERWISE NOTED ON THE DRAWINGS, COMPACTION REQUIREMENTS REFER TO PERCENT OF MAXIMUM DRY DENSITY AS DETERMINED IN ACCORDANCE WITH ASTM STANDARD D1557 METHOD "C". CARE SHOULD BE TAKEN TO SHAPE PIPE BEDDING TO FIT THE LOWER PART OF THE PIPE. BACKFILLING AND COMPACTION SHOULD PROGRESS EVENLY ALONG THE PIPE SIDEWALLS AND TO THE TOP OF PIPE BEDDING.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OF DIMENSIONS, ELEVATIONS AND LOCATIONS DURING PRECONSTRUCTION FIELD VERIFICATION. SUCH INFORMATION SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR VERIFICATION OR MODIFICATION OF THE PLANS.
- THE CONTRACTOR SHALL PROVIDE AS-BUILT RECORD DRAWINGS INCLUDING, AS A MINIMUM, THE FOLLOWING INFORMATION AS WELL AS ALL REQUIREMENTS OF THE SPECIFICATION:
 - RECORD OF ALL UTILITIES ENCOUNTERED IN TRENCH EXCAVATION. INFORMATION SHALL INCLUDE DIAMETER OF UTILITY, DEPTH OF BURIAL, ELEVATION AND LOCATION WITH REFERENCE TO NEAREST STRUCTURE SHOWN ON DRAWINGS. THIS INFORMATION SHALL BE KEPT CURRENT ON A WEEKLY BASIS. FAILURE TO DO SO MAY RESULT IN WITHHOLDING OF PAYMENTS.
 - DISTANCE TIES TO ALL MANHOLES, CLEANOUTS, BENDS AND CORPORATION STOPS.
 - UTILITY REPAIRS, SIDEWALK, AND DRIVEWAY REPLACEMENTS CENTERLINE.
 - STATIONS OF BENDS, CLEANOUTS, VALVES AND CORPORATION STOPS.
 - PERIODIC OFFSETS.
 - DENOTE BENCH MARK REFERENCE USED.
 - RECORD DETAILS NOT SHOWN ON THE ORIGINAL CONTRACT DOCUMENTS, ANY FIELD CHANGES OF DIMENSIONS AND DETAILS AND ANY CHANGES MADE BY CHANGE ORDER OR FIELD ORDER.
 - CERTIFICATE OF SUBSTANTIAL COMPLETION SHALL NOT BE ISSUED UNTIL AS-BUILT INFORMATION IS ACCEPTABLE.
 - PROVIDE TWO (2) SETS OF FINAL COMPLETE RECORD DRAWINGS. CONTRACTOR SHALL FURNISH AS-BUILT DATA ON PLAN SHEETS.
- ALL WATER MAIN AND SERVICE WORK MUST BE COORDINATED WITH THE CITY OF WATERTOWN WATER DEPARTMENT. WATER DEPARTMENT REQUIREMENTS SUPERCEDE ALL OTHER PLANS AND SPECIFICATIONS PROVIDED. ONLY THE CITY OF WATERTOWN WATER DEPARTMENT SHALL OPERATE WATER VALVES AND HYDRANTS. UNLESS PRIOR AUTHORIZATION IS RECEIVED FROM THE CITY OF WATERTOWN, THE CONTRACTOR SHALL COORDINATE ANY REQUIRED VALVE OPERATION 48 HOURS PRIOR TO NEEDED OPERATIONS.
- PRIOR TO ANY PLANNED WATER SHUTDOWN, THE CONTRACTOR SHALL COORDINATE WITH THE WATER DEPARTMENT. A MINIMUM OF ONE WEEK PRIOR, CONSUMERS SHALL BE NOTIFIED 24 HOURS PRIOR TO ANY PLANNED SHUTDOWN. SERVICES TO CONSUMERS SHALL BE MAINTAINED WITH AS LITTLE INTERRUPTIONS AS POSSIBLE. CONSUMER WATER SERVICE SHALL NOT BE INTERRUPTED LONGER THAN 4 HOURS, OR TEMPORARY WATER SERVICES MAY BE REQUIRED TO BE PROVIDED BY THE CONTRACTOR, AS ORDERED BY THE CITY ENGINEERING.
- ALL SANITARY SEWER MAIN AND LATERAL WORK MUST BE COORDINATED WITH THE CITY OF WATERTOWN DPW. PRIOR TO ANY PLANNED SHUTDOWN, THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF WATERTOWN DPW, A MINIMUM OF ONE WEEK PRIOR, CONSUMERS SHALL BE NOTIFIED 24 HOURS PRIOR TO ANY PLANNED SHUTDOWN. LATERALS TO CONSUMERS SHALL BE MAINTAINED WITH AS LITTLE INTERRUPTIONS AS POSSIBLE. CONSUMER SANITARY SEWER LATERALS SHALL NOT BE INTERRUPTED LONGER THAN 4 HOURS, OR TEMPORARY SERVICES MAY BE REQUIRED BY THE CONTRACTOR, AS ORDERED BY CITY ENGINEERING.
- ALL WORK TO BE PERFORMED WITHIN THE CITY OF WATERTOWN MARGIN SHALL BE BUILT ACCORDING TO THE APPROVED SITE PLAN AND APPLICABLE CITY OF WATERTOWN STANDARDS. COMPACTION TESTING WILL BE REQUIRED FOR ALL WORK TO BE PERFORMED WITHIN THE CITY OF WATERTOWN MARGIN AND MUST BE SUBMITTED TO THE CITY OF WATERTOWN CODES DEPARTMENT.
- CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO CITY ENGINEERING PRIOR TO ANY CONSTRUCTION ACTIVITIES. CONTRACTOR MAY BE ABLE TO CLOSE STREETS TO "LOCAL TRAFFIC ONLY" DURING CONSTRUCTION ACTIVITIES. TRAVEL LANES AND ACCESS TO ALL PROPERTIES SHALL BE PROVIDED AT THE END OF EACH WORKING DAY.

EXISTING	PROPOSED	LEGEND
---	---	MAJOR CONTOUR
---	---	MINOR CONTOUR
---	---	PROPERTY LINE
---	---	RIGHT OF WAY
---	---	SETBACK LINE
---	---	BUILDING OUTLINE
---	---	EDGE OF PAVEMENT
---	---	EDGE OF GRAVEL
---	---	CENTERLINE
---	---	PAVEMENT MARKINGS
---	---	CURB LINE
---	---	SIDEWALK
---	---	TREELINE
---	---	FENCE LINE
---	---	STREAM/EDGE OF WATER
---	---	WATER LINE
---	---	SANITARY SEWER LINE
---	---	FORCEMAIN
---	---	STORM SEWER LINE
---	---	STORM DRAIN LINE
---	---	GAS LINE
---	---	OVERHEAD UTILITY LINE
---	---	UNDERGROUND UTILITY LINE
---	---	OVERHEAD ELECTRIC LINE
---	---	UNDERGROUND ELECTRIC LINE
---	---	TELEPHONE LINE
---	---	FIRE HYDRANT
---	---	WATER VALVE
---	---	WATER SHUTOFF
---	---	SANITARY SEWER MANHOLE
---	---	CLEANOUT
---	---	STORM SEWER MANHOLE
---	---	CATCH BASIN
---	---	UTILITY POLE
---	---	LIGHT POLE
---	---	TELEPHONE PEDESTAL
---	---	GAS VALVE
---	---	TRACER WIRE BOX
---	---	BOLLARD
---	---	CITY MONUMENT
---	---	SIGN
---	---	CITY STREET LIGHT
---	---	SHRUB
---	---	TREES
---	---	STUMP
---	---	BENCHMARK
---	---	SURVEY CONTROL POINT

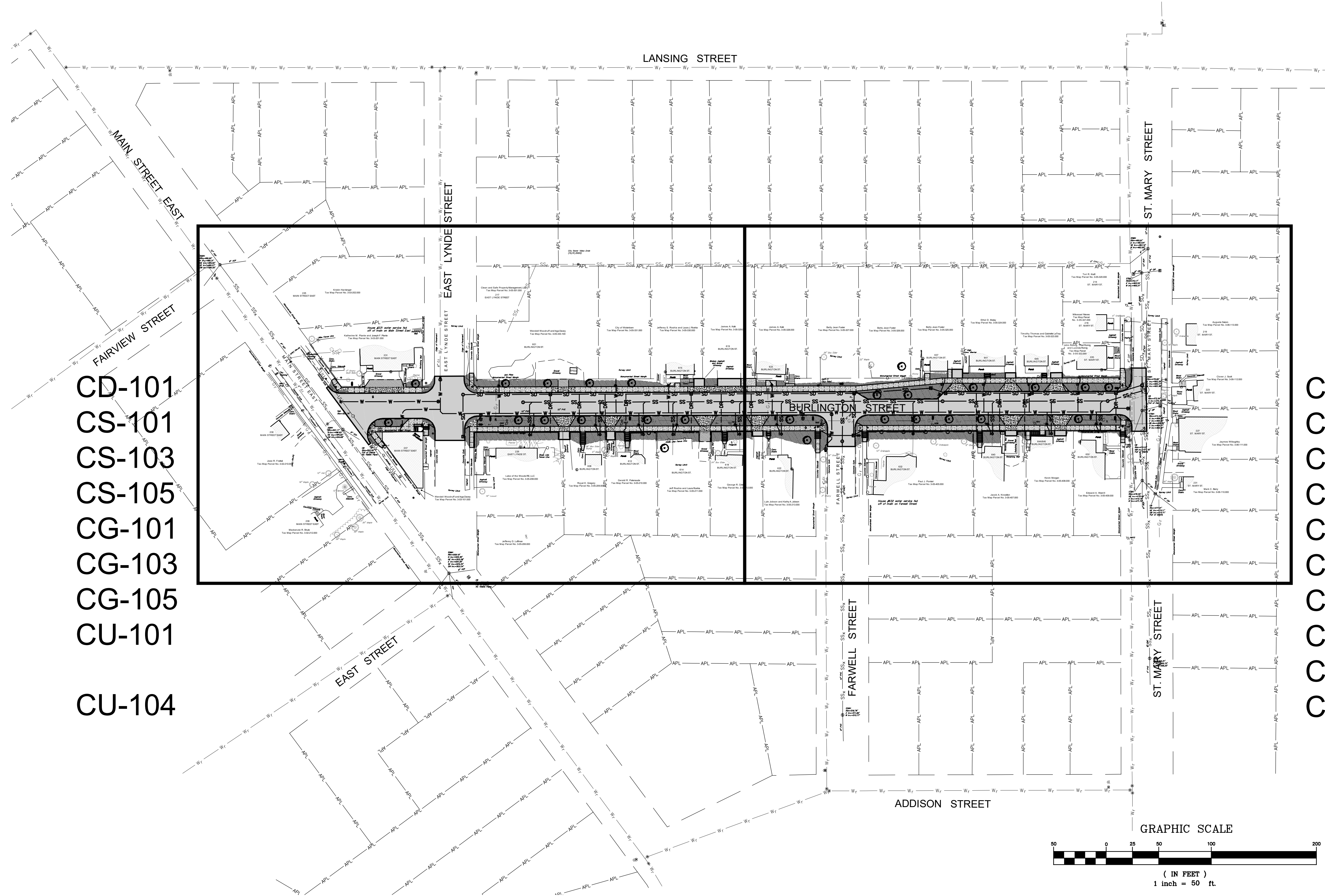
NOTE: THE EXISTING UTILITIES INDICATED BY LINETYPES WITH THE SUBSCRIPT "x" ARE DERIVED FROM CITY OF WATERTOWN GIS RECORD FILES. THE EXISTING UTILITIES INDICATED BY LINETYPES WITH SUBSCRIPT "s" ARE DERIVED FROM CITY OF WATERTOWN PUBLIC WORKS PAINT MARKINGS/FLAGGING AND/OR VISUAL VERIFICATION.

PROJECT NO:	2023-052
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INDEX OF DRAWINGS,
GENERAL NOTES, LEGEND
AND STREET BASELINE

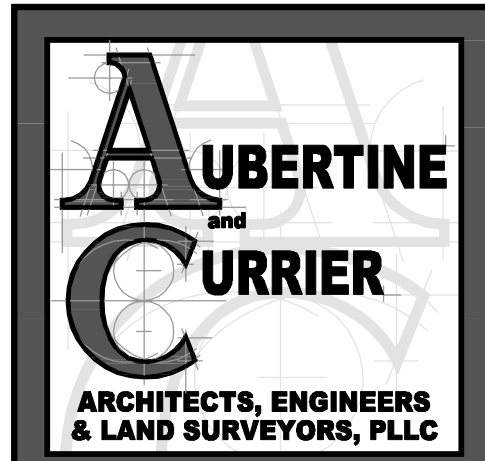
G-100

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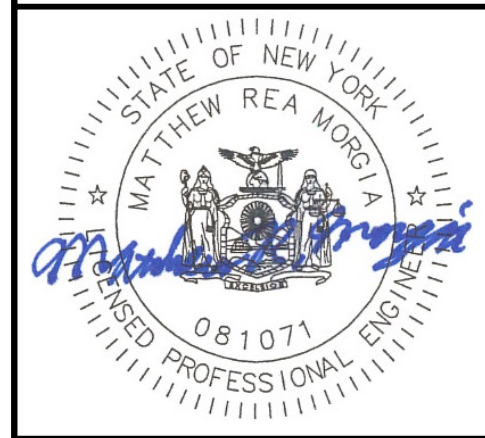


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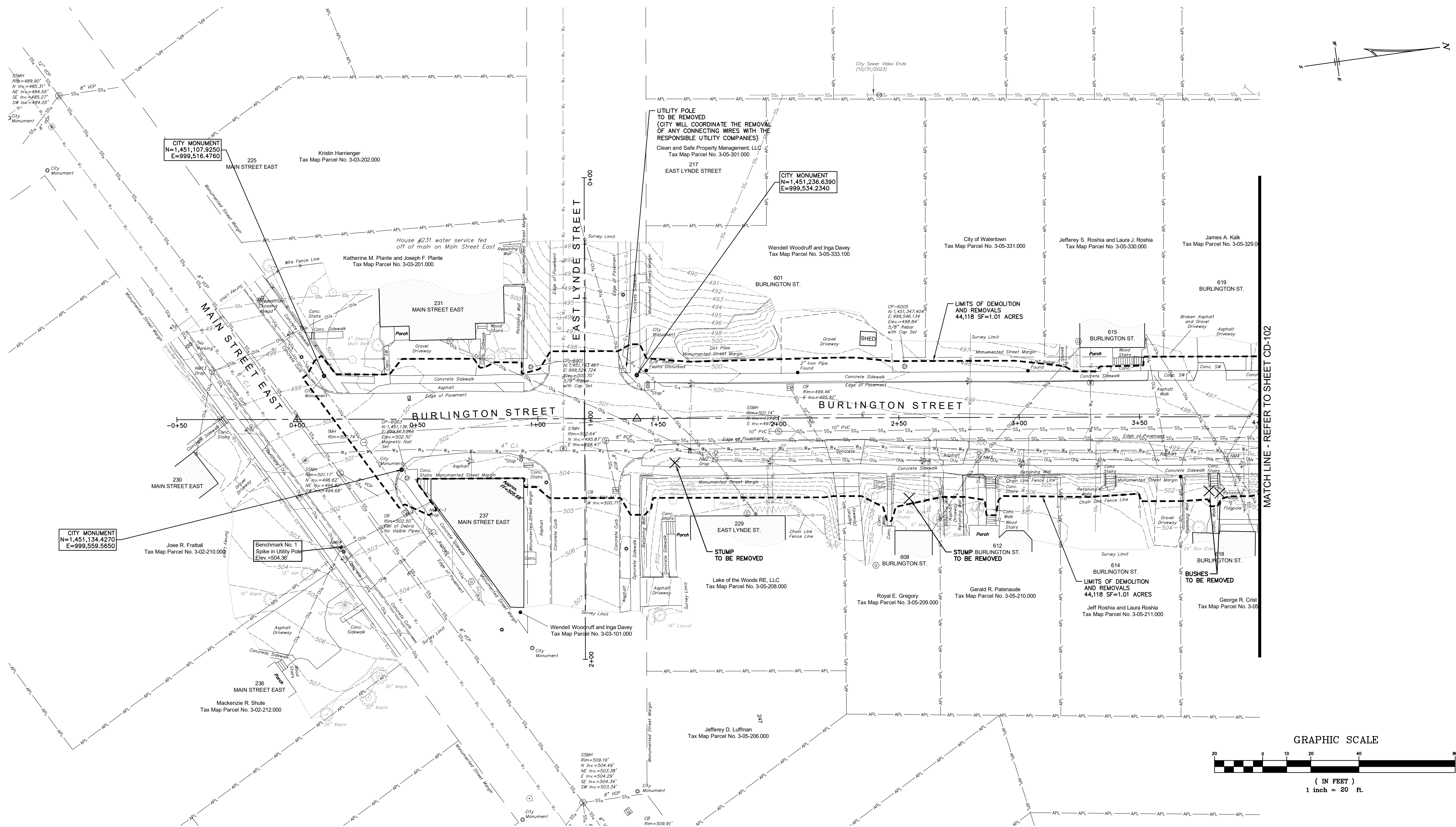


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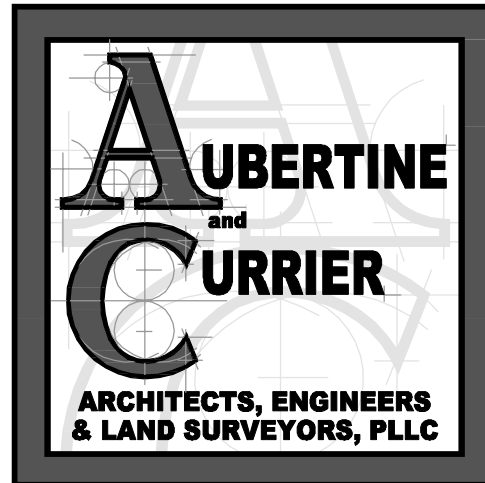
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OVERALL PLAN AND SHEET KEY MAP

C-100



- TREE REMOVAL GENERAL NOTES:**
- REMOVE ALL PRIVATELY OWNED TREES, INCLUDING THE STUMPS, AND SITE RESTORATION, AS INDICATED BELOW, AND THE PLAN SHEETS.
 - REMOVE STUMP AT 229 EAST LYNDE STREET.
 - REMOVE STUMP AT 608 BURLINGTON STREET.
 - REMOVE TWO BUSHES AT 618 BURLINGTON STREET.
 - REMOVE 52" DIAMETER SILVER MAPLE AT 622 BURLINGTON STREET.
 - REMOVE A LARGE PORTION OF A PARTIALLY DOWN TRUNK OF A 24" DIAMETER BOX ELDER THAT IS RESTING ON GUY WIRE ON TAX MAP PARCEL 3-05-327.000.
 - REMOVE 14" DIAMETER ELM AT 640 BURLINGTON STREET.
 - REMOVE 4" DIAMETER BOX ELDER AT 644/646 BURLINGTON STREET.
 - THE CONTRACTOR WILL COORDINATE WITH THE CITY ARBORIST TO CAUSE THE REMOVAL OF A SPECIFIED TREE AS INDICATED IN THIS SECTION BY WAY OF A PROFESSIONAL TREE SERVICE. THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL OF ALL REMAINING PARTS OF THE TREES AND STUMPS FROM THE JOB SITE.
 - ANY WORK PERFORMED NEAR TREES NOT BEING REMOVED REQUIRES A TREE PROTECTION SYSTEM, LIKE THE ONE SHOWN ON THE PLAN SHEET OR AS SPECIFIED BY THE CITY ARBORIST.
 - THE PROFESSIONAL TREE SERVICE CONTRACTOR SHALL GRIND ENTIRE STUMP, AND ANY RAISED AREAS ASSOCIATED WITH THE STUMP, TO A DEPTH OF A MINIMUM OF 9 INCHES. THIS WORK WILL INCLUDE SURFACE ROOTS AND ROOT PLATE. THE RESULTING GRADE WILL BE LEVEL WITH THAT OF THE SURROUNDING YARD. THE STUMP CAVITY SHALL BE CLEARED OF ALL GROUND CHIP AND OTHER WOODY DEBRIS, BACKFILLED WITH TOPSOIL AND SEEDED.
 - IMMEDIATELY CONTACT THE CITY ARBORIST AND CITY ENGINEER OF ANY PROPERTY DAMAGED, I.E. FENCES, STAIRS, SIDEWALK BLOCKS, ETC., DISTURBED DURING THE REMOVAL OF TREES, BUSHES, OR STUMPS.
 - REMOVE ALL DEBRIS FROM JOB SITE, AND LEAVE THE SITES IN A NEAT AND ORDERLY FASHION.
 - TREE REMOVAL WORK CAN TAKE PLACE BETWEEN NOVEMBER 1 - MARCH 31 OF A GIVEN YEAR WITHOUT PERFORMING A BAT SURVEY. IF WORK IS CONDUCTED OUTSIDE OF THIS WINDOW, THE CONTRACTOR SHALL COORDINATE WITH THE CITY TO ENSURE A BAT SURVEY IS CONDUCTED PER NYS DEPARTMENT OF ENVIRONMENTAL CONSERVATION PROTOCOL.
 - THE CONTRACTOR AND ITS SUBCONTRACTOR SHALL FOLLOW ALL ANSI (AMERICAN NATIONAL STANDARDS INSTITUTE) Z133 SAFETY STANDARDS DURING TREE AND STUMP REMOVAL OPERATIONS. IF THE CONTRACTOR DOES NOT HAVE A COPY OF THE REFERENCED SAFETY STANDARDS, THE CITY WILL PROVIDE A COPY PRIOR TO THE START OF WORK.
 - THE COORDINATION WITH NATIONAL GRID WILL BE NECESSARY DUE TO THE PROXIMITY OF PARTICULAR TREE AND OVERHEAD PRIMARY LINES. NATIONAL GRID WILL COORDINATE WITH THE CITY AND ASSIST IN THE PARTIAL REMOVAL OF THE TREE PRIOR TO THE CONTRACTOR COMPLETING THEIR WORK.
 - ALL WOOD LEFT ON SITE BY NATIONAL GRID, SHALL BE REMOVED BY THE CONTRACTOR. IF NATIONAL GRID'S WORK CAUSES DISTURBANCE TO THE PRIVATELY OWNED LAWN AREA, THE CONTRACTOR SHALL RESTORE THE YARD TO PRE-REMOVAL CONDITION AS PART OF THIS CONTRACT.

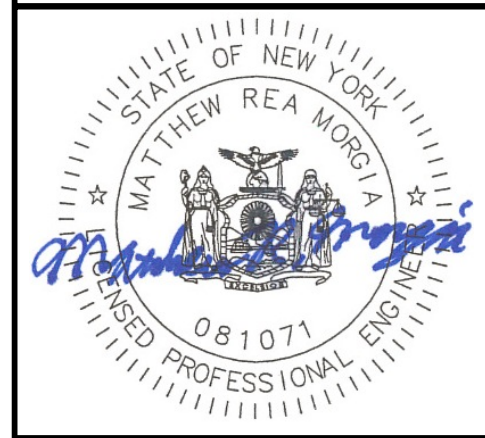


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EXISTING CONDITIONS AND DEMOLITION PLAN

CD-101

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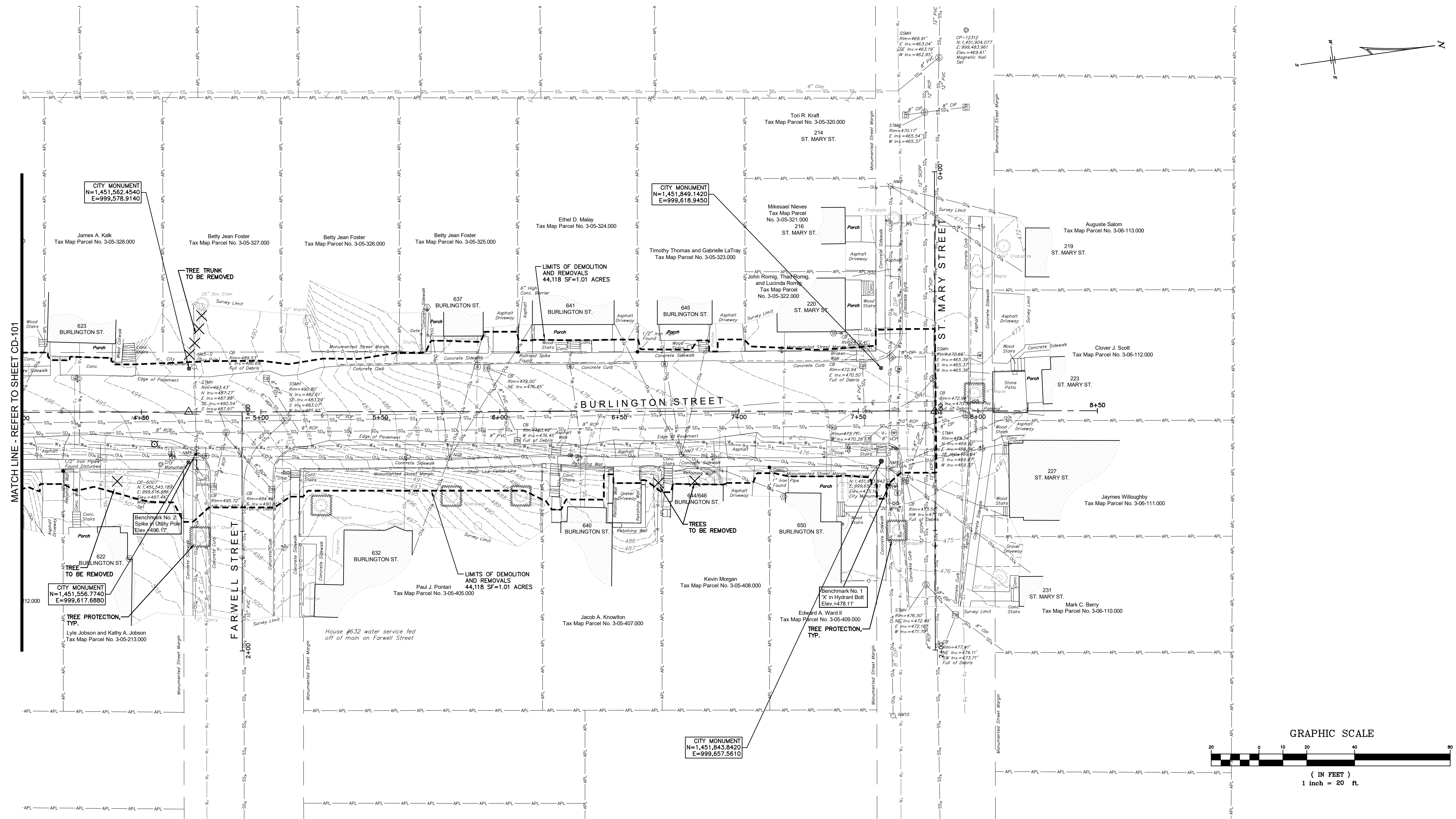
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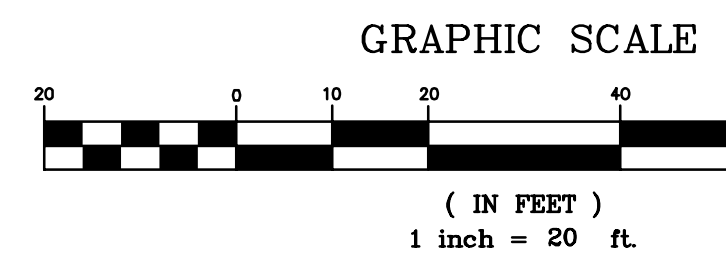
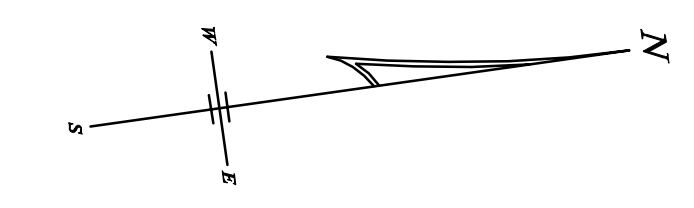
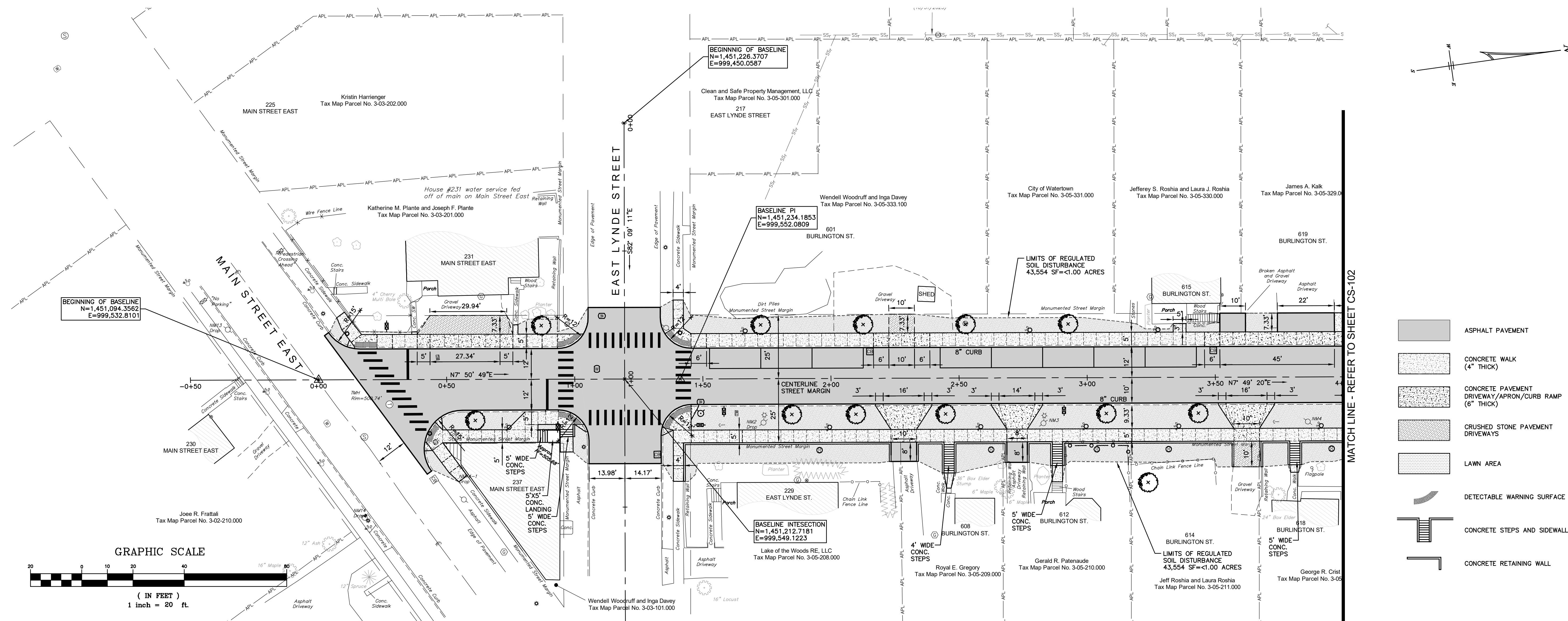
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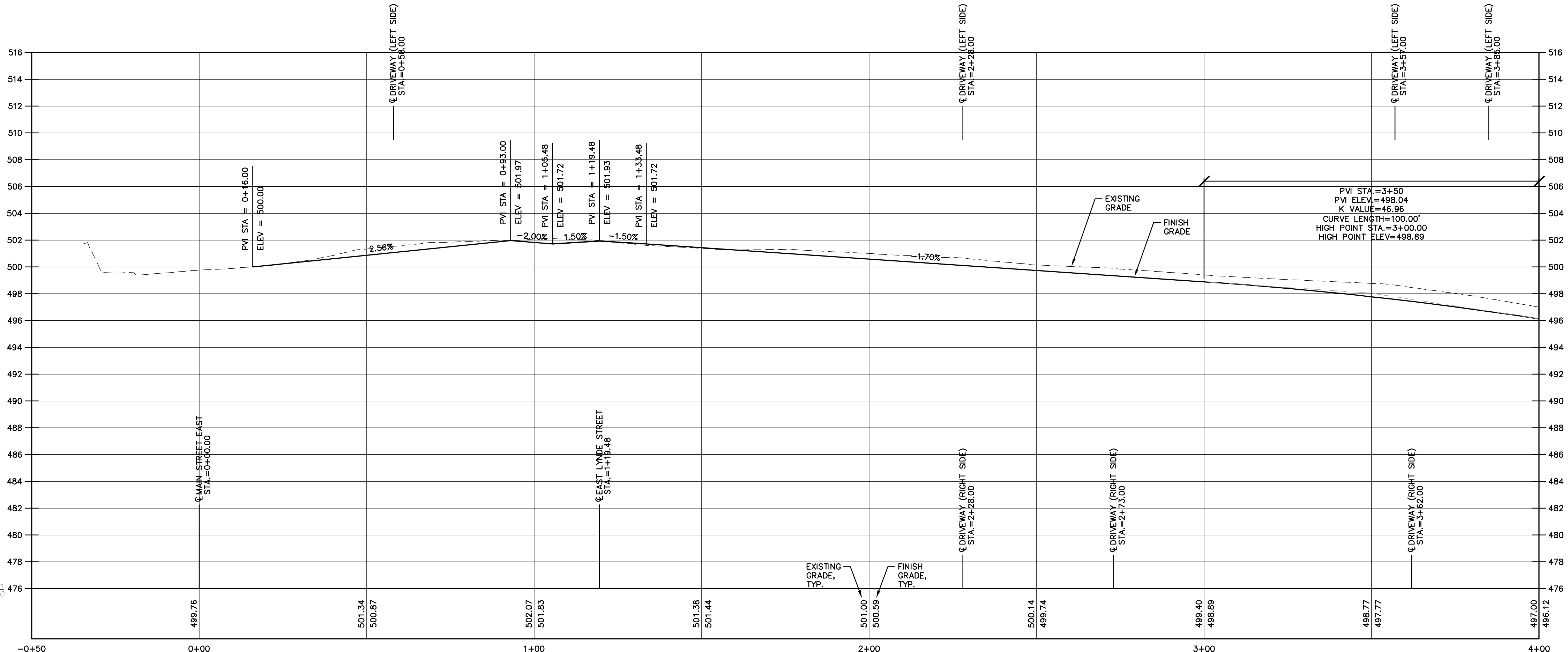


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 - REMOVE 4" DIAMETER BOX ELDER AT 644/646 BURLINGTON STREET.
 - THE CONTRACTOR WILL COORDINATE WITH THE CITY ARBORIST TO CAUSE THE REMOVAL OF A SPECIFIED TREE AS INDICATED IN THIS SECTION BY WAY OF A PROFESSIONAL TREE SERVICE. THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL OF ALL REMAINING PARTS OF THE TREES AND STUMPS FROM THE JOB SITE.
 - ANY WORK PERFORMED NEAR TREES NOT BEING REMOVED REQUIRES A TREE PROTECTION SYSTEM, LIKE THE ONE SHOWN ON THE PLAN SHEET OR AS SPECIFIED BY THE CITY ARBORIST.
 - THE PROFESSIONAL TREE SERVICE CONTRACTOR SHALL GRIND ENTIRE STUMP, AND ANY RAISED AREAS ASSOCIATED WITH THE STUMP, TO A DEPTH OF A MINIMUM OF 9 INCHES. THIS WORK WILL INCLUDE SURFACE ROOTS AND ROOT PLATE. THE RESULTING GRADE WILL BE LEVEL WITH THAT OF THE SURROUNDING YARD. THE STUMP CAVITY SHALL BE CLEARED OF ALL GROUND CHIP AND OTHER WOODY DEBRIS, BACKFILLED WITH TOPSOIL AND SEEDS.
 - IMMEDIATELY CONTACT THE CITY ARBORIST AND CITY ENGINEER OF ANY PROPERTY DAMAGED, I.E. FENCES, STAIRS, SIDEWALK BLOCKS, ETC., DISTURBED DURING THE REMOVAL OF TREES, BUSHES, OR STUMPS.
 - REMOVE ALL DEBRIS FROM JOB SITE, AND LEAVE THE SITES IN A NEAT AND ORDERLY FASHION.
 - TREE REMOVAL WORK CAN TAKE PLACE BETWEEN NOVEMBER 1 - MARCH 31 OF A GIVEN YEAR WITHOUT PERFORMING A BAT SURVEY. IF WORK IS CONDUCTED OUTSIDE OF THIS WINDOW, THE CONTRACTOR SHALL COORDINATE WITH THE CITY TO ENSURE A BAT SURVEY IS CONDUCTED PER NYS DEPARTMENT OF ENVIRONMENTAL CONSERVATION PROTOCOL.
 - THE CONTRACTOR AND ITS SUBCONTRACTOR SHALL FOLLOW ALL ANSI (AMERICAN NATIONAL STANDARDS INSTITUTE) Z133 SAFETY STANDARDS DURING TREE AND STUMP REMOVAL OPERATIONS. IF THE CONTRACTOR DOES NOT HAVE A COPY OF THE REFERENCED SAFETY STANDARDS, THE CITY WILL PROVIDE A COPY PRIOR TO THE START OF WORK.
 - THE COORDINATION WITH NATIONAL GRID WILL BE NECESSARY DUE TO THE PROXIMITY OF PARTICULAR TREE AND OVERHEAD PRIMARY LINES. NATIONAL GRID WILL COORDINATE WITH THE CITY AND ASSIST IN THE PARTIAL REMOVAL OF THE TREE PRIOR TO THE CONTRACTOR COMPLETING THEIR WORK.
 - ALL WOOD LEFT ONSITE BY NATIONAL GRID, SHALL BE REMOVED BY THE CONTRACTOR. IF NATIONAL GRID'S WORK CAUSES DISTURBANCE TO THE PRIVATELY OWNED LAWN AREA, THE CONTRACTOR SHALL RESTORE THE YARD TO PRE-REMOVAL CONDITION AS PART OF THIS CONTRACT.

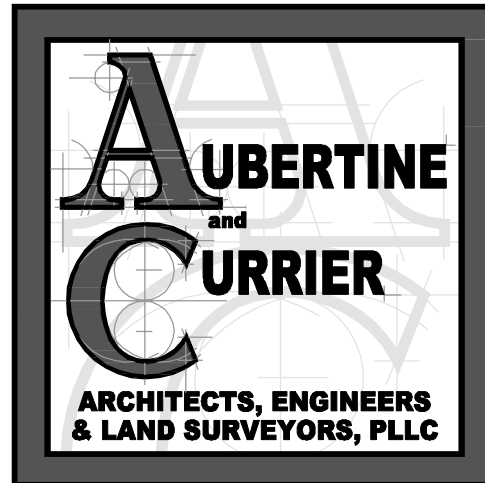
MATCH LINE - REFER TO SHEET CD-101



- ASPHALT PAVEMENT
-



CENTERLINE PROFILE - BURLINGTON STREET (STA.0+00 TO STA.4+00)
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

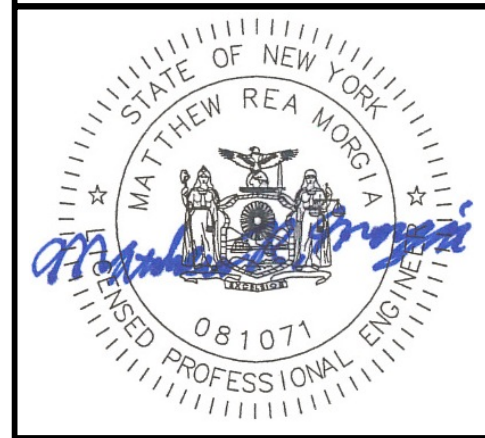


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 Watertown, New York 13601
 Phone: (315)782-2005

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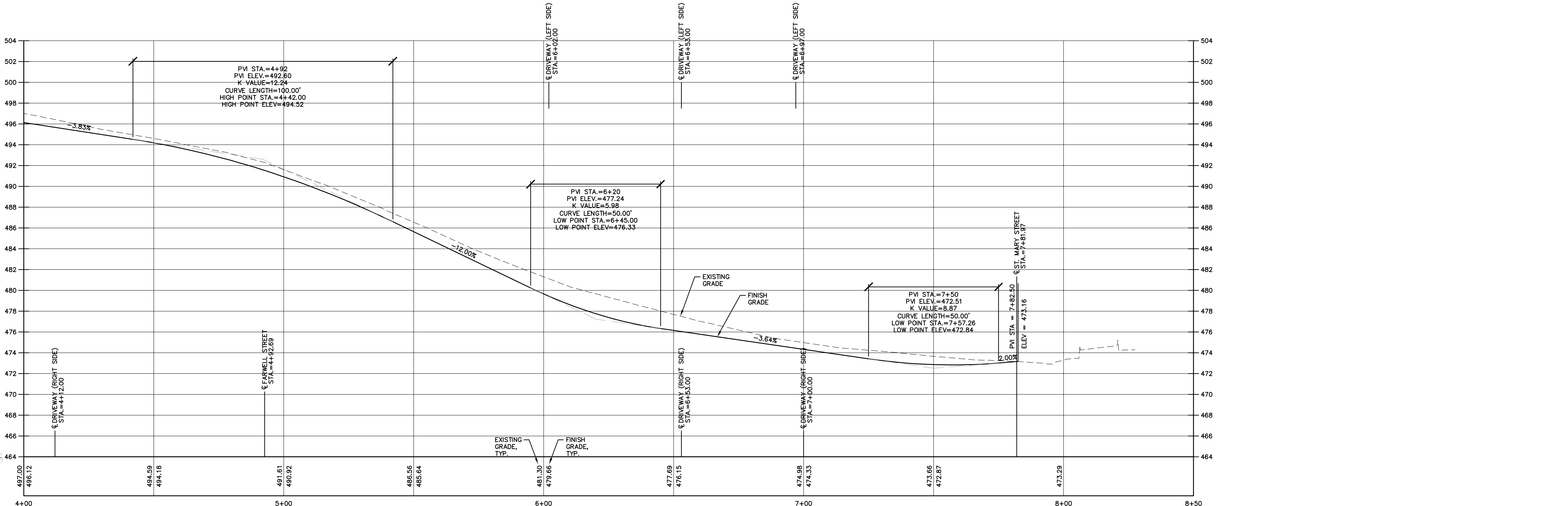
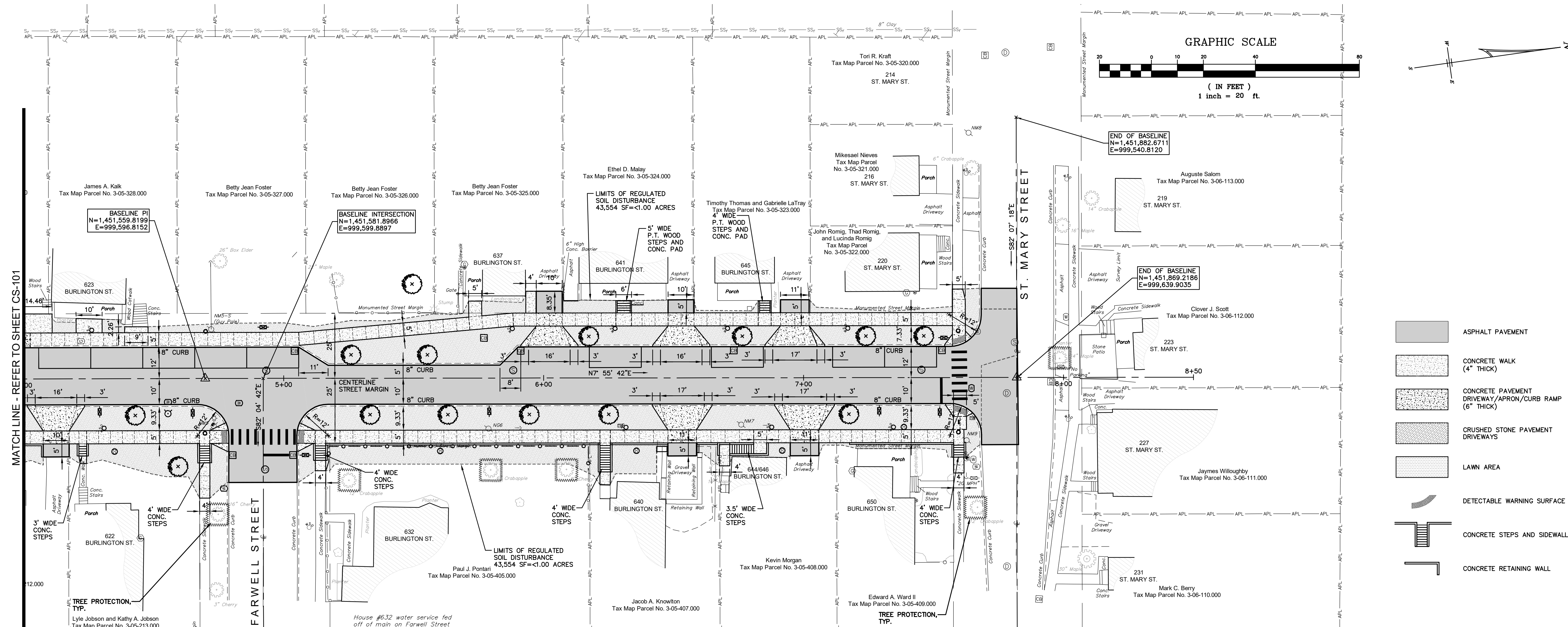


**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

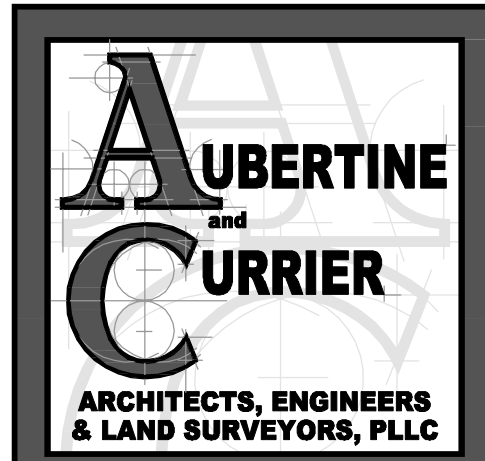
SITE PLAN

CS-101



CENTERLINE PROFILE - BURLINGTON STREET (STA.4+00 TO STA.8+00)
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

12/13/2024 BID DOCUMENTS
 FOR CONSTRUCTION

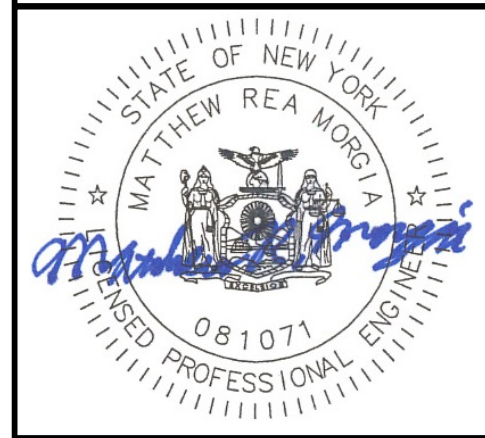


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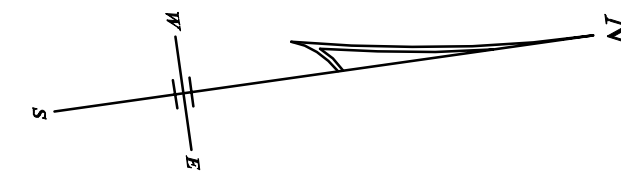


**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

SITE PLAN

CS-102

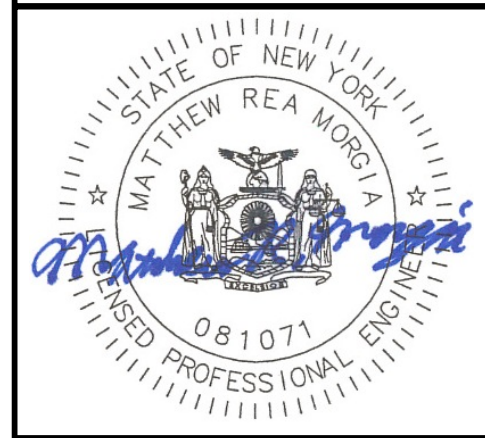


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BURLINGTON STREET RECONSTRUCTION PROJECT

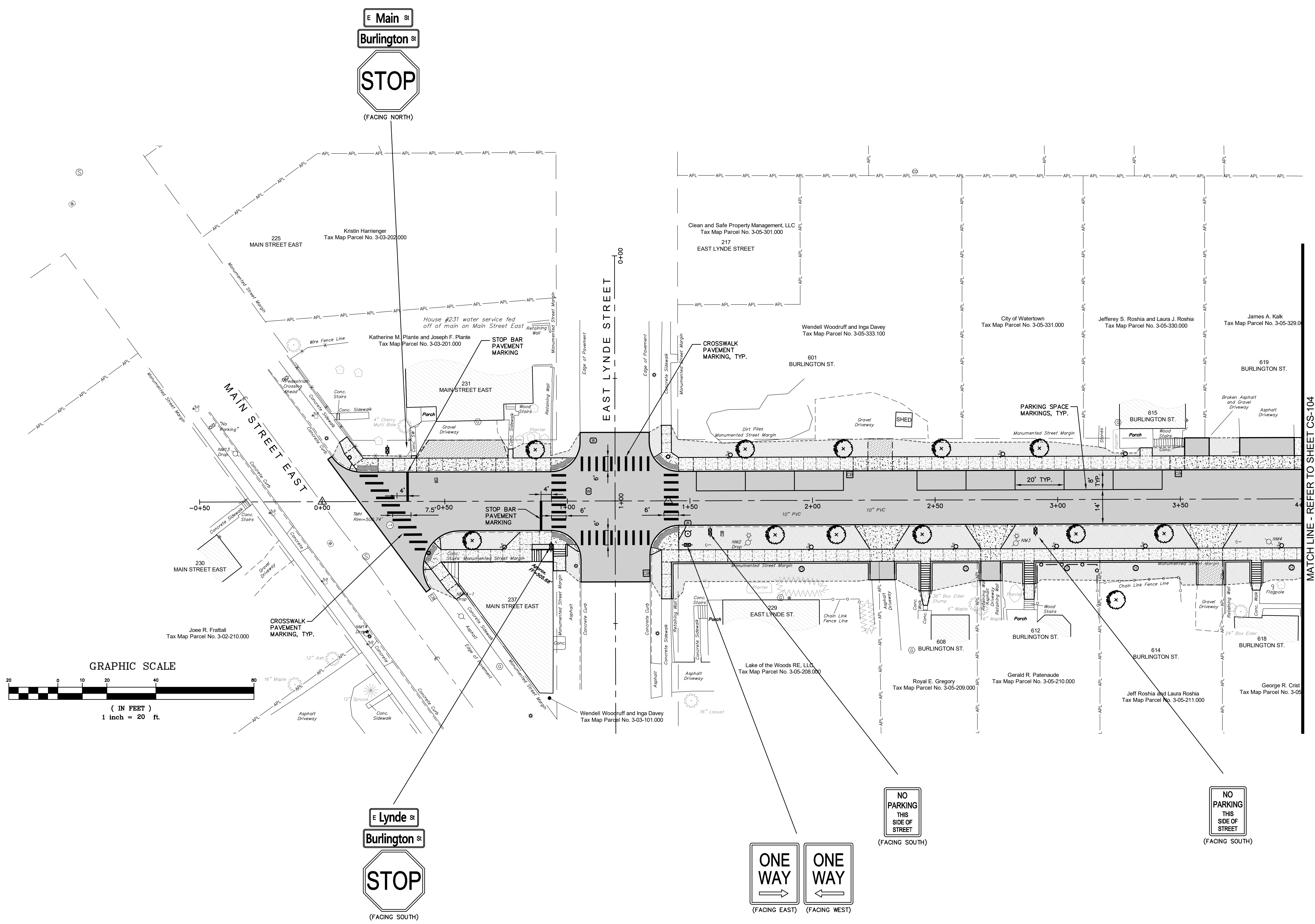
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

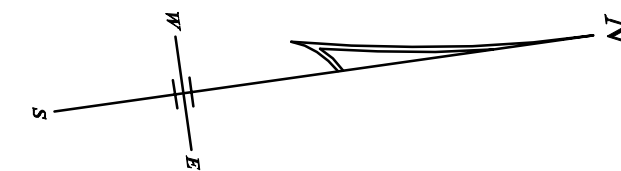
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PAVEMENT MARKINGS
AND SIGNAGE PLAN

CS-103

12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



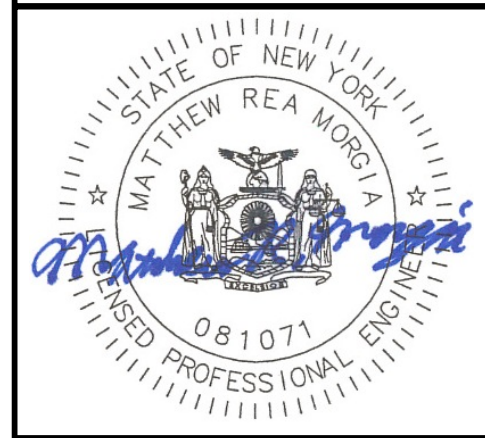


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BURLINGTON STREET RECONSTRUCTION PROJECT

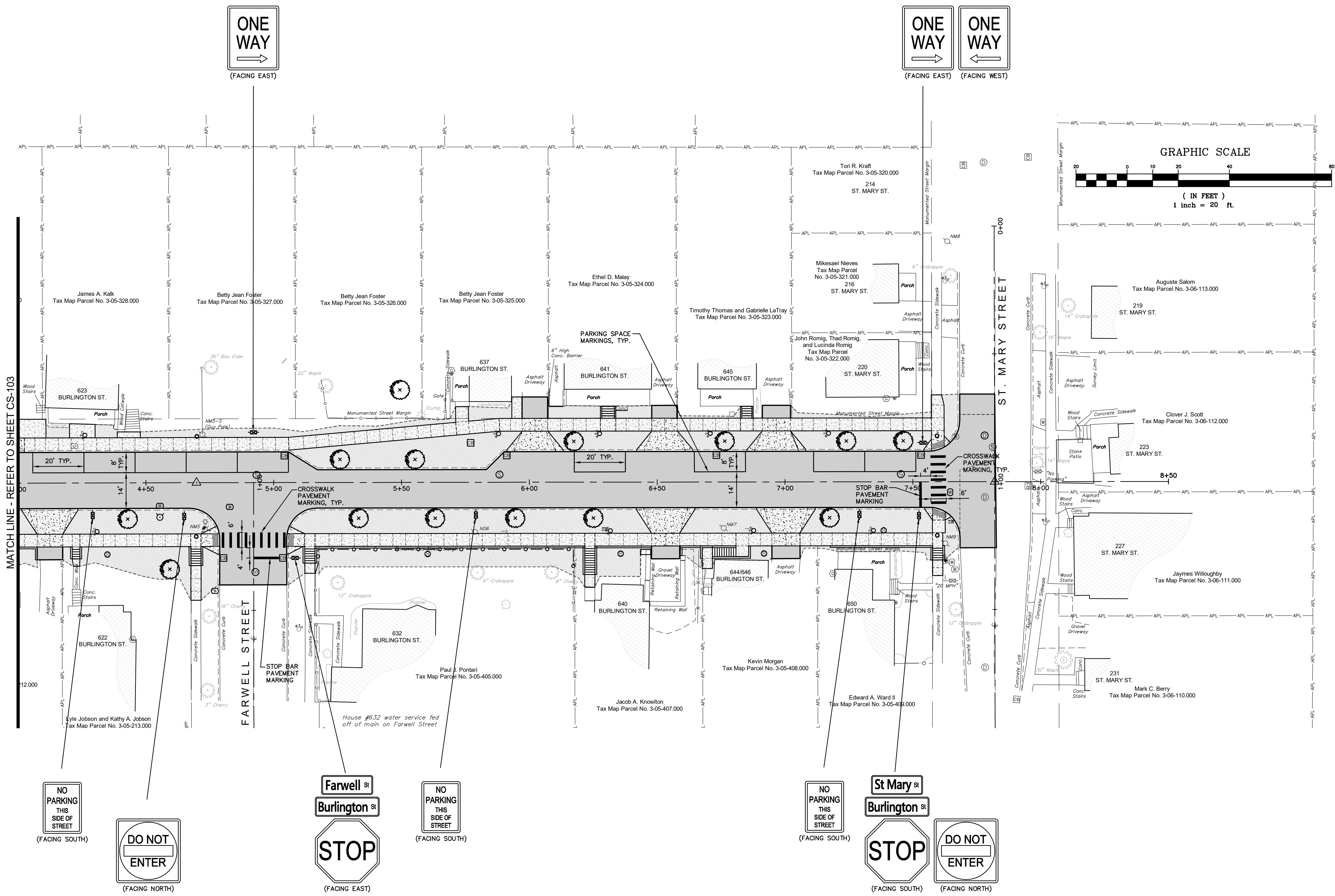
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

PAVEMENT MARKINGS
AND SIGNAGE PLAN

CS-104

12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



NO PARKING
THIS SIDE OF
STREET
(FACING SOUTH)

DO NOT
ENTER
(FACING NORTH)

Farwell St
Burlington St
STOP
(FACING EAST)

NO PARKING
THIS SIDE OF
STREET
(FACING SOUTH)

NO PARKING
THIS SIDE OF
STREET
(FACING SOUTH)

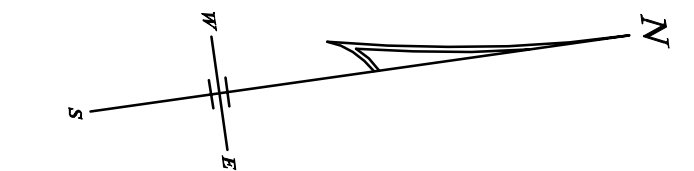
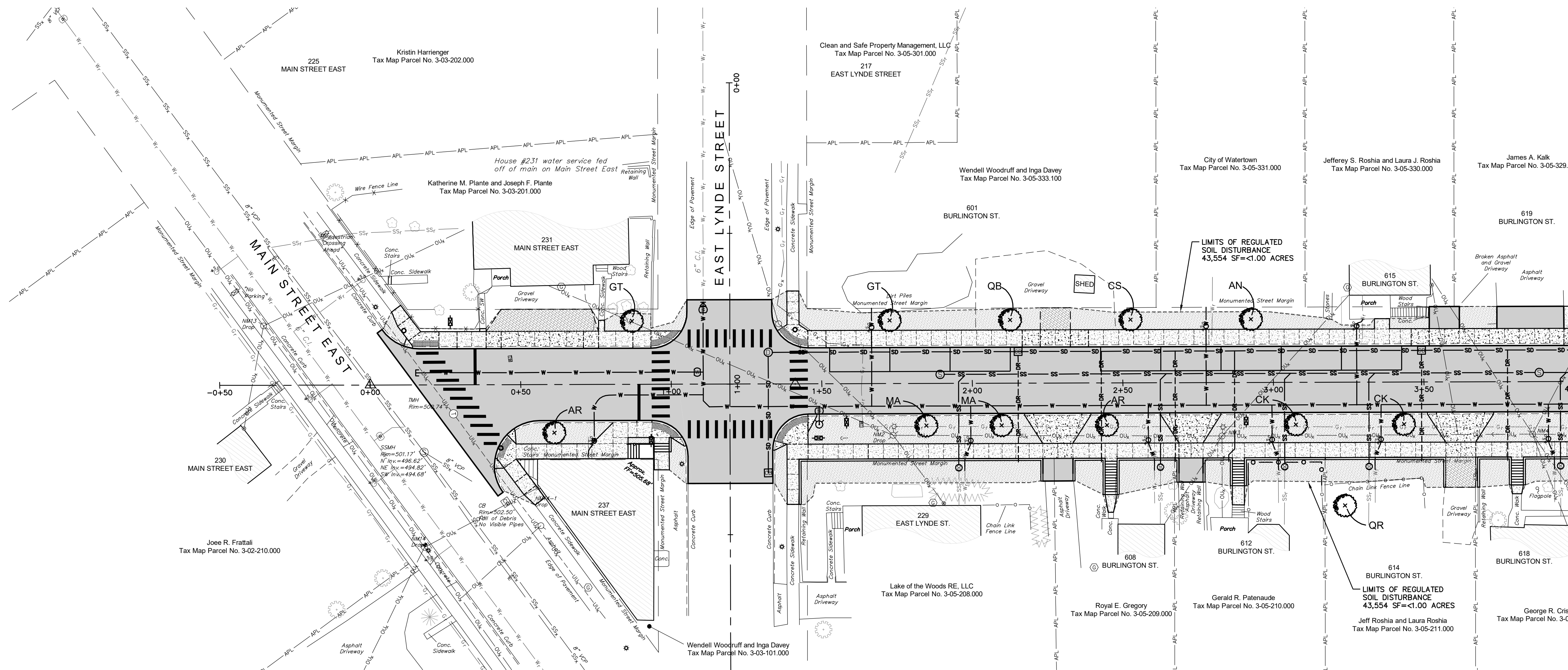
St Mary St
Burlington St
STOP
DO NOT
ENTER
(FACING SOUTH) (FACING NORTH)

ONE
WAY
(FACING EAST)

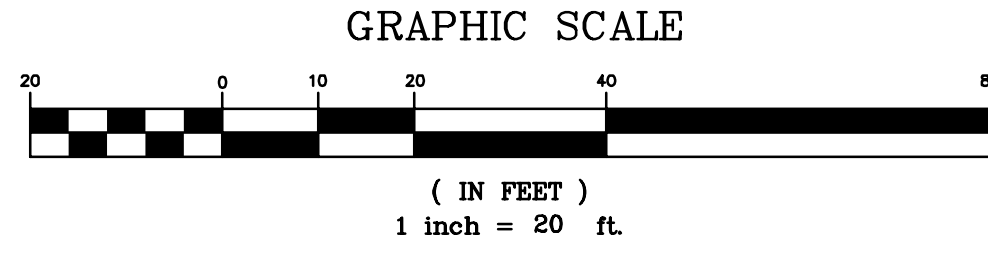
ONE
WAY (FACING EAST)
ONE
WAY (FACING WEST)

GRAPHIC SCALE
(IN FEET)
1 inch = 20 ft.

MATCH LINE - REFER TO SHEET CS-103

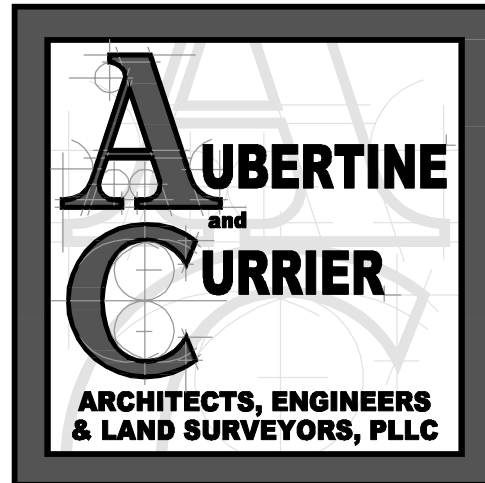


- ASPHALT PAVEMENT
- CONCRETE WALK (4" THICK)
- CONCRETE PAVEMENT DRIVEWAY/APRON/CURB RAMP (6" THICK)
- CRUSHED STONE PAVEMENT DRIVEWAYS
- LAWN AREA
- DETECTABLE WARNING SURFACE
- CONCRETE STEPS AND SIDEWALKS
- CONCRETE RETAINING WALL



TREE PLANTING SCHEDULE						
LABEL	ADDRESS	BOTANICAL NAME	COMMON NAME	SIZE (CALIPER)	NOTES	STATION OFFSET
GT	231 MAIN STREET EAST	GLEDITSIA TRIACANTHOS 'STREETKEEPER'	STREET KEEPER HONEYLOCUST	2"	BALLED AND BURLAPPED	STA:0+87, 21' LT.
AR	237 MAIN STREET EAST	AMELANCHIER X GANDIFLORA 'ROBIN HILL'	ROBIN HILL SERVICEBERRY	2"	BALLED AND BURLAPPED	STA:0+61, 16' RT.
MA	229 EAST LYNDE STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA:1+84, 14' RT.
MA	229 EAST LYNDE STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA:2+10, 14' RT.
GT	601 BURLINGTON STREET	GLEDITSIA TRIACANTHOS 'STREETKEEPER'	STREET KEEPER HONEYLOCUST	2"	BALLED AND BURLAPPED	STA:1+73, 21' LT.
QB	601 BURLINGTON STREET	QUERCUS BICOLOR	SWAMP WHITE OAK	2"	BALLED AND BURLAPPED	STA:2+13, 21' LT.
CS	601 BURLINGTON STREET	CATALPA SPECIOSA	NORTHERN CATALPA	2"	BALLED AND BURLAPPED	STA:2+53, 21' LT.
AR	608 BURLINGTON STREET	AMELANCHIER X GANDIFLORA 'ROBIN HILL'	ROBIN HILL SERVICEBERRY	2"	BALLED AND BURLAPPED	STA:2+44, 14' RT.
AN	BURLINGTON STREET	ACER NIGRUM	BLACK MAPLE	2"	BALLED AND BURLAPPED	STA:2+93, 21' LT.
CK	612 BURLINGTON STREET	CADRASTIS KENTUKEA	YELLOWWOOD	2"	BALLED AND BURLAPPED	STA:3+07, 14' RT.
CK	614 BURLINGTON STREET	CADRASTIS KENTUKEA	YELLOWWOOD	2"	BALLED AND BURLAPPED	STA:3+43, 14' RT.
QR	614 BURLINGTON STREET	QUERCUS X MACDANIELII 'CLEMONS'	HERITAGE OAK	2"	BALLED AND BURLAPPED	STA:3+24, 41' RT.
MA	622 BURLINGTON STREET	MALUS 'ADIRONDACK'	ADIRONDACK CRABAPPLE	2"	BALLED AND BURLAPPED	STA:4+43, 21' RT.
ZS	622 BURLINGTON STREET	ZELKOVA SERRATA	CITY SPRITE ZELKOVA	2"	BALLED AND BURLAPPED	STA:4+59, 34' RT.
PS	632 BURLINGTON STREET	PRUNUS SARGENTII 'PINK FLAIR'	PINK FLAIR SARGENT CHERRY	2"	BALLED AND BURLAPPED	STA:5+33, 14' RT.
PS	632 BURLINGTON STREET	PRUNUS SARGENTII 'PINK FLAIR'	PINK FLAIR SARGENT CHERRY	2"	BALLED AND BURLAPPED	STA:5+63, 14' RT.
CC	632 BURLINGTON STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA:5+94, 14' RT.
CC	640 BURLINGTON STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA:6+25, 14' RT.
AN	637 BURLINGTON STREET	ACER NIGRUM	BLACK MAPLE	2"	BALLED AND BURLAPPED	STA:5+49, 36' LT.
PA	637 BURLINGTON STREET	PLATANUS X ACERIFOLIA 'EXCLAMATION'	EXCLAMATION LONDON PLANE	2"	BALLED AND BURLAPPED	STA:5+26, 9' LT.
PA	637 BURLINGTON STREET	PLATANUS X ACERIFOLIA 'EXCLAMATION'	EXCLAMATION LONDON PLANE	2"	BALLED AND BURLAPPED	STA:5+55, 9' LT.
AF	641 BURLINGTON STREET	ACER FREEMANII 'MARMO'	MARMO FREEMAN MAPLE	2"	BALLED AND BURLAPPED	STA:6+17, 16' LT.
TA	645 BURLINGTON STREET	TILIA AMERICANA 'BOULEVARD'	BOULEVARD LINDEN	2"	BALLED AND BURLAPPED	STA:6+76, 16' LT.
AR	650 BURLINGTON STREET	AMELANCHIER X GANDIFLORA 'ROBIN HILL'	ROBIN HILL SERVICEBERRY	2"	BALLED AND BURLAPPED	STA:7+17, 14' RT.
QI	220 ST. MARY STREET	QUERCUS IMBRICARIA	SHINGLE OAK	2"	BALLED AND BURLAPPED	STA:7+24, 16' LT.
GT	220 ST. MARY STREET	GLEDITSIA TRIACANTHOS 'STREETKEEPER'	STREET KEEPER HONEYLOCUST	2"	BALLED AND BURLAPPED	STA:7+46, 16' LT.

12/13/2024 BID DOCUMENTS FOR CONSTRUCTION

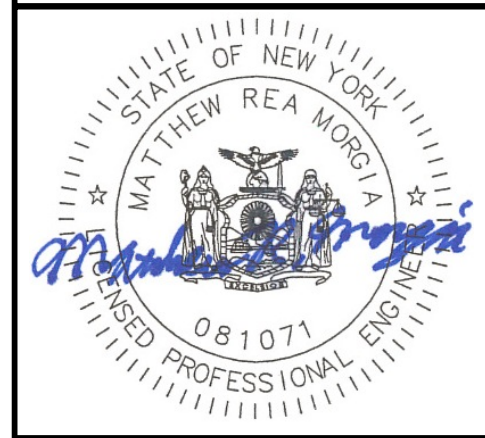


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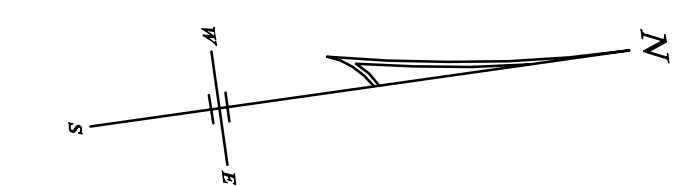
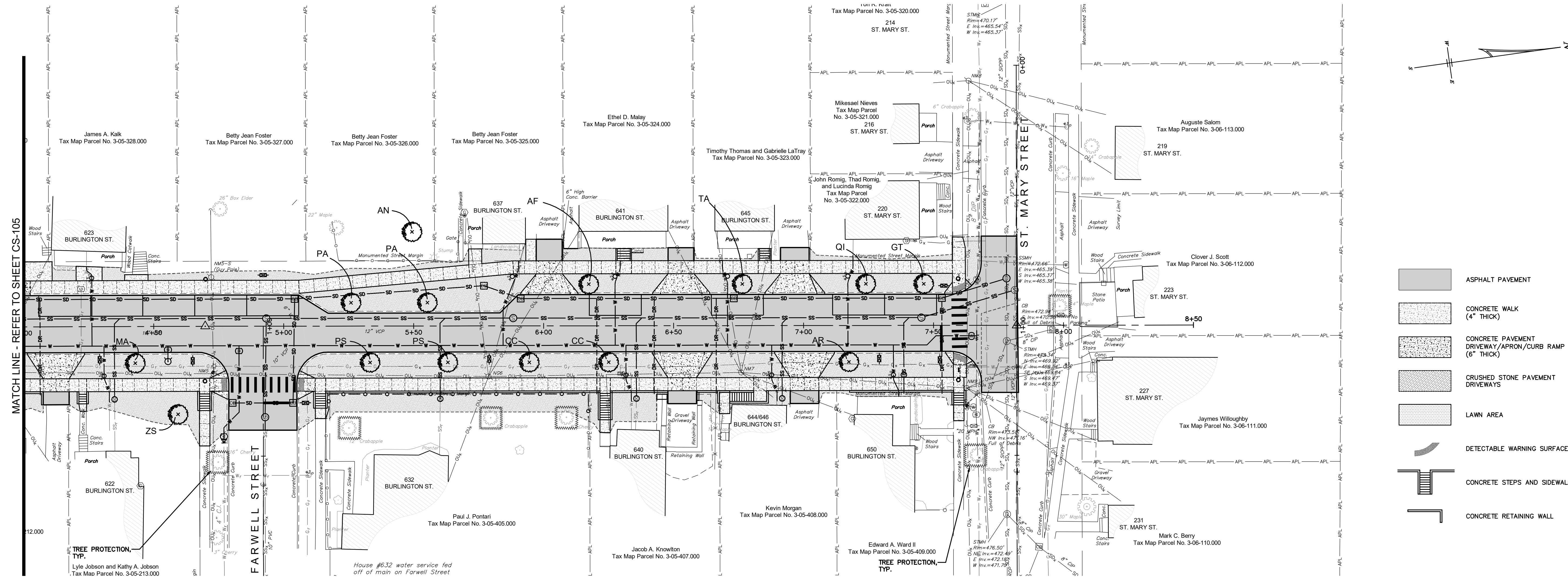



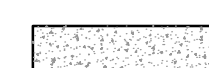
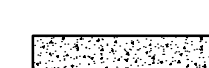


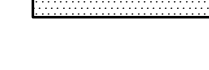


BURLINGTON STREET RECONSTRUCTION PROJECT
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=20'
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/30/2024
12/13/2024

TREE PROTECTION AND PLANTING PLAN

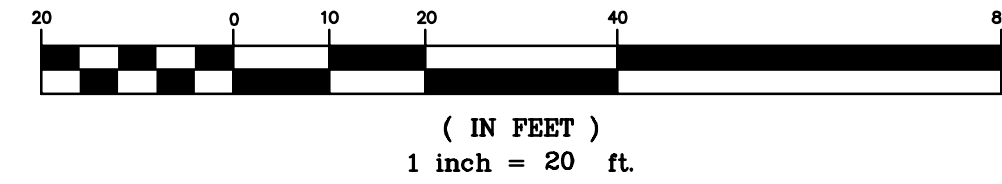
CS-105



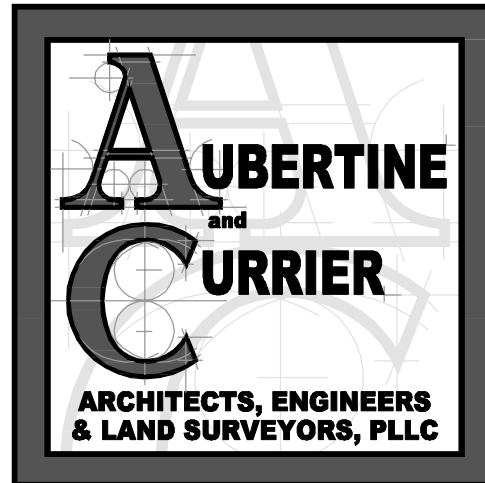
-  ASPHALT PAVEMENT
-  CONCRETE WALK (4" THICK)
-  CONCRETE PAVEMENT DRIVEWAY/APRON/CURB RAMP (6" THICK)
-  CRUSHED STONE PAVEMENT DRIVEWAYS
-  LAWN AREA
-  DETECTABLE WARNING SURFACE
-  CONCRETE STEPS AND SIDEWALLS
-  CONCRETE RETAINING WALL

MATCH LINE - REFER TO SHEET CS-105

GRAPHIC SCALE



TREE PLANTING SCHEDULE						
LABEL	ADDRESS	BOTANICAL NAME	COMMON NAME	SIZE (CALIPER)	NOTES	STATION OFFSET
GT	231 MAIN STREET EAST	GLEDITSIA TRIACANTHOS 'STREETKEEPER'	STREET KEEPER HONEYLOCUST	2"	BALLED AND BURLAPPED	STA.:0+87, 21' LT.
AR	237 MAIN STREET EAST	AMELANCHIER X GANDIFLORA 'ROBIN HILL'	ROBIN HILL SERVICEBERRY	2"	BALLED AND BURLAPPED	STA.:0+61, 16' RT.
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AN	BURLINGTON STREET	ACER NIGRUM	BLACK MAPLE	2"	BALLED AND BURLAPPED	STA.:2+83, 21' LT.
CK	612 BURLINGTON STREET	CADRASTIS KENTUKEA	YELLOWWOOD	2"	BALLED AND BURLAPPED	STA.:3+07, 14' RT.
CK	614 BURLINGTON STREET	CADRASTIS KENTUKEA	YELLOWWOOD	2"	BALLED AND BURLAPPED	STA.:3+43, 14' RT.
QR	614 BURLINGTON STREET	QUERCUS X MACDANIELII 'CLEMONS'	HERITAGE OAK	2"	BALLED AND BURLAPPED	STA.:3+24, 41' RT.
MA	622 BURLINGTON STREET	MALUS 'ADRIONDACK'	ADRIONDACK CRABAPPLE	2"	BALLED AND BURLAPPED	STA.:4+43, 214' RT.
ZS	622 BURLINGTON STREET	ZELKOVA SERRATA	CITY SPRITE ZELKOVA	2"	BALLED AND BURLAPPED	STA.:4+59, 34' RT.
PS	632 BURLINGTON STREET	PRUNUS SARGENTII 'PINK FLAIR'	PINK FLAIR SARGENT CHERRY	2"	BALLED AND BURLAPPED	STA.:5+33, 14' RT.
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CC	632 BURLINGTON STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA.:5+94, 14' RT.
CC	640 BURLINGTON STREET	CERCIS CANADENSIS	EASTERN REDBUD	2"	BALLED AND BURLAPPED	STA.:6+25, 14' RT.
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PA	637 BURLINGTON STREET	PLATANUS X ACERIFOLIA 'EXCLAMATION'	EXCLAMATION LONDON PLANE	2"	BALLED AND BURLAPPED	STA.:5+26, 9' LT.
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QI	220 ST. MARY STREET	QUERCUS IMBRICARIA	SHINGLE OAK	2"	BALLED AND BURLAPPED	STA.:7+24, 16' LT.
GT	220 ST. MARY STREET	GLEDITSIA TRIACANTHOS 'STREETKEEPER'	STREET KEEPER HONEYLOCUST	2"	BALLED AND BURLAPPED	STA.:7+46, 16' LT.

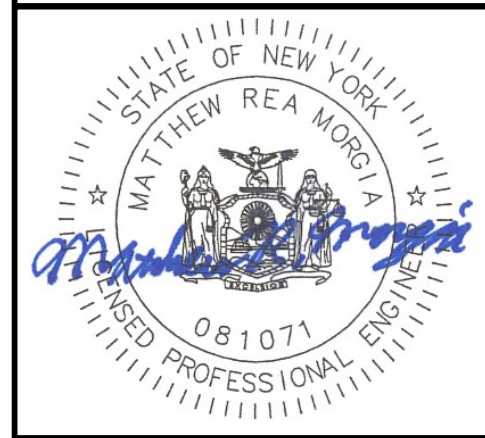


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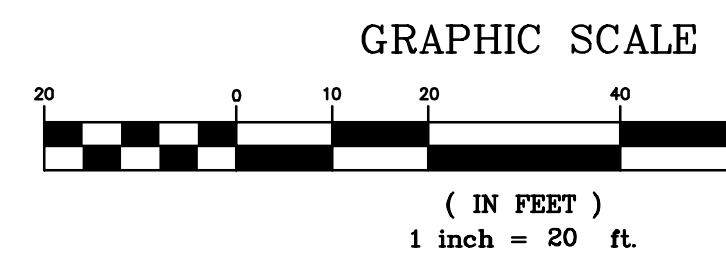
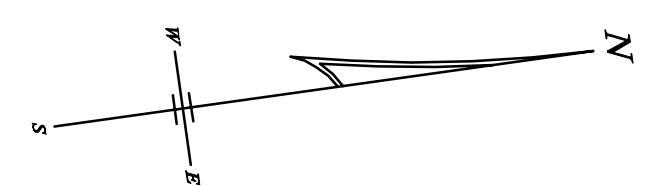
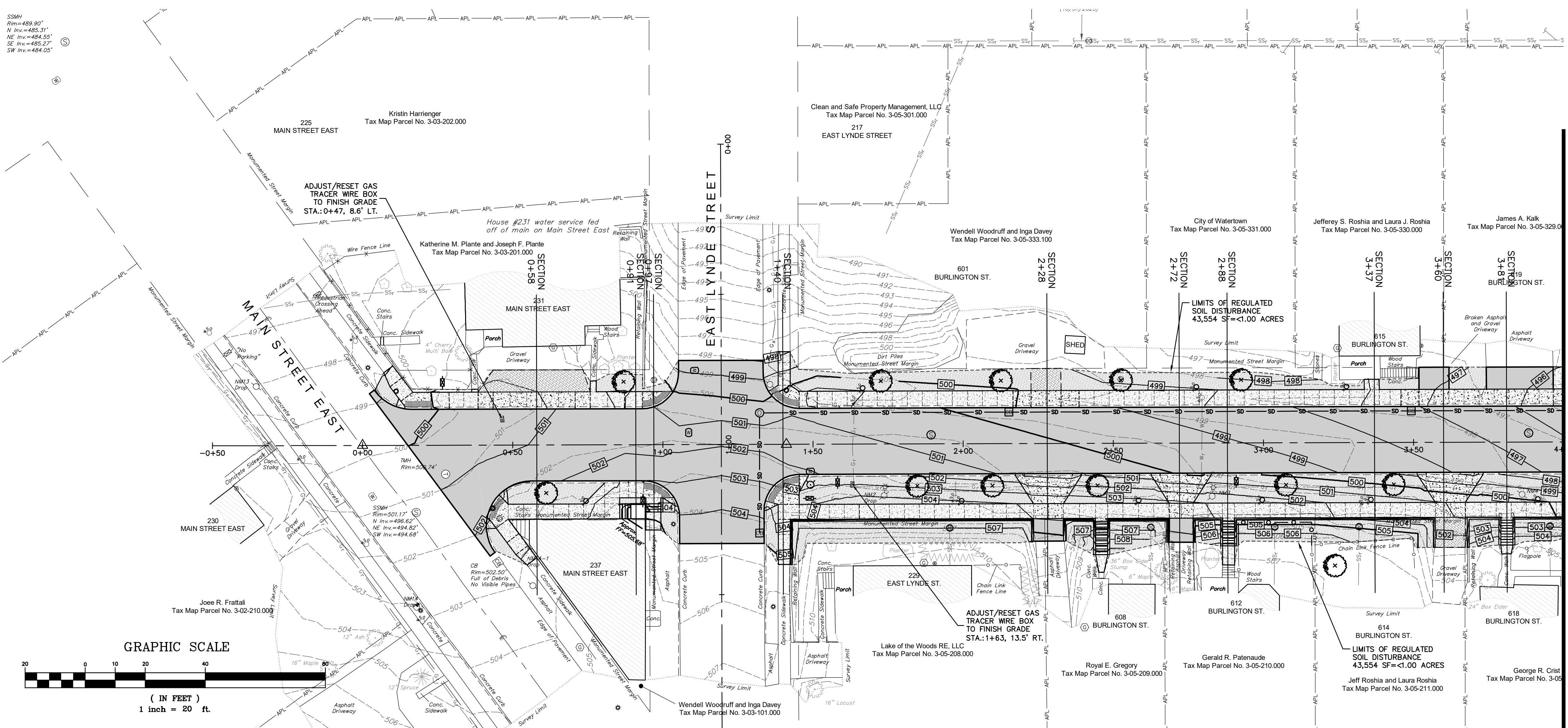
BURLINGTON STREET RECONSTRUCTION PROJECT
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=20'
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/30/2024
12/13/2024

TREE PROTECTION AND PLANTING PLAN

CS-106

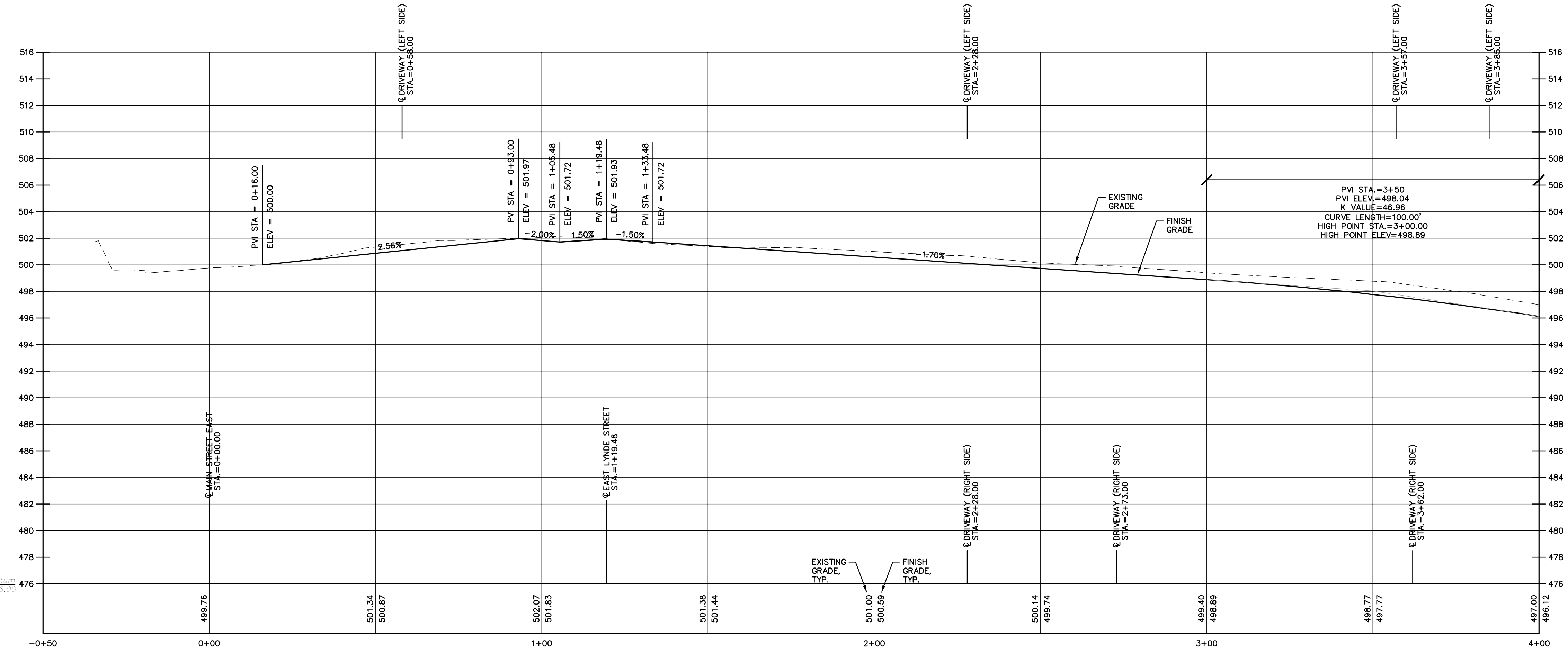
SSM
 Rim=489.20'
 N. Inv.=485.31'
 NE Inv.=484.55'
 SE Inv.=485.27'
 SW Inv.=484.05'



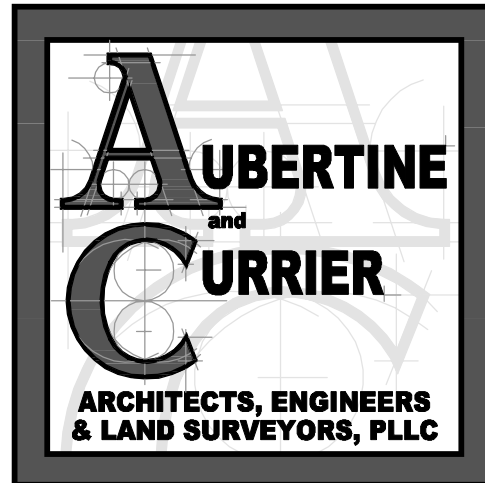
- ASPHALT PAVEMENT
- CONCRETE WALK (4" THICK)
- CONCRETE PAVEMENT DRIVEWAY/APRON/CURB RAMP (6" THICK)
- CRUSHED STONE PAVEMENT DRIVEWAYS
- LAWN AREA
- DETECTABLE WARNING SURFACE
- CONCRETE STEPS AND SIDEWALLS
- CONCRETE RETAINING WALL

MATCH LINE - REFER TO SHEET CG-102

- REFER TO ENLARGED GRADING PLANS ON CG-400 SERIES DRAWINGS FOR ADDITIONAL GRADING AND SPOT ELEVATIONS.
 - REFER TO STREET CROSS SECTIONS ON SHEETS CG-200 TO CG-204.



CENTERLINE PROFILE - BURLINGTON STREET (STA.0+00 TO STA.4+00)
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

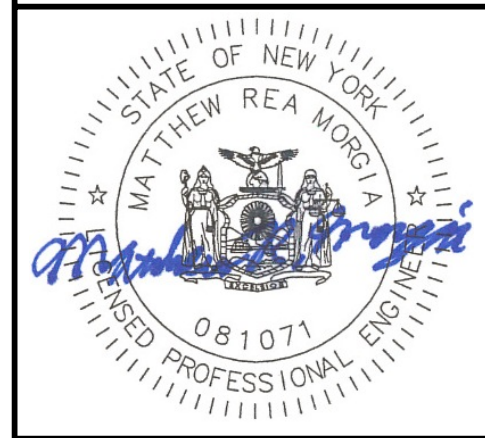


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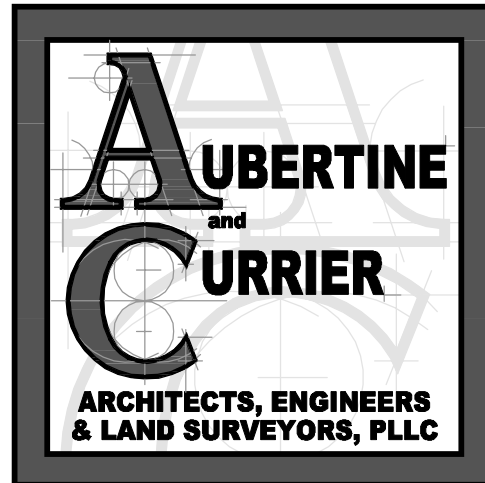


BURLINGTON STREET RECONSTRUCTION PROJECT
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
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DRAWN BY:	TFT
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ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

GRADING PLAN AND CENTERLINE PROFILE

CG-101

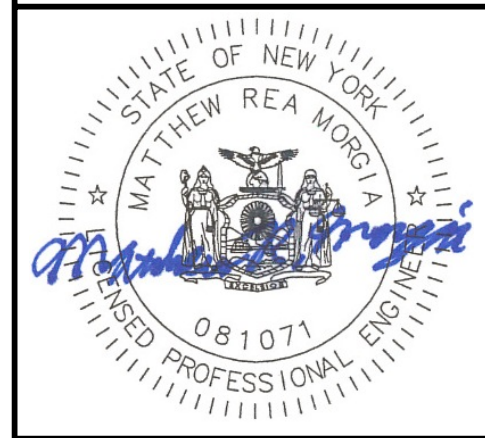


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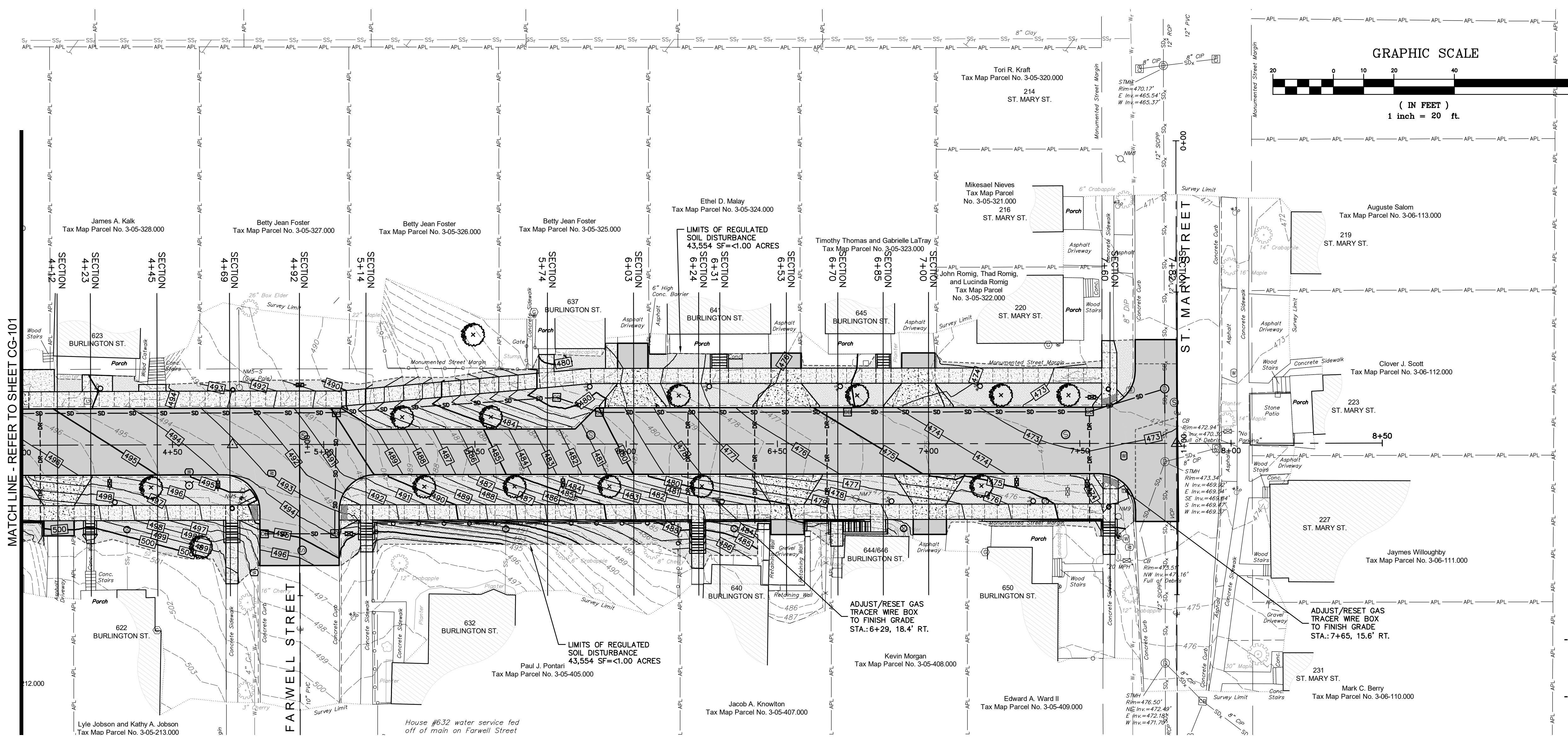
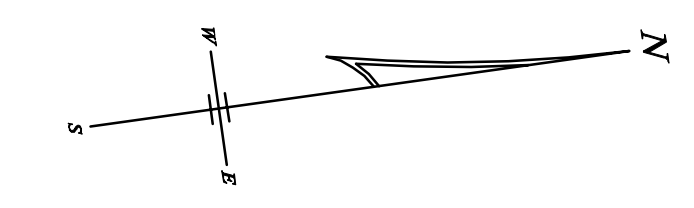
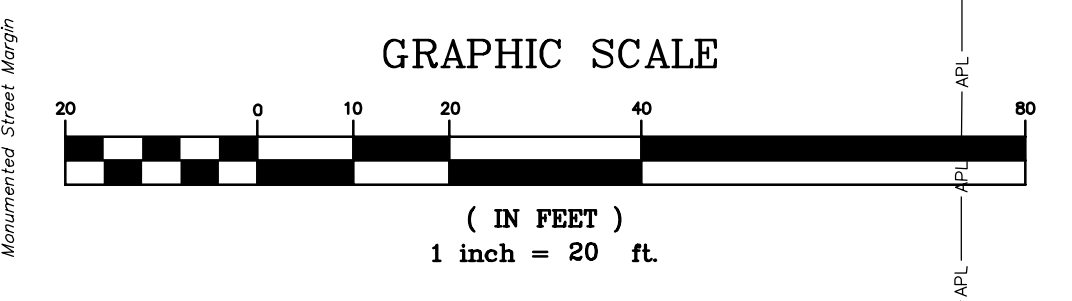


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
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ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

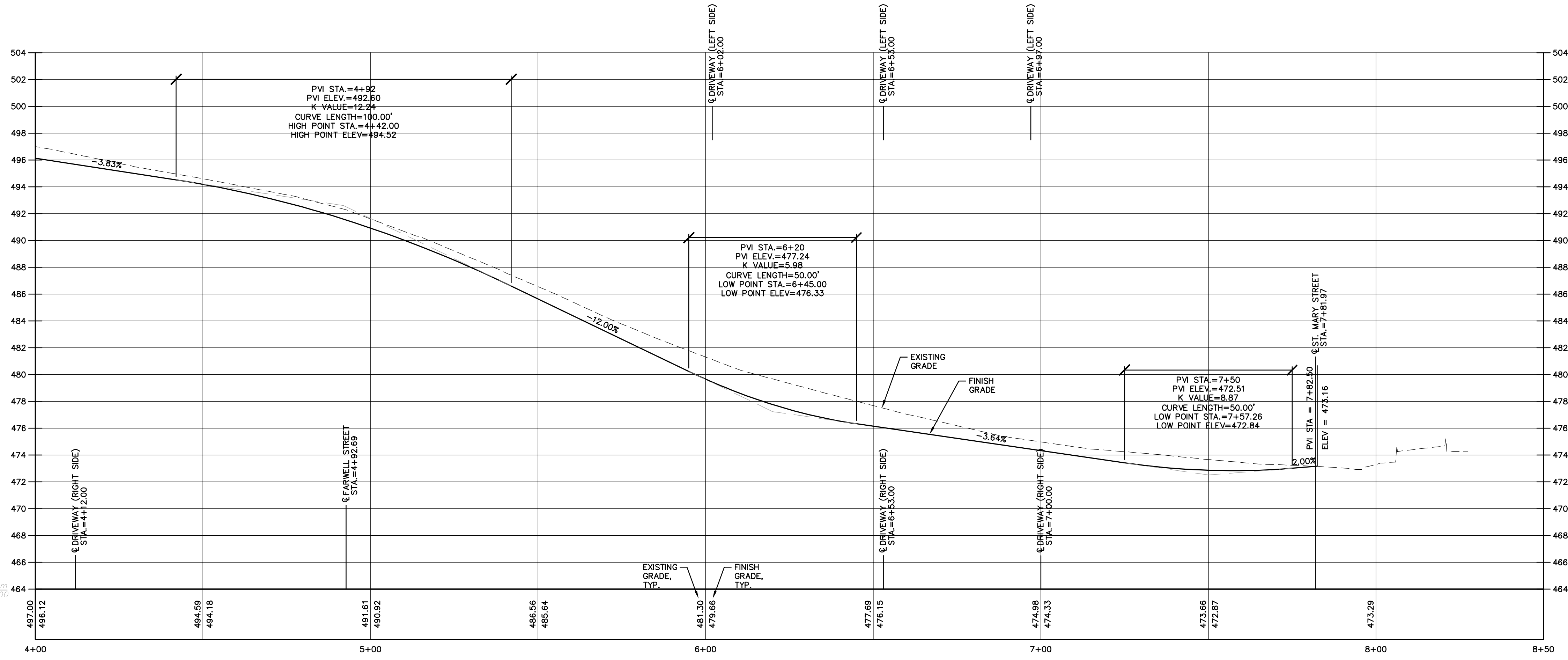
GRADING PLAN AND
CENTERLINE PROFILE

CG-102

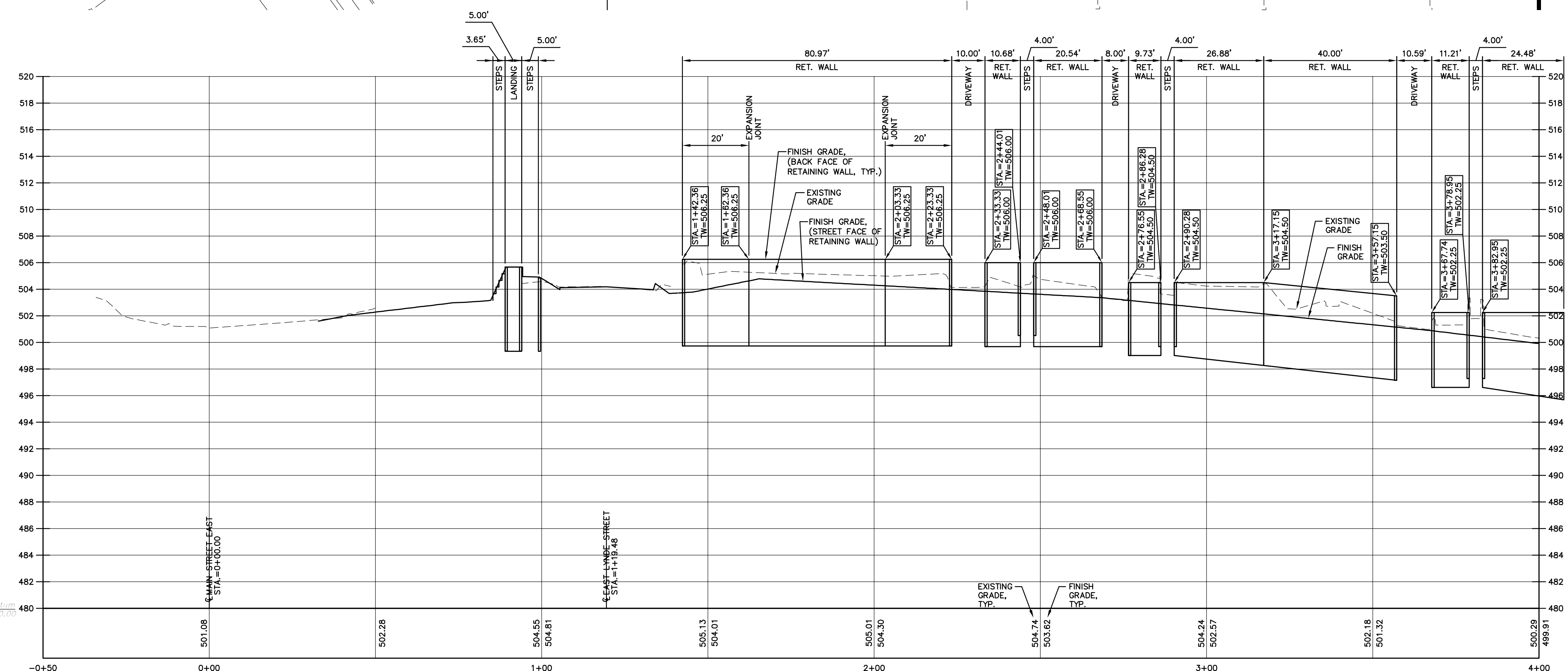
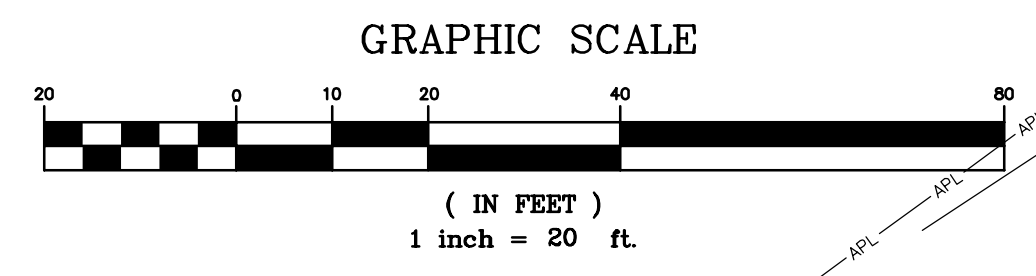
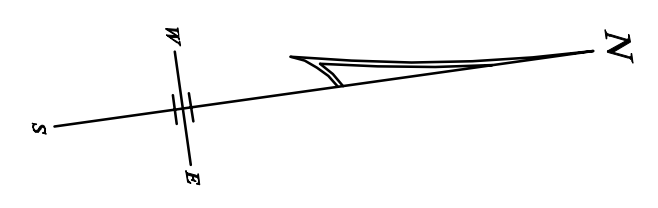
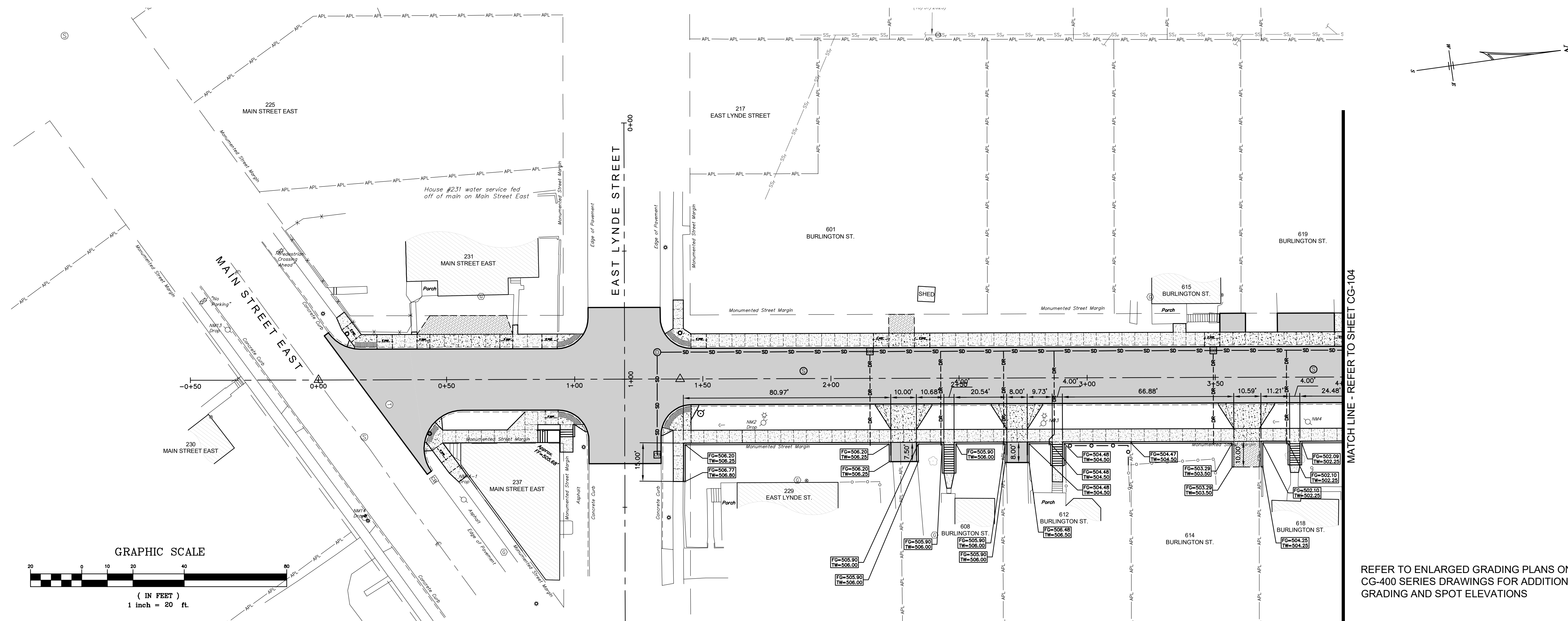


- ASPHALT PAVEMENT
- CONCRETE WALK (4" THICK)
- CONCRETE PAVEMENT DRIVEWAY/APRON/CURB RAMP (6" THICK)
- CRUSHED STONE PAVEMENT DRIVEWAYS
- LAWN AREA
- DETECTABLE WARNING SURFACE
- CONCRETE STEPS AND SIDEWALLS
- CONCRETE RETAINING WALL

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- REFER TO STREET CROSS SECTIONS ON SHEETS CG-200 TO CG-204.



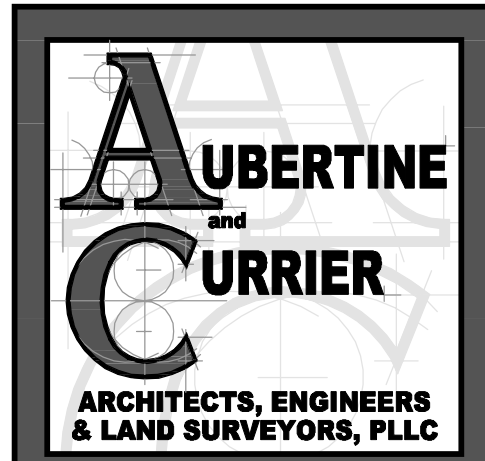
CENTERLINE PROFILE - BURLINGTON STREET (STA. 4+00 TO STA. 8+00)
SCALE: HORIZONTAL 1" = 20'
VERTICAL 1" = 5'



RETAINING WALL PROFILE - (STA.0+00 TO STA.4+00 OFFSET ALONG EAST STREET MARGIN OF BURLINGTON STREET)
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'

MATCH LINE - REFER TO SHEET CG-104

REFER TO ENLARGED GRADING PLANS ON CG-400 SERIES DRAWINGS FOR ADDITIONAL GRADING AND SPOT ELEVATIONS

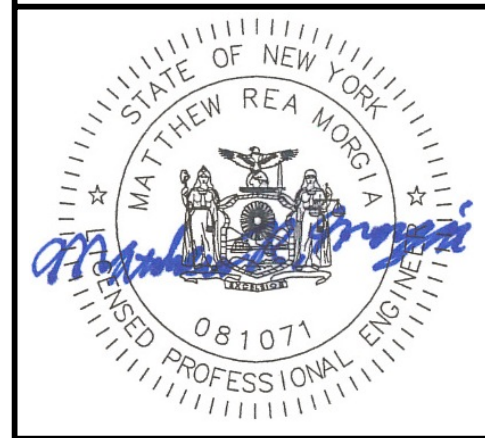


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**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

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ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

RETAINING WALL
 PLAN AND PROFILE

CG-103

SSM
 Rm=429.20'
 N Inv.=485.31'
 NE Inv.=484.55'
 SE Inv.=485.27'
 SW Inv.=484.05'

City Monument

City Monument

City Monument

City Monument

City Monument

City Monument

City Monument

City Monument

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City Monument

City Monument

City Monument

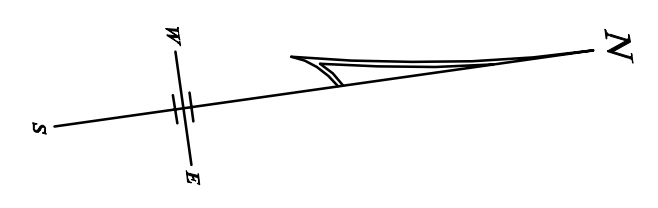
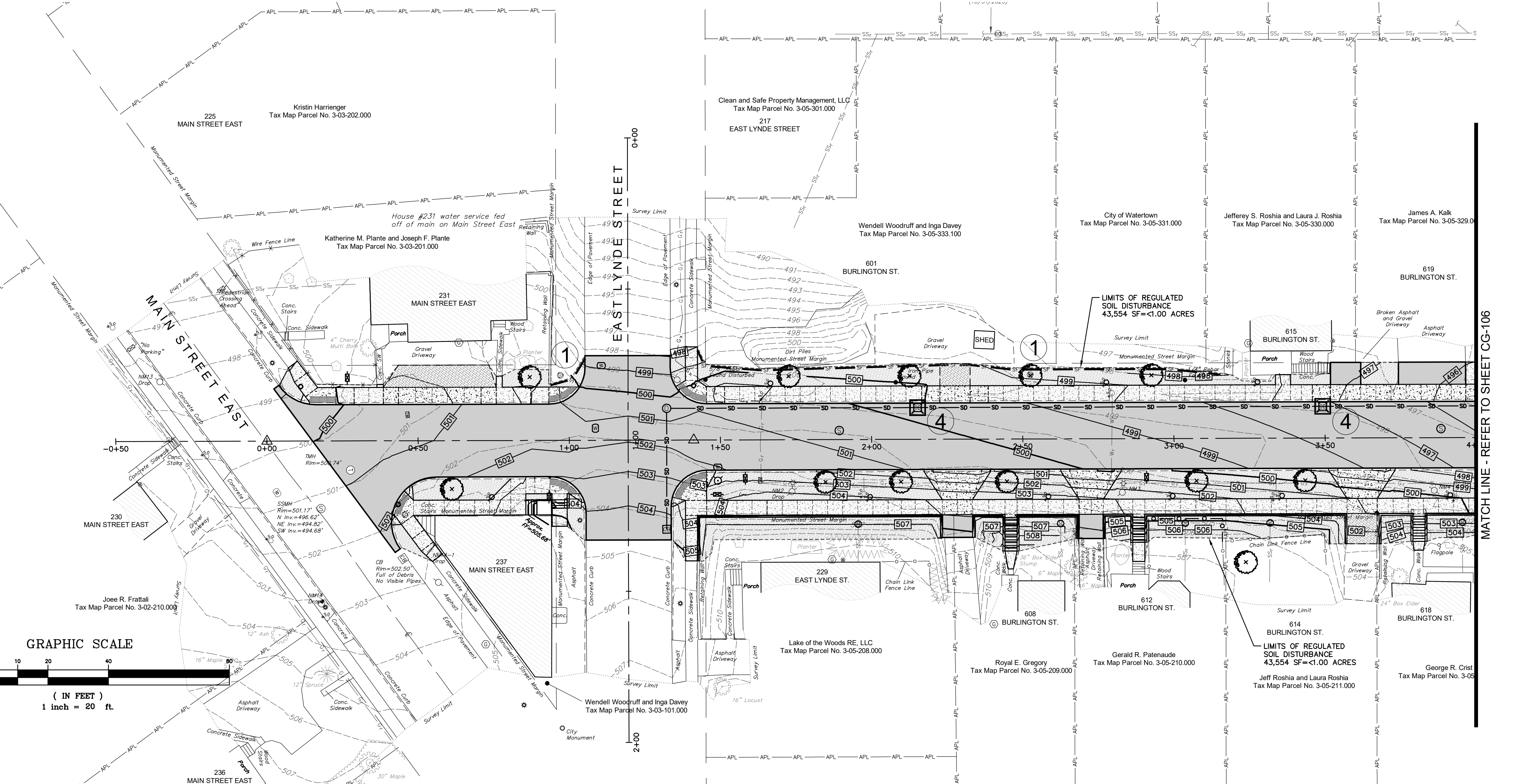
City Monument

City Monument

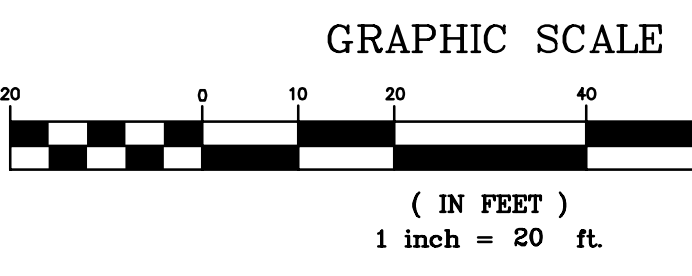
City Monument

City Monument

City Monument



- ASPHALT PAVEMENT
- CONCRETE WALK (4" THICK)
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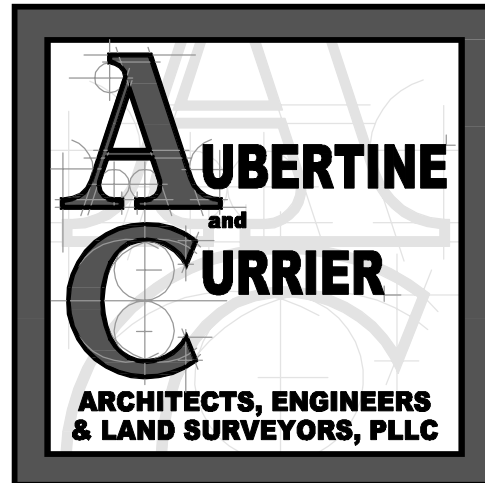
TEMPORARY MEASURES:

1. INSTALL SILT FENCE IN LOCATIONS INDICATED AND WHERE THERE IS THE POTENTIAL FOR OFFSITE RUNOFF TO OCCUR PRIOR TO ANY CONSTRUCTION WITHIN THOSE AREAS. FENCE MUST BE MAINTAINED AND MUST REMAIN IN PLACE UNTIL PROJECT HAS BEEN FINAL GRADED AND VEGETATION HAS BEEN ESTABLISHED.
2. CONSTRUCTION ENTRANCES SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OF SEDIMENT ONTO PUBLIC RIGHT OF WAY OR STREETS. ALL SEDIMENT SPILLED, DROPPED OR WASHED ONTO PUBLIC RIGHTS OF WAY MUST BE REMOVED IMMEDIATELY.
3. CHECK DAMS SHALL BE INSTALLED IN DRAINAGE CHANNELS AS NECESSARY TO REDUCE EROSION WITHIN THE CHANNELS AND THE TRANSPORTATION OF SEDIMENT DOWNSTREAM.
4. INLET PROTECTION SHALL BE INSTALLED AROUND CATCH BASINS FOLLOWING THEIR INSTALLATION.



EROSION AND SEDIMENT CONTROL NOTES:

1. PRIOR TO COMMENCING ANY CLEARING GRUBBING, EARTHWORK ACTIVITIES, ETC. AT THE SITE, THE CONTRACTOR SHALL FLAG THE WORK LIMITS AND SHALL INSTALL ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES (I.E. SILT FENCES, TREE PROTECTION/BARRIER FENCES, STABILIZED CONSTRUCTION ENTRANCES, STORM DRAIN SEDIMENT FILTERS, DRAINAGE DITCH SEDIMENT FILTERS, ETC.) INDICATED ON THE PROJECT DRAWINGS. TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THEIR TRIBUTARY AREAS. ONCE CONSTRUCTED, ALL MEASURES SHALL BE PROPERLY MAINTAINED AND/OR REPLACED AS NECESSARY AND THEN REMOVED FROM THE SITE ONCE VEGETATION AND PAVEMENT ARE IN PLACE.
2. EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WHERE TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED. ONCE ALL MEASURES ARE INSTALLED TO THE SATISFACTION OF THE ENGINEER, THE REMAINDER OF THE CLEARING AND GRADING ACTIVITIES SHALL COMMENCE.
3. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN STRICT COMPLIANCE WITH THE "NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL" CURRENT EDITION.
4. THE CONTRACTOR SHALL COMPLY WITH THE PROVISIONS OF ALL ENVIRONMENTAL PERMITS ISSUED FOR THIS PROJECT. THESE PLANS REFLECT THE PROVISIONS AND REQUIREMENTS OF SAID PERMIT(S). PERMIT(S) WILL BE AVAILABLE FROM THE ENGINEER-IN-CHARGE PRIOR TO THE START OF CONSTRUCTION.
5. CONSTRUCTION IS TO PROCEED IN ACCORDANCE WITH THE CONSTRUCTION PHASING SCHEDULED SUPPLIED BY THE CONTRACTOR OR SHOWN ON THE PLANS. ALL ELEMENTS OF THE SCHEDULE SHALL BE COMPLETED PRIOR TO BEGINNING THE NEXT CONSTRUCTION PHASE. THESE ELEMENTS INCLUDE ALL UTILITY CONSTRUCTION, THE BASE COURSE OF ASPHALT PAVING, AND ESTABLISHING GRASSES ON ALL DISTURBED AREAS. FOR TIME FRAMES OUTSIDE THE GROWING SEASON, OTHER METHODS OF SOIL STABILIZATION (SUCH AS THE USE OF JUTE MESH) SHALL BE USED UNTIL SUCH TIME AS GRASSES CAN BE ESTABLISHED.
6. THE CONTRACTOR SHALL INSPECT AND MAINTAIN THE INTEGRITY AND FUNCTION OF ALL TEMPORARY EROSION CONTROL MEASURES THROUGHOUT THE DURATION OF THE DEVELOPMENT PROCESS. TO ASSURE PROPER FUNCTION, SILTATION BARRIERS SHALL BE MAINTAINED IN GOOD CONDITION AND REINFORCED, EXTENDED, REPAIRED OR REPLACED AS NECESSARY. WASHOUTS SHALL BE IMMEDIATELY REPAIRED, RE-SEEDED AND PROTECTED FROM FURTHER EROSION. ALL ACCUMULATED SEDIMENT SHALL BE REMOVED AND CONTAINED IN APPROPRIATE SPOIL AREAS. WATER SHALL BE APPLIED TO NEWLY SEEDDED AREAS AS NEEDED UNTIL GRASS COVER IS WELL ESTABLISHED TO EFFECTIVELY CONTROL WIND EROSION. WATER SHALL BE APPLIED TO ALL EXPOSED SOILS AS NECESSARY UNTIL GRASS COVER IS PERMANENTLY ESTABLISHED.
7. THE STABILIZED CONSTRUCTION ENTRANCE, UTILIZED DURING CONSTRUCTION, SHALL BE MAINTAINED IN A CONDITION THAT SHALL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND/OR REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHTS-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. PERIODIC INSPECTIONS AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
8. IMMEDIATELY FOLLOWING COMPLETION OF ANY AND ALL STORM DRAIN INLETS, STORM DRAIN INLET PROTECTION SHALL BE CONSTRUCTED. THE INLET PROTECTION SHALL FUNCTION TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAINS. THEY SHALL BE MAINTAINED IN GOOD CONDITION UNTIL FINAL VEGETATIVE COVER IS WELL ESTABLISHED.
9. AS MUCH AS IS PRACTICAL, EXISTING VEGETATION SHALL BE PRESERVED. FOLLOWING THE COMPLETION OF CONSTRUCTION ACTIVITIES IN ANY PORTION OF THE SITE, PERMANENT VEGETATION SHALL BE ESTABLISHED ON ALL EXPOSED SOILS.
10. IN SOME INSTANCES, ESTABLISHING VEGETATION WILL BE NECESSARILY DELAYED WHILE CONSTRUCTION IS IN PROGRESS. DURING THESE TIMES, SEDIMENT CONTROL MEASURES WILL BE EMPLOYED TO PREVENT SEDIMENT FROM LEAVING THE SITE. VEGETATION SHALL BE ESTABLISHED IN THESE AREAS AS SOON AS IT IS PRACTICAL.
11. SITE PREPARATION ACTIVITIES SHALL BE PLANNED TO MINIMIZE THE SCOPE AND DURATION OF SOIL DISRUPTION.
12. PERMANENT TRAFFIC CORRIDORS SHALL BE ESTABLISHED AND "ROUTES OF CONVENIENCE" SHALL BE AVOIDED. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT ALL POINTS OF ENTRY ONTO THE PROJECT SITE.
13. AREAS UNDERGOING CLEARING OR GRADING AND WHERE WORK IS DELAYED OR COMPLETED AND WILL NOT BE REDISTURBED FOR 21 DAYS OR MORE SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT VEGETATIVE COVER WITHIN 14 DAYS.
14. TOPSOIL AND FILL THAT IS TO REMAIN STOCKPILED ON-SITE FOR PERIODS GREATER THAN 30 DAYS SHALL BE STABILIZED BY SEEDING. PRIOR TO THE SEEDING OPERATION, THE STOCKPILED MATERIAL SHALL BE GRADED AS NEEDED AND FEASIBLE TO PERMIT THE USE OF CONVENTIONAL EQUIPMENT FOR SEEDBED PREPARATION, FERTILIZATION, SEEDING, MULCH APPLICATIONS AND MULCH ANCHORING.
15. SILT FENCES SHALL BE CONSTRUCTED AROUND ALL STOCKPILES OF FILL, TOPSOIL, AND EXCAVATED OVERBURDEN. SILT FENCES SHALL BE ANCHORED AND MAINTAINED IN GOOD CONDITION UNTIL SUCH TIME AS SAID STOCKPILES ARE REMOVED AND STOCKPILING AREAS ARE BROUGHT TO FINAL GRADE AND PERMANENTLY STABILIZED.
16. IN NO CASE SHALL ERODIBLE MATERIALS BE STOCKPILED WITHIN 25 FEET OF ANY DITCH STREAM OR OTHER SURFACE WATER BODY.
17. DAMAGE TO SURFACE WATERS RESULTING FROM EROSION AND SEDIMENTATION SHALL BE MINIMIZED BY STABILIZING DISTURBED AREAS AND BY REMOVING SEDIMENT FROM CONSTRUCTION SITE DISCHARGES.
18. CONSTRUCTION TRAFFIC SHALL NOT CROSS STREAMS OR DITCHES EXCEPT AT SUITABLE CROSSING FACILITIES, AND SHALL NOT OPERATE UNNECESSARILY WITHIN WATERWAYS OR DRAINAGE DITCHES.
19. NO SYNTHETIC EROSION CONTROL MATERIAL, FENCING OR MATTING SHALL BE PART OF THE PERMANENT INSTALLATION.
20. WHERE CONCENTRATED FLOWS ARE CREATED AS A RESULT OF CONSTRUCTION OPERATIONS, CHECK DAMS SHALL BE INSTALLED DEEMED NECESSARY.

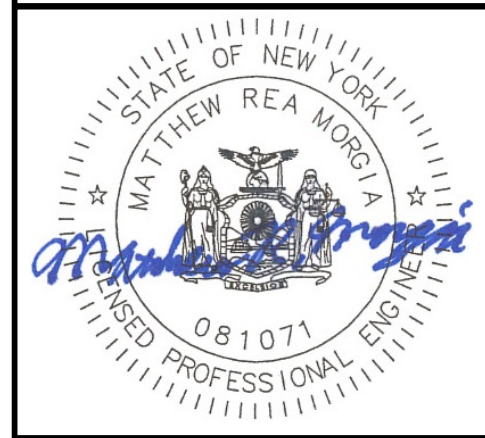


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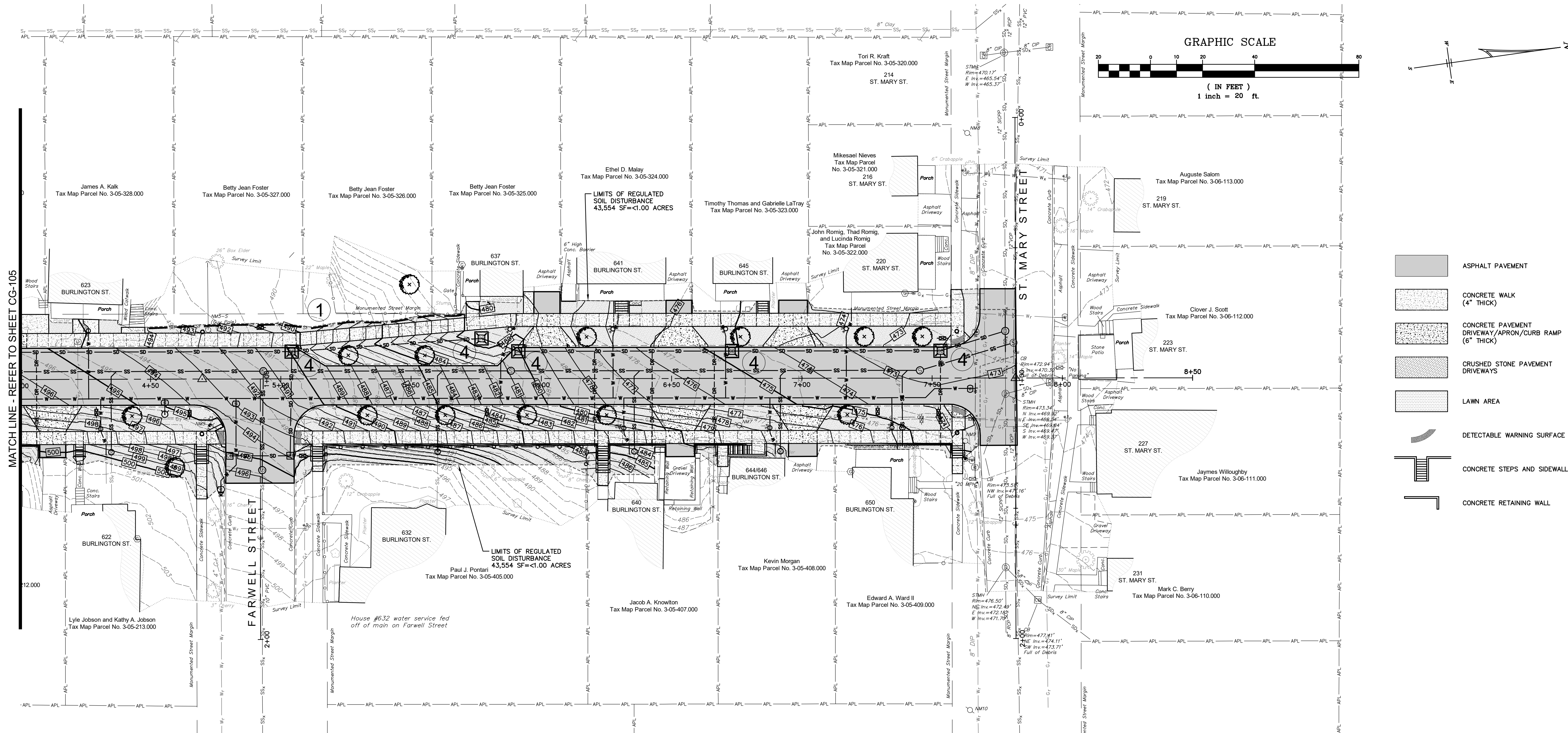


**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

EROSION AND SEDIMENT CONTROL PLAN

CG-105

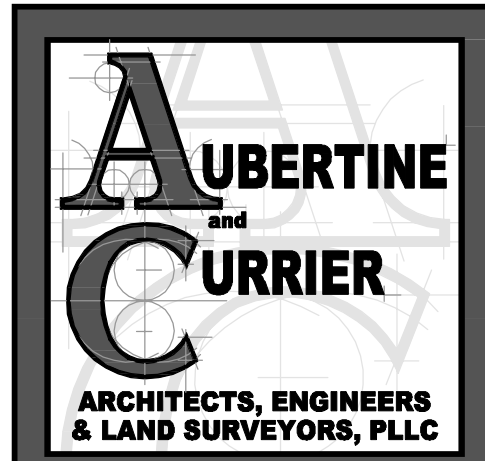
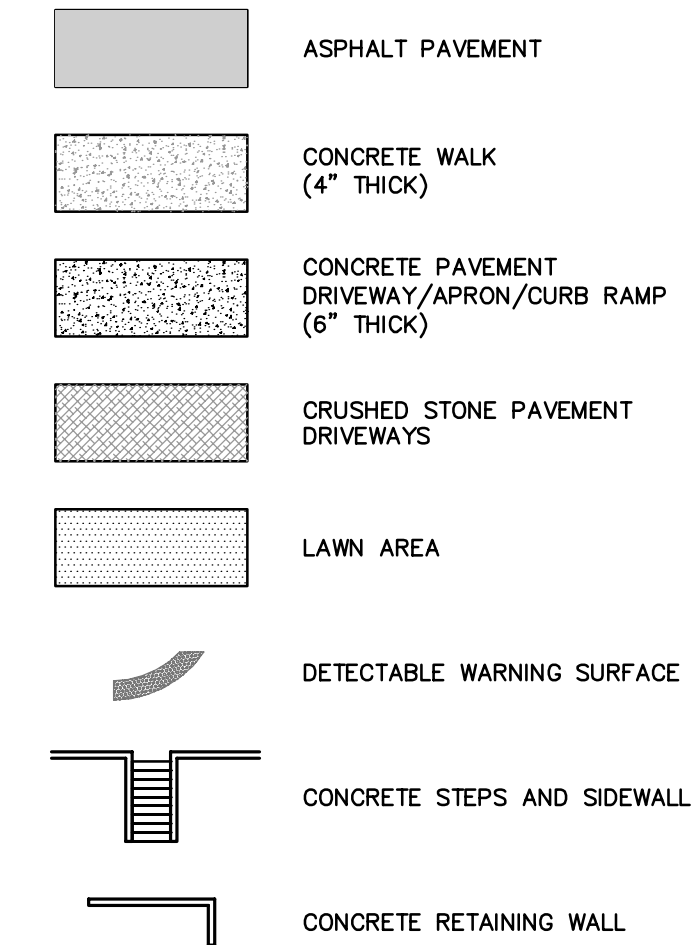


MATCH LINE - REFER TO SHEET CG-105

12,000

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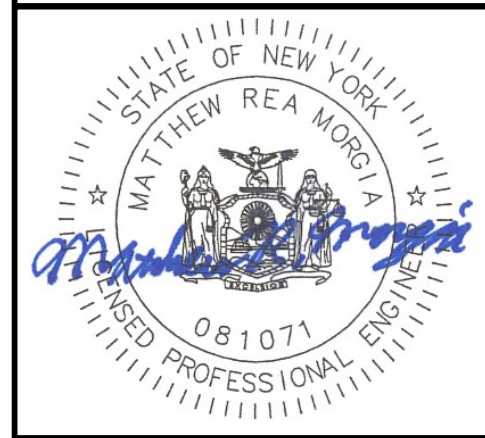


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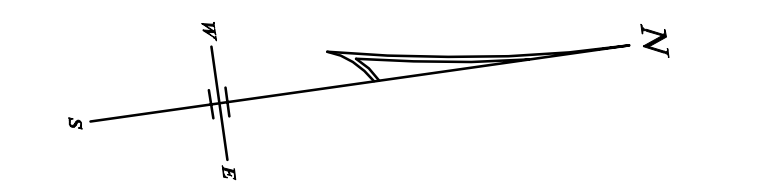
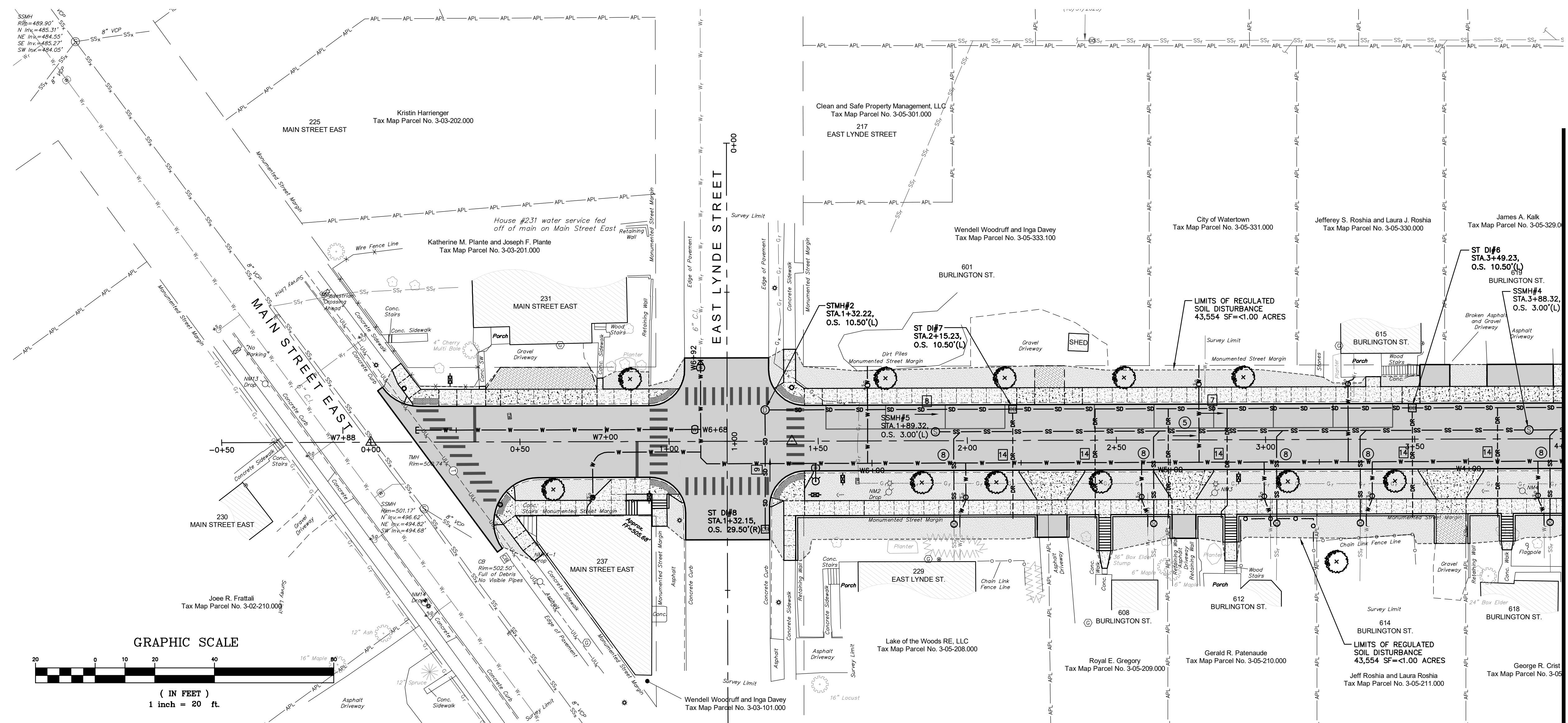


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

EROSION AND SEDIMENT CONTROL PLAN

CG-106

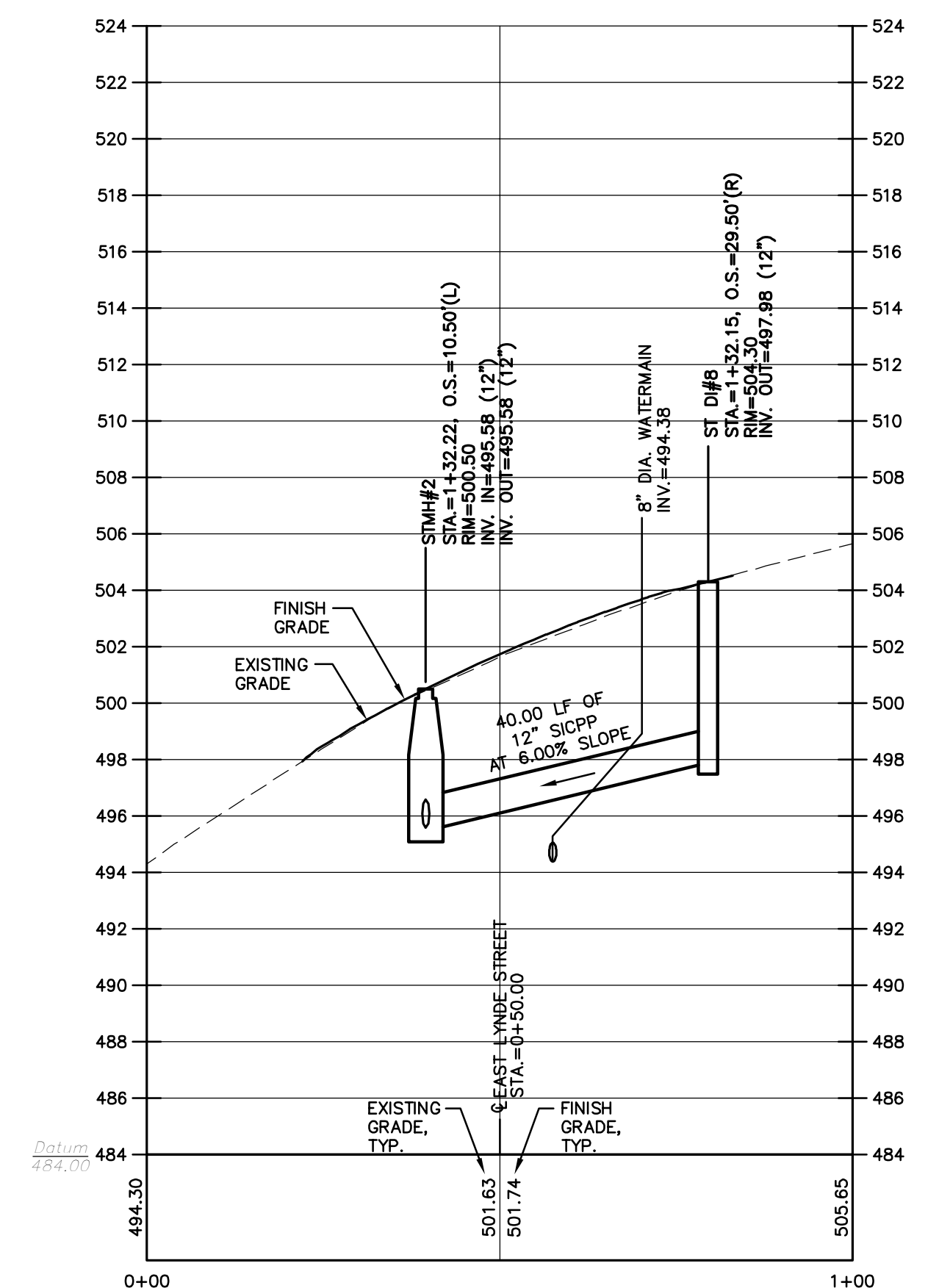
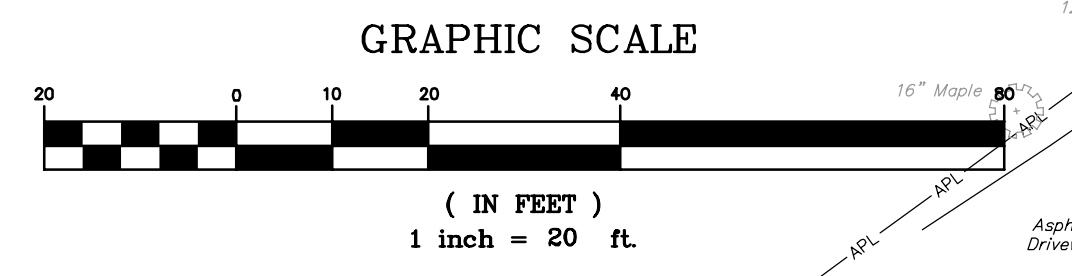


SANITARY SEWER STRUCTURE TABLE		
STRUCTURE NAME	STATION AND OFFSET	RIMS AND INVERTS
Existing SSMH	STA.=7+81.25, OFFSET 13.54', L	RIM=472.70 INV IN=465.37
SSMH#1	STA.=7+45.31, OFFSET 3.00', L	RIM=472.74 INV IN=465.66 INV OUT=465.56
SSMH#2	STA.=5+88.31, OFFSET 3.00', L	RIM=480.86 INV IN=474.72 INV OUT=472.72
SSMH#3	STA.=4+93.31, OFFSET 3.00', L	RIM=491.29 INV IN=484.32 INV OUT=484.22
SSMH#4	STA.=3+88.32, OFFSET 3.00', L	RIM=496.38 INV IN=490.80 INV OUT=490.62
SSMH#5	STA.=1+89.32, OFFSET 3.00', L	RIM=500.59 INV IN=494.38
SSMH#6	STA.=4+92.83, OFFSET 35.00', R	RIM=495.58 INV IN=486.51 INV OUT=486.41

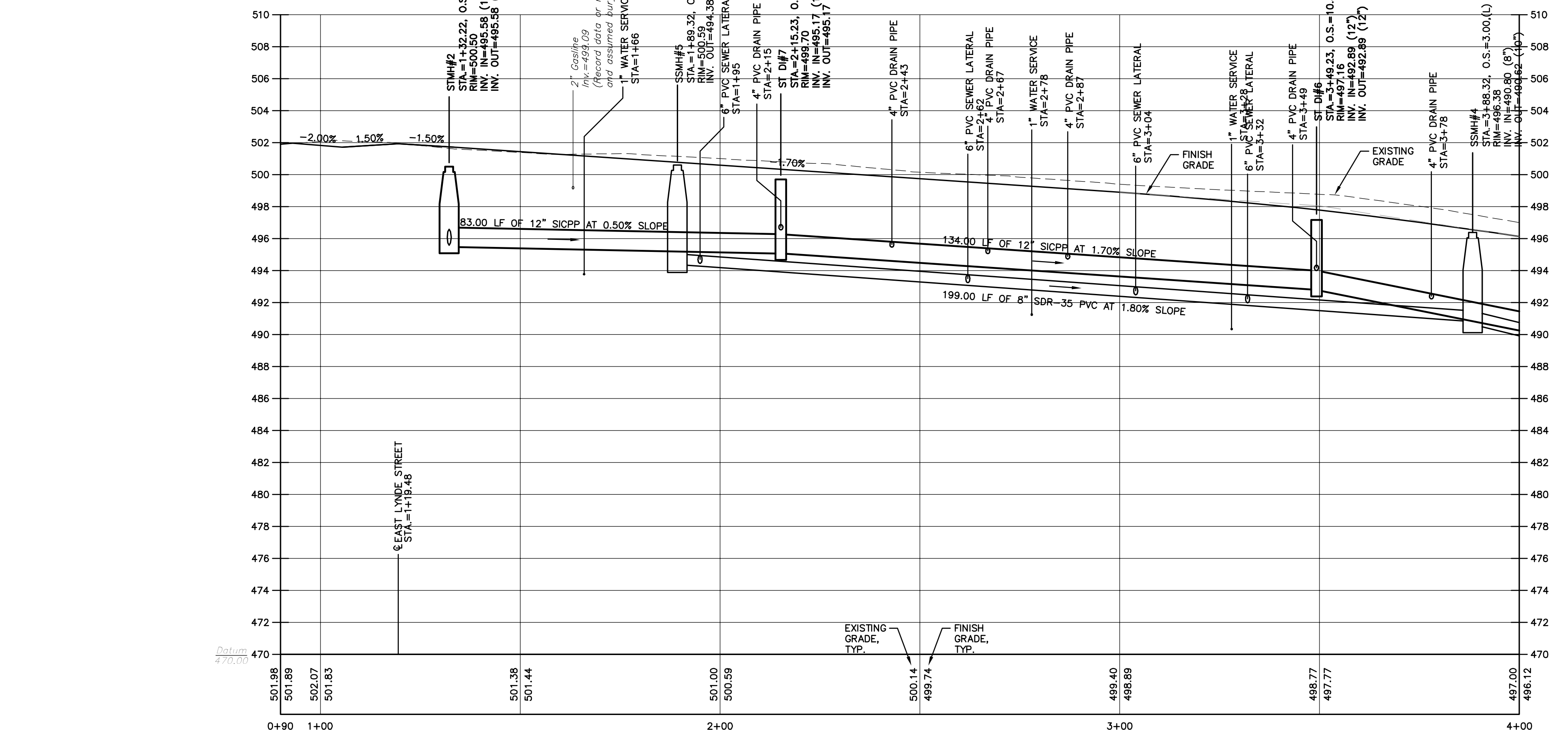
SANITARY SEWER PIPE TABLE			
PIPE TAG	SIZE	LENGTH	SLOPE
1	10"	37.45'	0.50%
2	10"	157.00'	4.50%
3	10"	95.00'	10.00%
4	10"	105.00'	6.00%
5	8"	199.00'	1.80%
6	10"	38.00'	5.50%
7	10"	5.00'	5.50%
8	6" SDR-35 PVC SANITARY SEWER LATERAL PIPE AT MIN. 2.00% SLOPE. (CONFIRM SIZE, TYPE, LOCATION AND ELEVATION OF EXISTING LATERAL. INSTALL CLEANOUT AND RECONNECT.)		

STORM SEWER STRUCTURE TABLE		
STRUCTURE NAME	STATION AND OFFSET	FRAMES AND INVERTS
ST DI#1	STA.=7+53.21, OFFSET -10.50, L	FRAME=472.25 INV IN=468.90' INV OUT=468.90
ST DI#2	STA.=6+73.21, OFFSET -10.50, L	FRAME=474.68 INV IN=470.50' INV OUT=470.50
ST DI#3	STA.=5+91.20, OFFSET -10.50, L	FRAME=480.07 INV IN=474.19' INV OUT=474.19
ST DI#4	STA.=5+77.05, OFFSET -10.50, L	FRAME=479.25 INV IN=474.79' INV OUT=474.79
ST DI#5	STA.=5+04.21, OFFSET -10.50, L	FRAME=489.92 INV IN=485.14' INV OUT=485.14
ST DI#6	STA.=3+49.23, OFFSET -10.50, L	FRAME=497.16 INV IN=492.89' INV OUT=492.89
ST DI#7	STA.=2+15.23, OFFSET -10.50, L	FRAME=499.70 INV IN=495.17' INV OUT=495.17
ST DI#8	STA.=1+32.15, OFFSET 29.50, R	FRAME=504.30 INV IN=497.98
ST DI#9	STA.=5+03.57, OFFSET 29.49, R	FRAME=493.90 INV IN=488.34' INV OUT=488.34
ST DI#10	STA.=4+80.37, OFFSET 29.49, R	FRAME=495.16 INV IN=489.04
STMH#1 (CUT-IN)	STA.=7+78.08, OFFSET -18.08, L	FRAME=472.55 INV IN=468.67' INV OUT=468.66
STMH#2	STA.=1+32.22, OFFSET -10.50, L	FRAME=500.50 INV IN=495.58' INV OUT=495.58

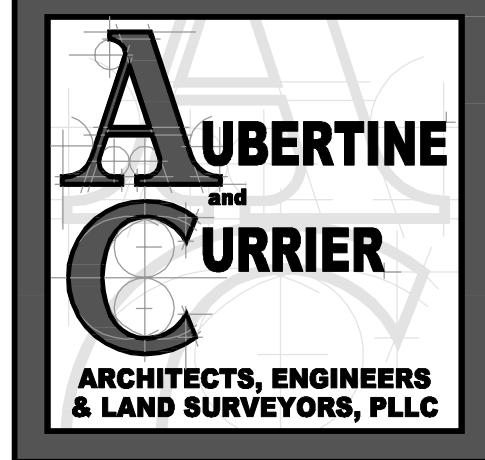
STORM SEWER PIPE TABLE			
PIPE TAG	SIZE	LENGTH	SLOPE
1	12"	26.00'	0.50%
2	12"	80.00'	2.00%
3	12"	82.00'	4.50%
4	12"	15.00'	4.00%
5	12"	73.01'	14.18%
6	12"	155.00'	5.00%
7	12"	134.00'	1.70%
8	12"	83.00'	0.50%
9	12"	40.00'	6.00%
10	12"	40.00'	8.00%
11	12"	23.20'	3.00%
12	12"	8.00'	2.95%
13	12"	8.00'	2.95%
14	4" SDR-35 PVC SOLID STORM DRAIN PIPE AT MIN. 1.00% SLOPE. (PROVIDE BOOTED CONNECTION TO CITY SINGLE CATCH BASIN OR DIRECT CONNECTION TO SICPP STORM SEWER PIPE WITH AN INSERT-A-TEE FITTING INTO THE STORM SEWER SYSTEM. CONNECT SOLID PIPE TO PERFORATED UNDERDRAIN PIPE ON THE BACK SIDE OF THE RETAINING WALL.)		



STORM SEWER PROFILE - EAST LYNDE STREET
SCALE: HORIZONTAL 1" = 20'
VERTICAL 1" = 5'



BURLINGTON STREET (STA.0+90 TO STA.4+00) - SANITARY AND STORM SEWER PROFILE
SCALE: HORIZONTAL 1" = 20'
VERTICAL 1" = 5'

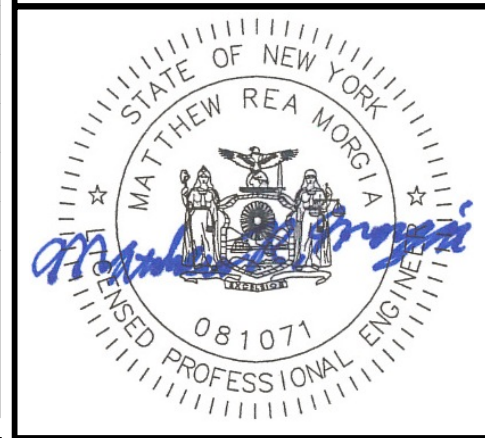


NYS WBE/DBE Certified
SBA Woman Owned
Small Business (WOSB)

aubertinecurrier.com

522 Bradley Street
Watertown, New York 13601
Phone: (315)782-2005

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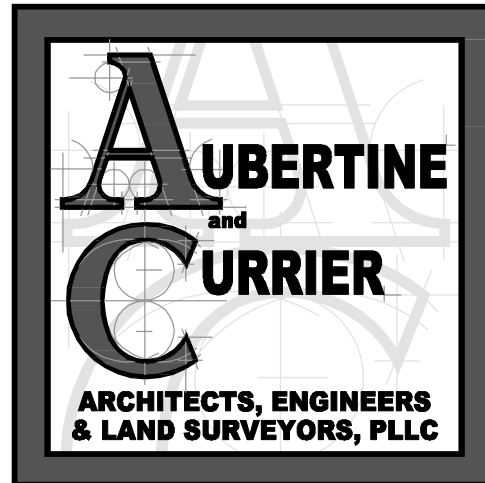
**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=20'
DRAWN BY: TFT
CHECKED BY: MRM

ISSUE DATES:
02/23/2024
04/30/2024
12/13/2024

UTILITY PLAN AND PROFILE
SANITARY AND STORM SEWER

CU-101



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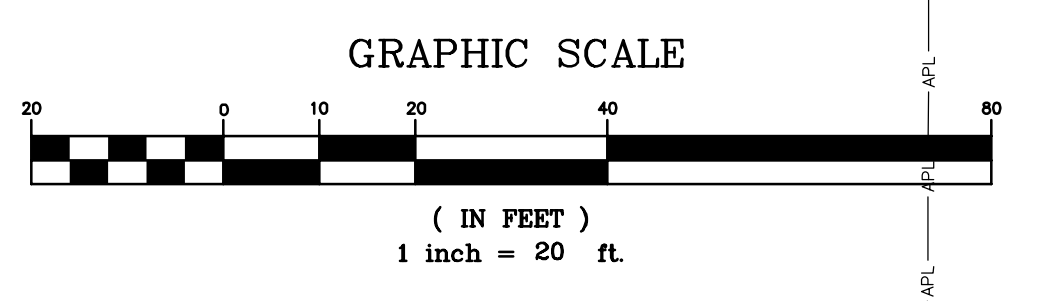
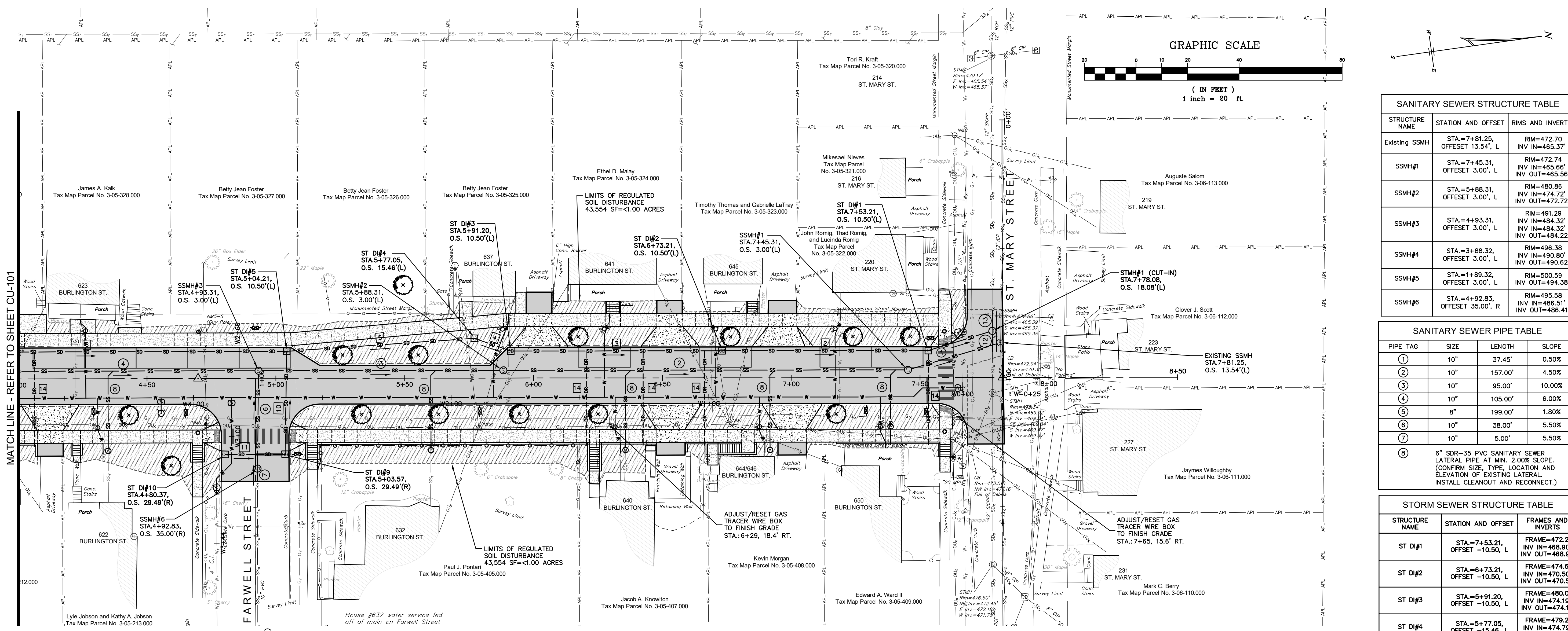
BURLINGTON STREET RECONSTRUCTION PROJECT
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=20'
DRAWN BY: TTF
CHECKED BY: MRM

ISSUE DATES:
02/29/2024
04/03/2024
12/19/2024

UTILITY PLAN AND PROFILE
SANITARY AND STORM SEWER

CU-102



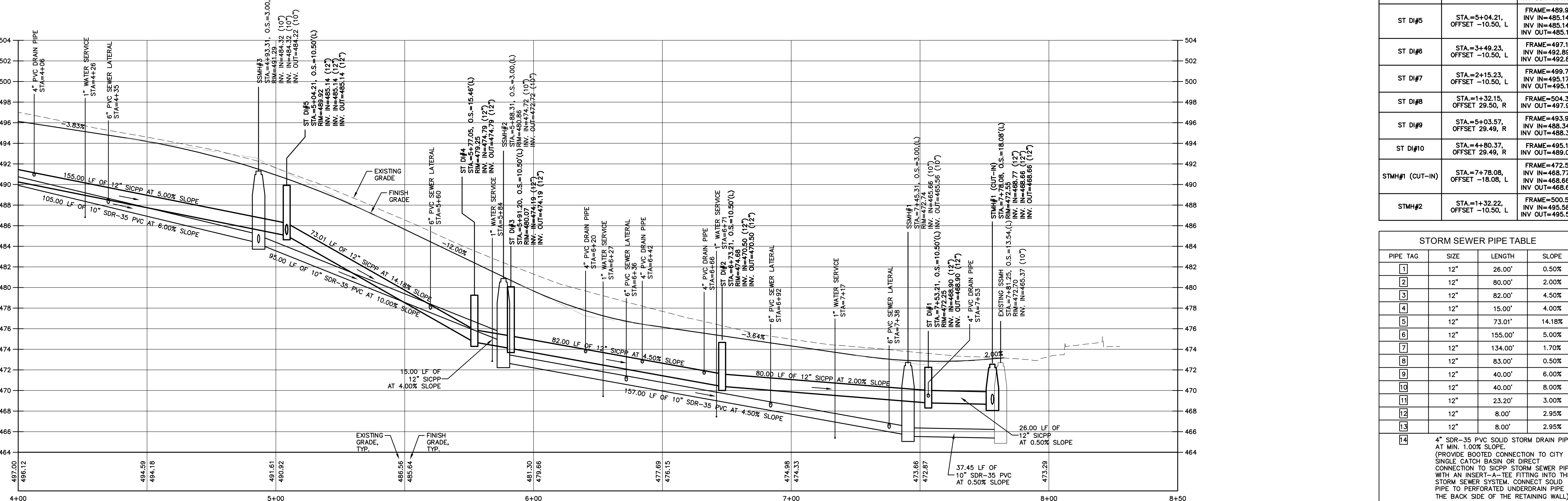
SANITARY SEWER STRUCTURE TABLE		
STRUCTURE NAME	STATION AND OFFSET	RIMS AND INVERTS
Existing SSMH	STA.=7+81.25, OFFSET 13.54', L	RIM=472.70, INV IN=465.37
SSMH#1	STA.=7+45.31, OFFSET 3.00', L	RIM=472.74, INV IN=465.56, INV OUT=465.56
SSMH#2	STA.=5+88.32, OFFSET 3.00', L	RIM=480.86, INV IN=474.72, INV OUT=472.72
SSMH#3	STA.=4+93.31, OFFSET 3.00', L	RIM=491.29, INV IN=484.32, INV OUT=484.22
SSMH#4	STA.=3+88.32, OFFSET 3.00', L	RIM=496.38, INV IN=490.80, INV OUT=490.62
SSMH#5	STA.=1+89.32, OFFSET 3.00', L	RIM=500.59, INV IN=494.38
SSMH#6	STA.=4+92.83, OFFSET 35.00', R	RIM=495.58, INV IN=486.51, INV OUT=486.41

SANITARY SEWER PIPE TABLE			
PIPE TAG	SIZE	LENGTH	SLOPE
1	10"	37.45'	0.50%
2	10"	157.00'	4.50%
3	10"	95.00'	10.00%
4	10"	105.00'	6.00%
5	8"	199.00'	1.80%
6	10"	38.00'	5.50%
7	10"	5.00'	5.50%

6" SDR-35 PVC SANITARY SEWER LATERAL PIPE AT MIN. 2.00% SLOPE. (CONFIRM SIZE, TYPE, LOCATION AND ELEVATION OF EXISTING LATERAL PIPE TO BE RECONNECTED AND RECONNECT.)

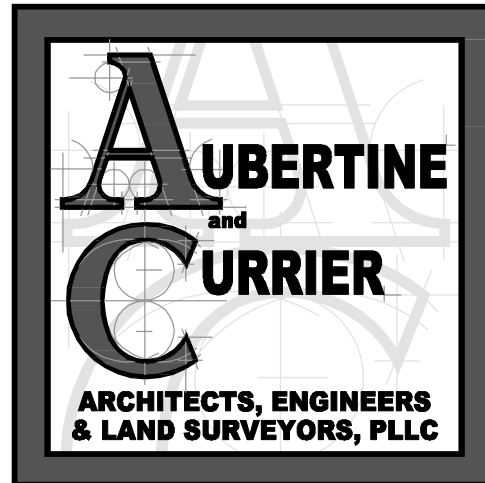
STORM SEWER STRUCTURE TABLE		
STRUCTURE NAME	STATION AND OFFSET	FRAMES AND INVERTS
ST DI#1	STA.=7+53.21, OFFSET -10.50, L	FRAME=472.25, INV IN=468.90, INV OUT=468.90
ST DI#2	STA.=6+73.21, OFFSET -10.50, L	FRAME=474.68, INV IN=470.50, INV OUT=470.50
ST DI#3	STA.=5+91.20, OFFSET -10.50, L	FRAME=480.07, INV IN=474.19, INV OUT=474.19
ST DI#4	STA.=5+77.05, OFFSET -15.46, L	FRAME=479.25, INV IN=474.79, INV OUT=474.79
ST DI#5	STA.=5+04.21, OFFSET -10.50, L	FRAME=489.92, INV IN=485.14, INV OUT=485.14
ST DI#6	STA.=3+49.23, OFFSET -10.50, L	FRAME=497.16, INV IN=492.89, INV OUT=492.89
ST DI#7	STA.=2+15.23, OFFSET -10.50, L	FRAME=499.70, INV IN=495.17, INV OUT=495.17
ST DI#8	STA.=1+32.15, OFFSET 29.50, R	FRAME=504.30, INV IN=497.98
ST DI#9	STA.=5+03.57, OFFSET 29.49, R	FRAME=493.90, INV IN=488.34, INV OUT=488.34
ST DI#10	STA.=4+80.37, OFFSET 29.49, R	FRAME=495.18, INV IN=489.04
STMH#1 (CUT-IN)	STA.=7+78.08, OFFSET -18.08, L	FRAME=472.55, INV IN=468.77, INV OUT=468.66
STMH#2	STA.=1+32.22, OFFSET -10.50, L	FRAME=500.50, INV IN=495.58, INV OUT=495.58

STORM SEWER PIPE TABLE			
PIPE TAG	SIZE	LENGTH	SLOPE
1	12"	26.00'	0.50%
2	12"	80.00'	2.00%
3	12"	82.00'	4.50%
4	12"	15.00'	4.00%
5	12"	73.01'	14.18%
6	12"	155.00'	5.00%
7	12"	134.00'	1.70%
8	12"	83.00'	0.50%
9	12"	40.00'	6.00%
10	12"	40.00'	8.00%
11	12"	23.20'	3.00%
12	12"	8.00'	2.95%
13	12"	8.00'	2.95%
14	4" SDR-35 PVC SOLID STORM DRAIN PIPE AT MIN. 1.00% SLOPE. (PROVIDE BOOTED CONNECTION TO CITY SINGLE CATCH BASIN OR DIRECT CONNECTION TO SICPP STORM SEWER PIPE WITH AN INSERT-A-TEE FITTING INTO THE STORM SEWER SYSTEM. CONNECT SOLID PIPE TO PERFORATED UNDERDRAIN PIPE ON THE BACK SIDE OF THE RETAINING WALL.)		



BURLINGTON STREET (STA.4+00 TO STA.8+00) - SANITARY AND STORM SEWER PROFILE
SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=5'

12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



NYS WBE/DBE Certified
SBA Woman Owned
Small Business (WOSB)

aubertinecurrier.com

522 Bradley Street
Watertown, New York 13601
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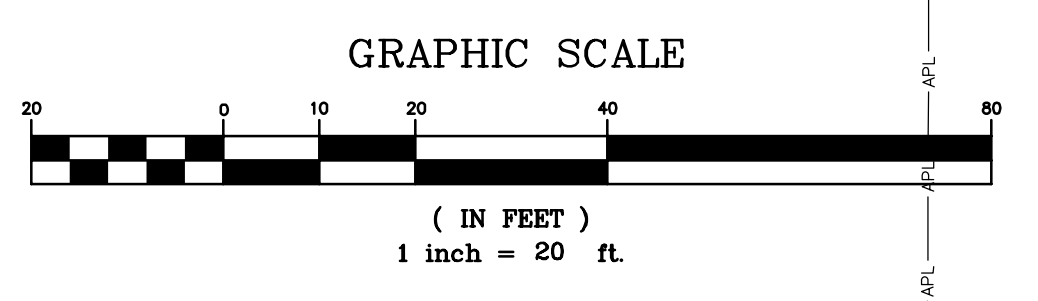
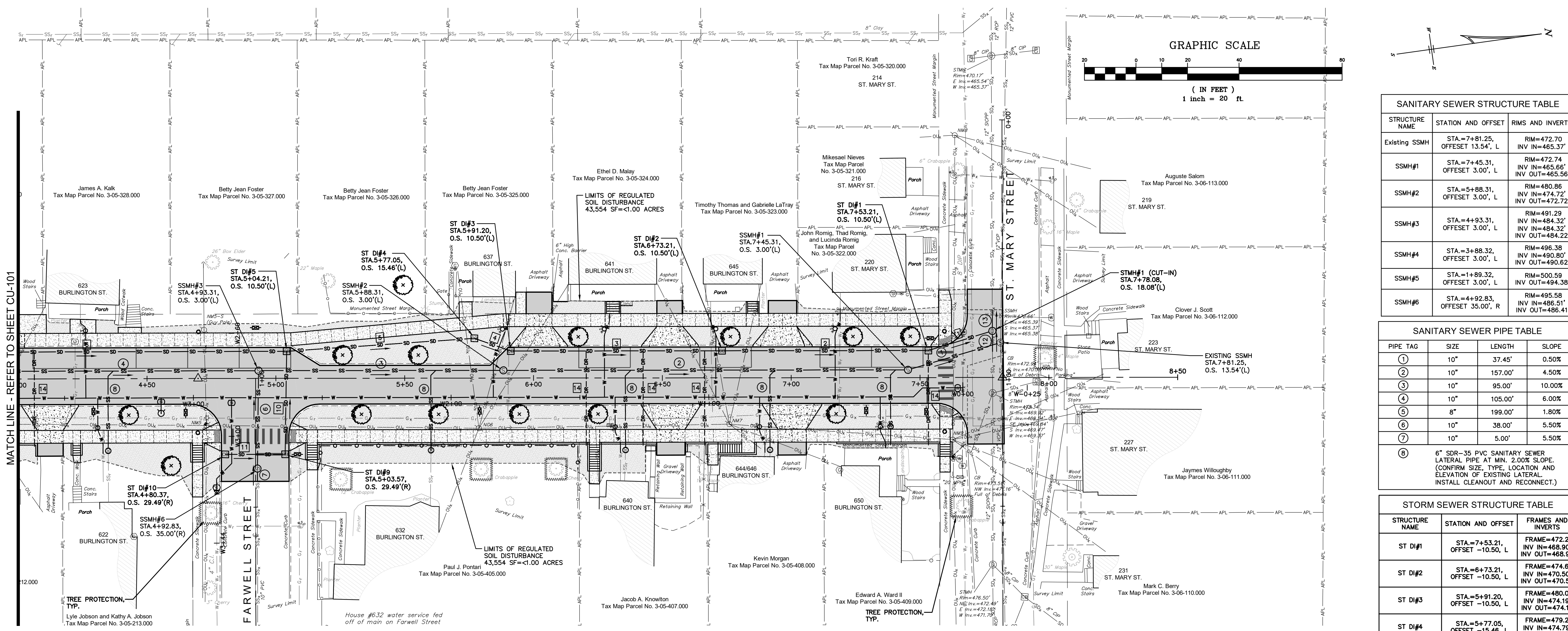


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=20'
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/03/2024
12/13/2024

SANITARY AND STORM SEWER
PROFILES

CU-103



SANITARY SEWER STRUCTURE TABLE

STRUCTURE NAME	STATION AND OFFSET	RIMS AND INVERTS
Existing SSMH	STA.=7+81.25, OFFSET 13.54', L	RIM=472.70 INV IN=465.37
SSMH#1	STA.=7+45.31, L	RIM=472.74 INV IN=465.66 INV OUT=465.56
SSMH#2	STA.=5+88.31, OFFSET 3.00', L	RIM=480.86 INV IN=474.72 INV OUT=472.72
SSMH#3	STA.=4+93.31, OFFSET 3.00', L	RIM=491.29 INV IN=484.32 INV OUT=484.22
SSMH#4	STA.=3+88.32, OFFSET 3.00', L	RIM=496.38 INV IN=490.80 INV OUT=490.62
SSMH#5	STA.=1+89.32, OFFSET 3.00', L	RIM=500.59 INV IN=494.38
SSMH#6	STA.=4+92.83, OFFSET 35.00', R	RIM=495.58 INV IN=486.51 INV OUT=486.41

SANITARY SEWER PIPE TABLE

PIPE TAG	SIZE	LENGTH	SLOPE
1	10"	37.45'	0.50%
2	10"	157.00'	4.50%
3	10"	95.00'	10.00%
4	10"	105.00'	6.00%
5	8"	199.00'	1.80%
6	10"	38.00'	5.50%
7	10"	5.00'	5.50%

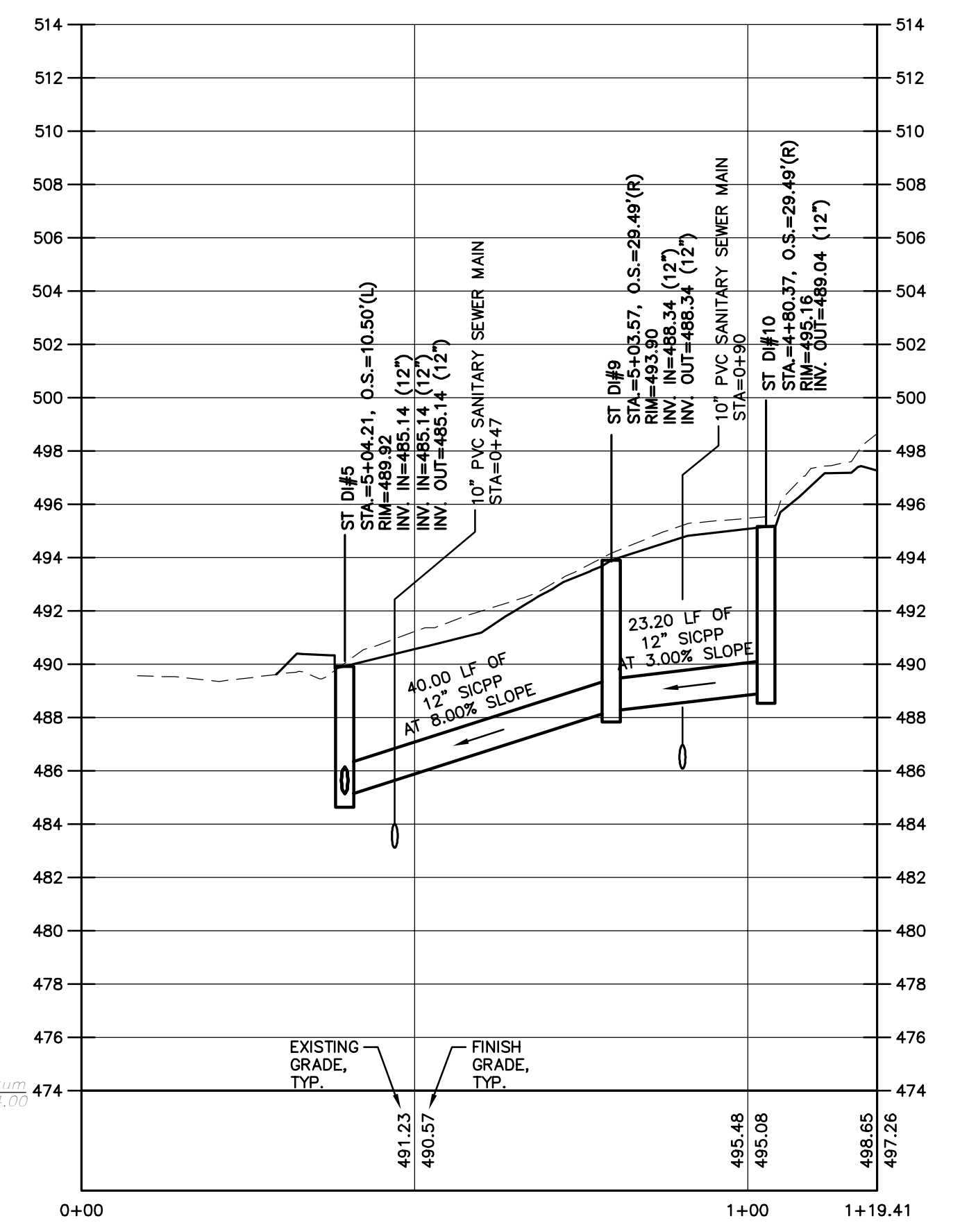
6" SDR-35 PVC SANITARY SEWER LATERAL PIPE AT MIN. 2.00% SLOPE. (CONFIRM SIZE, TYPE, LOCATION AND ELEVATION OF EXISTING LATERAL PIPE AND INSTALL CLEANOUT AND RECONNECT.)

STORM SEWER STRUCTURE TABLE

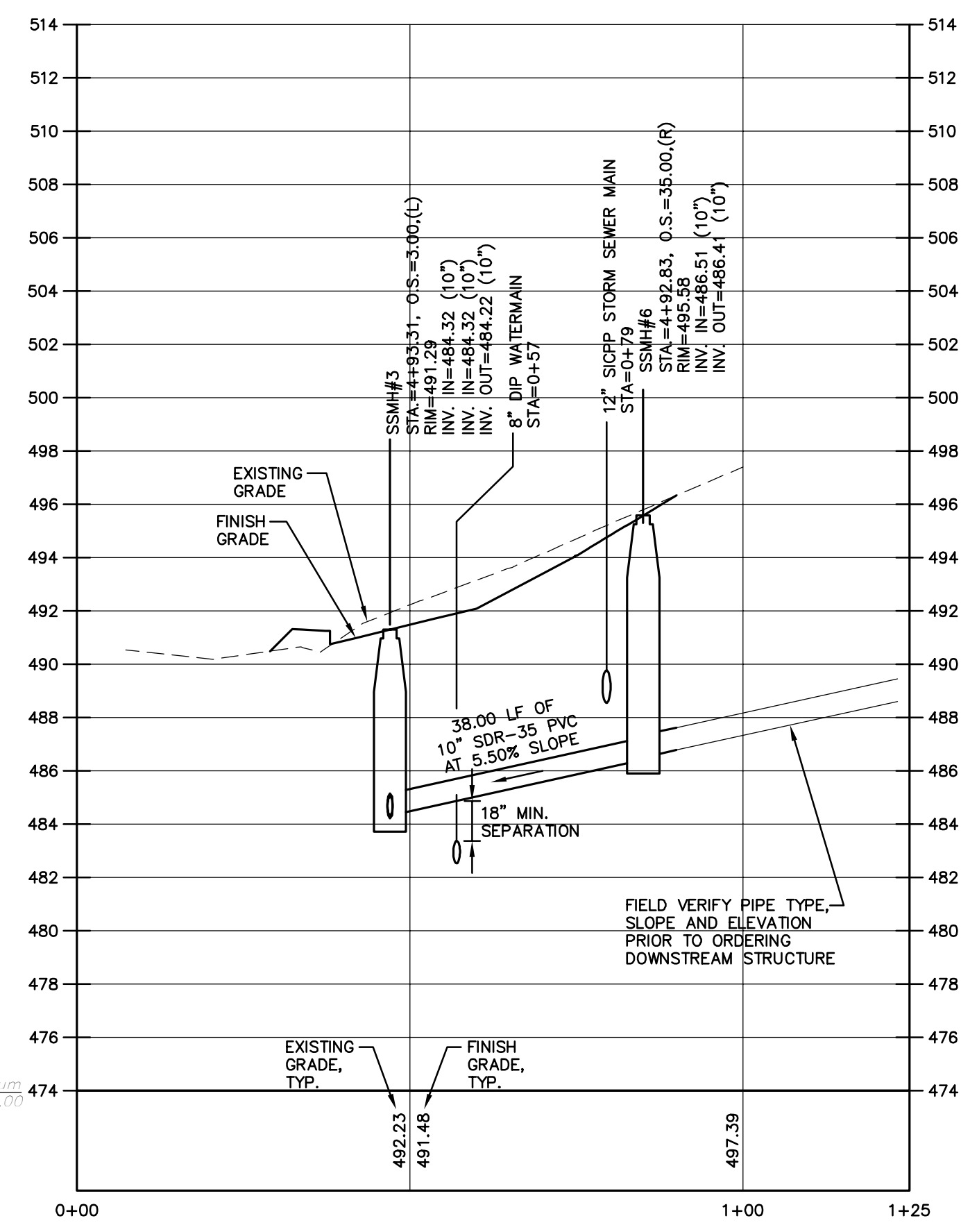
STRUCTURE NAME	STATION AND OFFSET	FRAMES AND INVERTS
ST DI#1	STA.=7+53.21, OFFSET -10.50, L	FRAME=472.25 INV IN=468.90' INV OUT=468.90
ST DI#2	STA.=6+73.21, L	FRAME=474.68 INV IN=470.50' INV OUT=470.50
ST DI#3	STA.=5+91.20, OFFSET -10.50, L	FRAME=480.07 INV IN=474.19' INV OUT=474.19
ST DI#4	STA.=5+77.05, OFFSET -15.46, L	FRAME=479.25 INV IN=474.79' INV OUT=474.79
ST DI#5	STA.=5+04.21, OFFSET -10.50, L	FRAME=489.92 INV IN=485.14' INV OUT=485.14
ST DI#6	STA.=3+49.23, OFFSET -10.50, L	FRAME=497.16 INV IN=492.89' INV OUT=492.89
ST DI#7	STA.=2+15.23, OFFSET -10.50, L	FRAME=499.70 INV IN=495.17' INV OUT=495.17
ST DI#8	STA.=1+32.15, OFFSET 29.50, R	FRAME=504.30 INV IN=497.98
ST DI#9	STA.=5+03.57, R	FRAME=493.90 INV IN=488.34' INV OUT=488.34
ST DI#10	STA.=4+80.37, R	FRAME=495.18 INV IN=489.04' INV OUT=489.04
STMH#1 (CUT-IN)	STA.=7+78.08, OFFSET -18.08, L	FRAME=472.55 INV IN=468.66' INV OUT=468.66
STMH#2	STA.=1+32.22, OFFSET -10.50, L	FRAME=500.50 INV IN=495.58' INV OUT=495.58

STORM SEWER PIPE TABLE

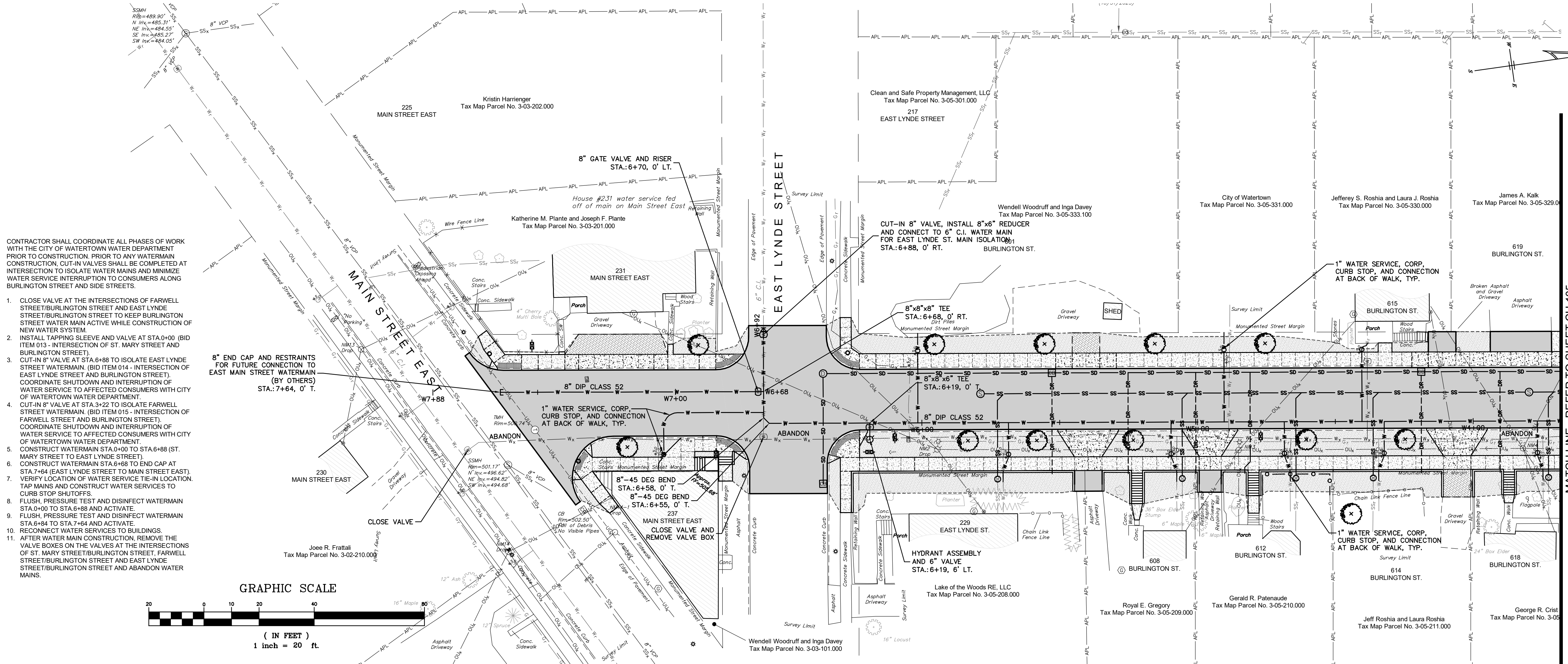
PIPE TAG	SIZE	LENGTH	SLOPE
1	12"	26.00'	0.50%
2	12"	80.00'	2.00%
3	12"	82.00'	4.50%
4	12"	15.00'	4.00%
5	12"	73.01'	14.18%
6	12"	155.00'	5.00%
7	12"	134.00'	1.70%
8	12"	83.00'	0.50%
9	12"	40.00'	6.00%
10	12"	40.00'	8.00%
11	12"	23.20'	3.00%
12	12"	8.00'	2.95%
13	12"	8.00'	2.95%
14	4" SDR-35 PVC SOLID STORM DRAIN PIPE AT MIN. 1.00% SLOPE. (PROVIDE BOOTED CONNECTION TO CITY SINGLE CATCH BASIN OR DIRECT CONNECTION TO SICPP STORM SEWER PIPE WITH AN INSERT-A-TEE FITTING INTO THE STORM SEWER SYSTEM. CONNECT SOLID PIPE TO PERFORATED UNDERDRAIN PIPE ON THE BACK SIDE OF THE RETAINING WALL.)		



STORM SEWER PROFILE - FARWELL STREET
SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=5'

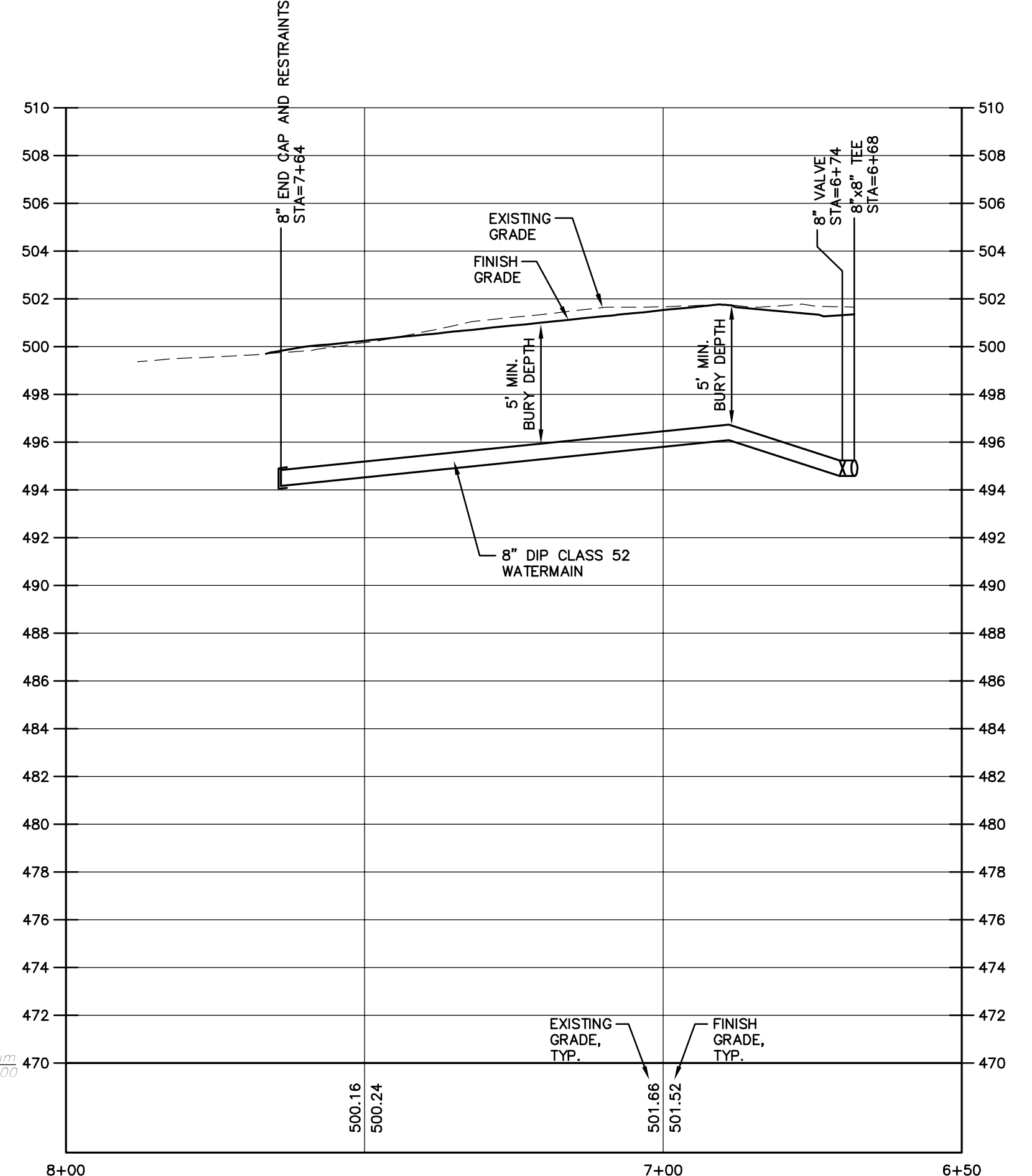
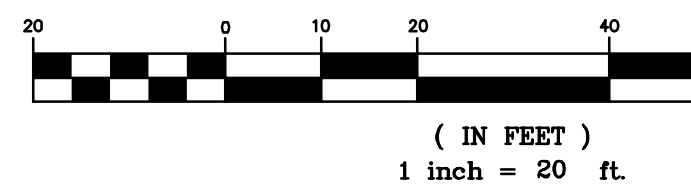


SANITARY SEWER PROFILE - FARWELL STREET
SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=5'

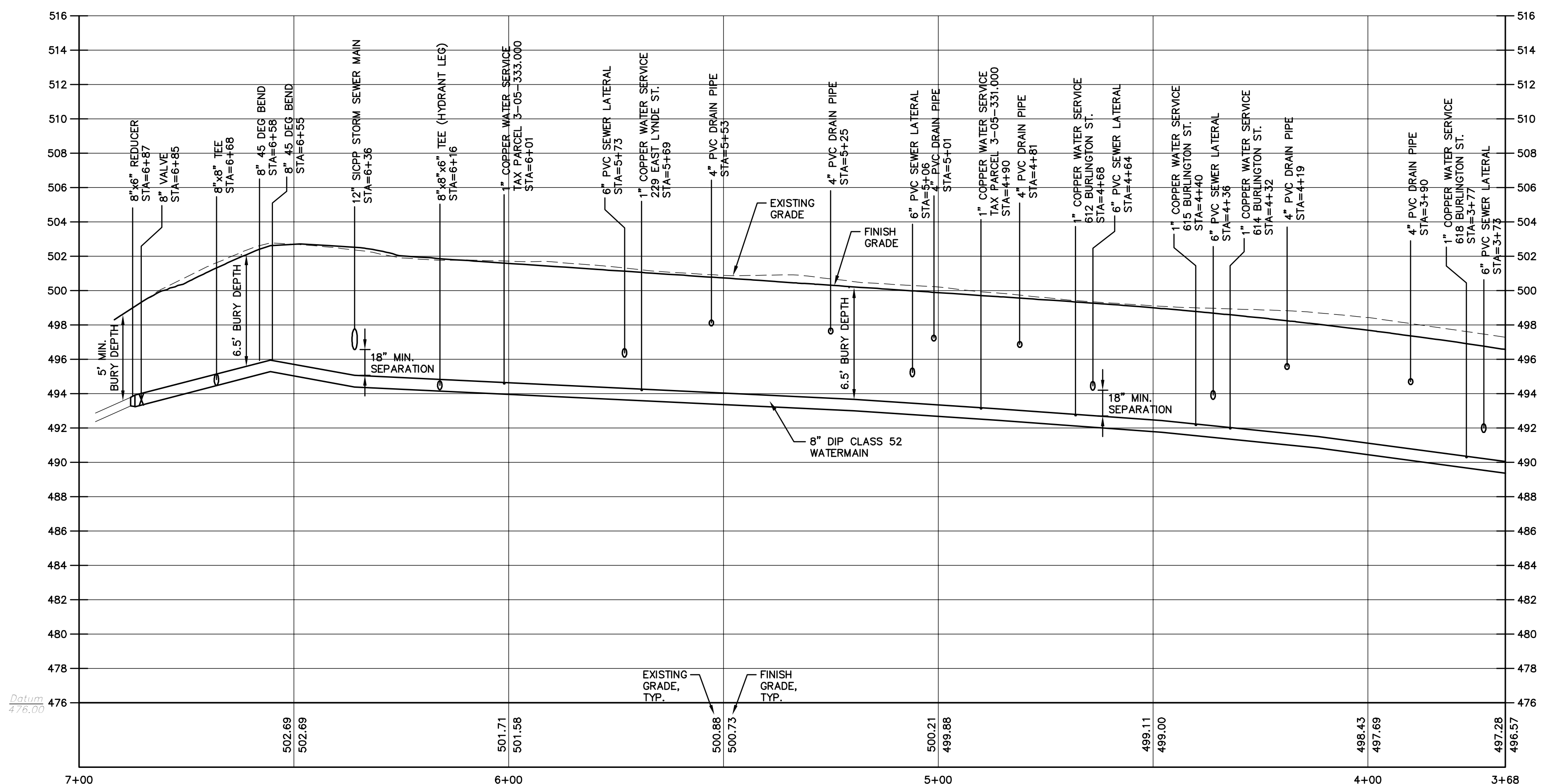


- CONTRACTOR SHALL COORDINATE ALL PHASES OF WORK WITH THE CITY OF WATERTOWN WATER DEPARTMENT PRIOR TO CONSTRUCTION. PRIOR TO ANY WATERMAIN CONSTRUCTION, CUT-IN VALVES SHALL BE COMPLETED AT INTERSECTION TO ISOLATE WATER MAINS AND MINIMIZE WATER SERVICE INTERRUPTION TO CONSUMERS ALONG BURLINGTON STREET AND SIDE STREETS.
- CLOSE VALVE AT THE INTERSECTIONS OF FARWELL STREET/BURLINGTON STREET AND EAST LYNDE STREET/BURLINGTON STREET TO KEEP BURLINGTON STREET WATER MAIN ACTIVE WHILE CONSTRUCTION OF NEW WATER SYSTEM
 - INSTALL TAPPING SLEEVE AND VALVE AT STA.0+00 (BID ITEM 013 - INTERSECTION OF ST. MARY STREET AND BURLINGTON STREET)
 - CUT-IN 8" VALVE AT STA.6+88 TO ISOLATE EAST LYNDE STREET WATERMAIN (BID ITEM 014 - INTERSECTION OF EAST LYNDE STREET AND BURLINGTON STREET). COORDINATE SHUTDOWN AND INTERRUPTION OF WATER SERVICE TO AFFECTED CONSUMERS WITH CITY OF WATERTOWN WATER DEPARTMENT.
 - CUT-IN 8" VALVE AT STA.3+22 TO ISOLATE FARWELL STREET WATERMAIN (BID ITEM 015 - INTERSECTION OF FARWELL STREET AND BURLINGTON STREET). COORDINATE SHUTDOWN AND INTERRUPTION OF WATER SERVICE TO AFFECTED CONSUMERS WITH CITY OF WATERTOWN WATER DEPARTMENT.
 - CONSTRUCT WATERMAIN STA.0+00 TO STA.6+88 (ST. MARY STREET TO EAST LYNDE STREET)
 - CONSTRUCT WATERMAIN STA.6+88 TO END CAP AT STA.7+64 (EAST LYNDE STREET TO MAIN STREET EAST)
 - VERIFY LOCATION OF WATER SERVICE TIE-IN LOCATION. TAP MAINS AND CONSTRUCT WATER SERVICES TO CURB STOP SHUTOFFS.
 - FLUSH, PRESSURE TEST AND DISINFECT WATERMAIN STA.0+00 TO STA.6+88 AND ACTIVATE.
 - FLUSH, PRESSURE TEST AND DISINFECT WATERMAIN STA.6+88 TO STA.7+64 AND ACTIVATE.
 - RECONNECT WATER SERVICES TO BUILDINGS.
 - AFTER WATER MAIN CONSTRUCTION, REMOVE THE VALVE BOXES ON THE VALVES AT THE INTERSECTIONS OF ST. MARY STREET/BURLINGTON STREET, FARWELL STREET/BURLINGTON STREET AND EAST LYNDE STREET/BURLINGTON STREET AND ABANDON WATER MAINS.

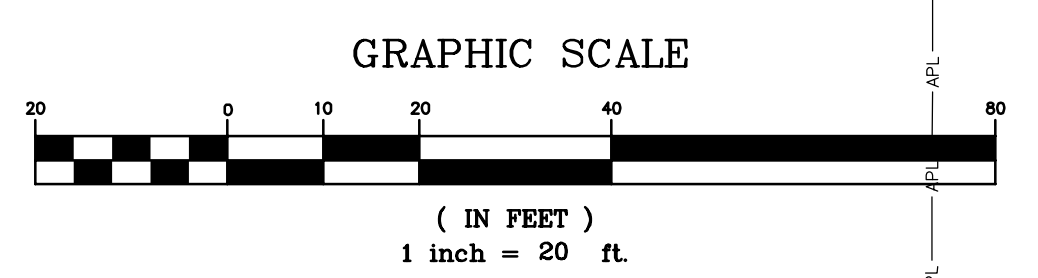
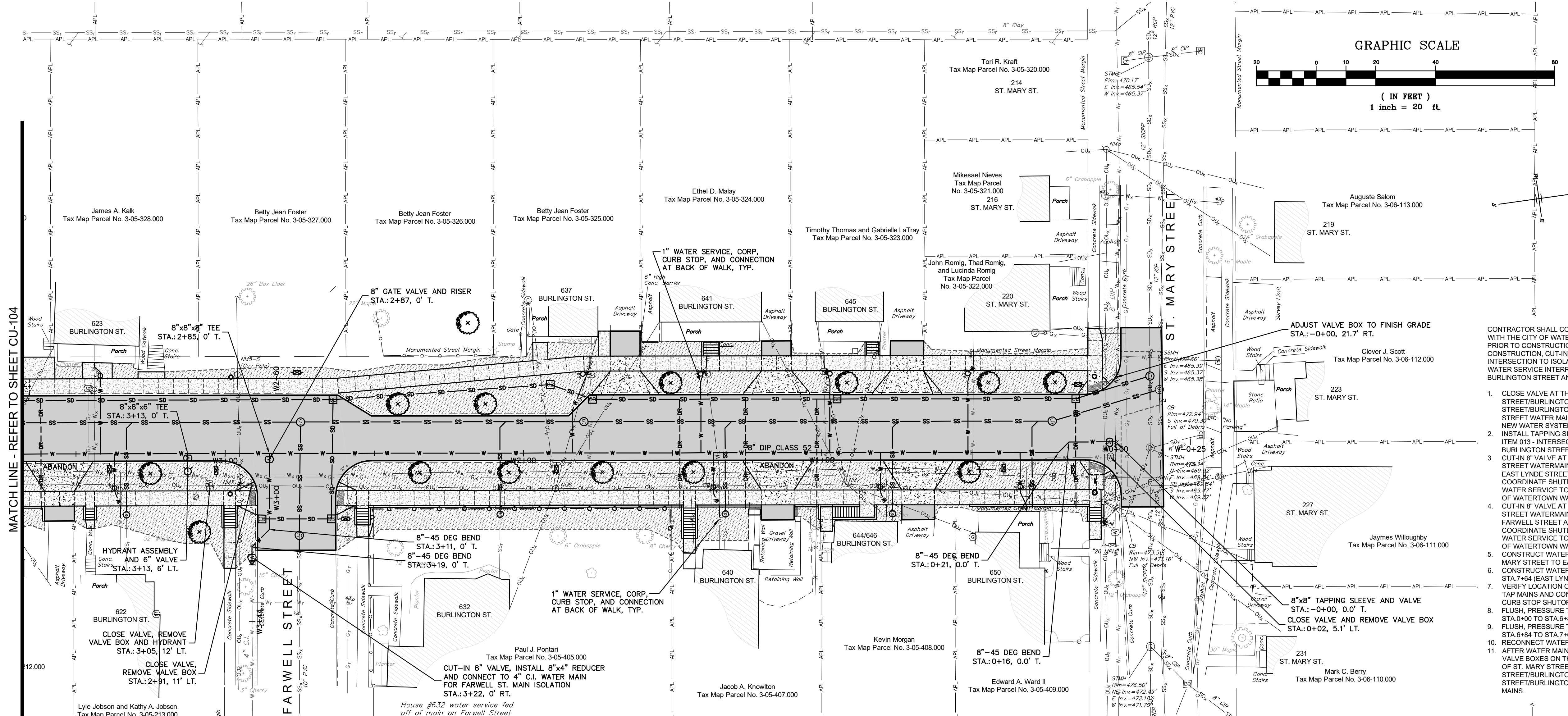
GRAPHIC SCALE



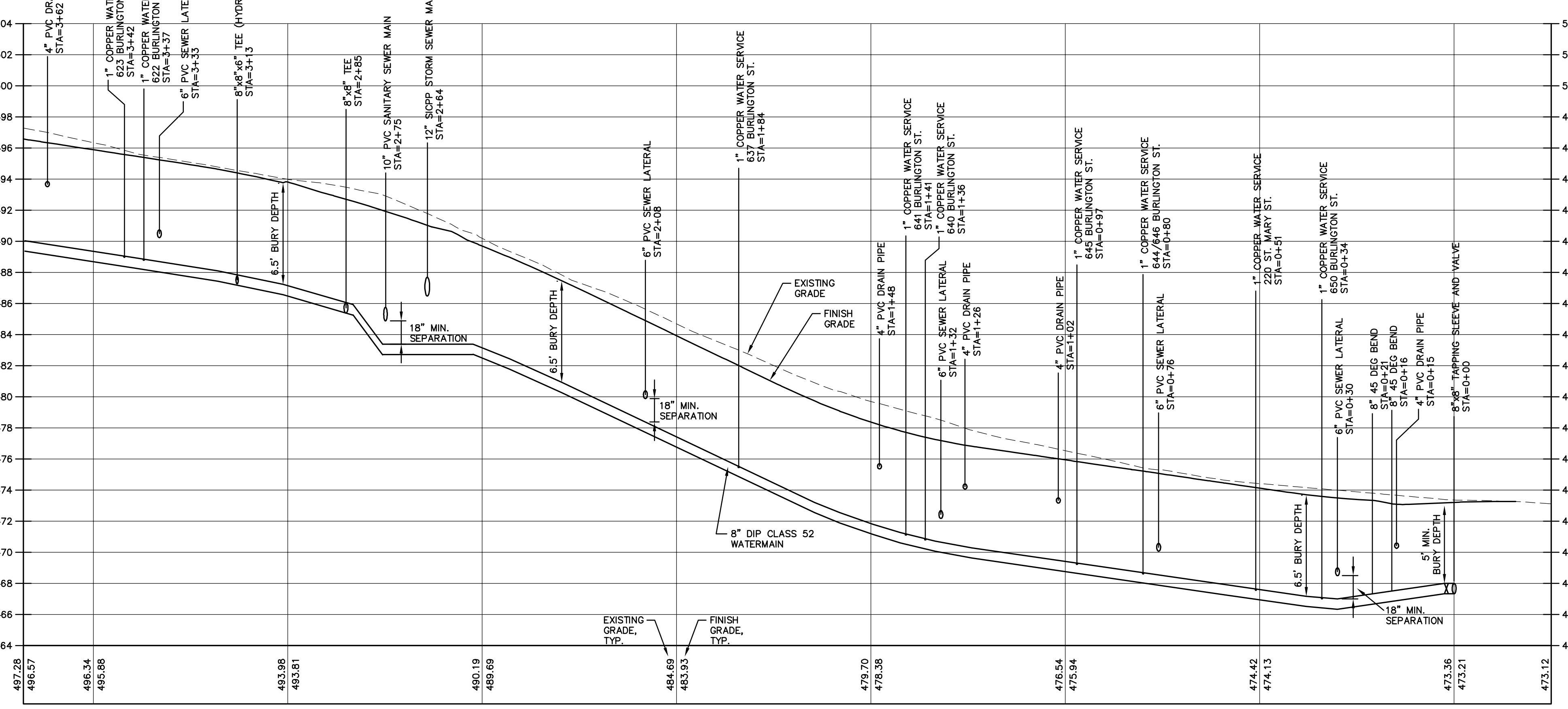
WATERMAIN PROFILE - STA.W 6+68 TO STA.W 7+64
SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=5'
(BURLINGTON STREET)



WATERMAIN PROFILE - STA.W 3+68 TO STA.W 6+85
SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=5'
(BURLINGTON STREET)

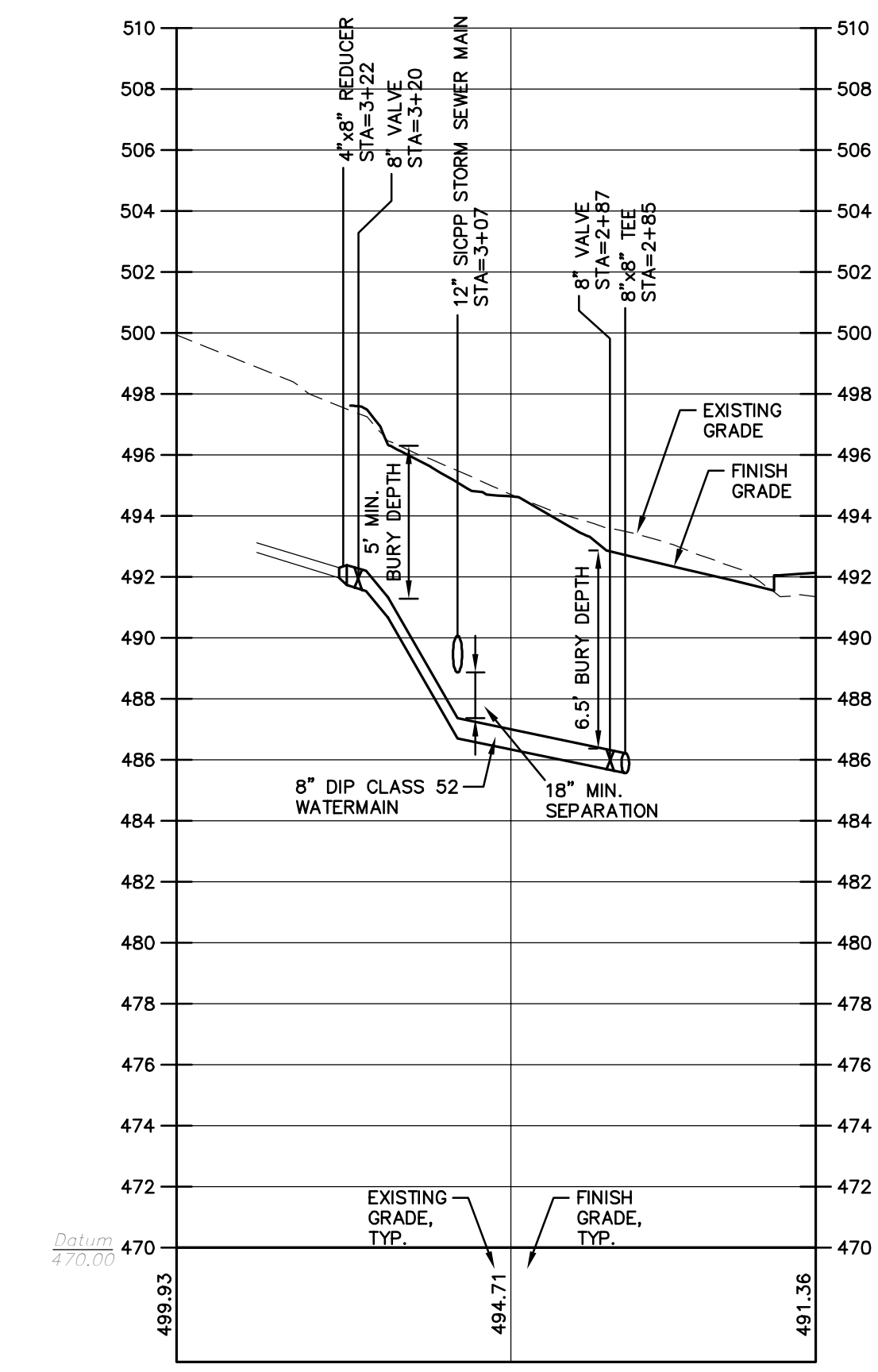


- CONTRACTOR SHALL COORDINATE ALL PHASES OF WORK WITH THE CITY OF WATERTOWN WATER DEPARTMENT PRIOR TO CONSTRUCTION. PRIOR TO ANY WATERMAIN CONSTRUCTION, CUT-IN VALVES SHALL BE COMPLETED AT INTERSECTION TO ISOLATE WATER MAINS AND MINIMIZE WATER SERVICE INTERRUPTION TO CONSUMERS ALONG BURLINGTON STREET AND SIDE STREETS.
- CLOSE VALVE AT THE INTERSECTIONS OF FARWELL STREET/BURLINGTON STREET AND EAST LYNDE STREET/BURLINGTON STREET TO KEEP BURLINGTON STREET WATER MAIN ACTIVE WHILE CONSTRUCTION OF NEW WATER SYSTEM.
 - INSTALL TAPPING SLEEVE AND VALVE AT STA.0+00 (BID ITEM 013 - INTERSECTION OF ST. MARY STREET AND BURLINGTON STREET).
 - CUT-IN 8" VALVE AT STA.6+88 TO ISOLATE EAST LYNDE STREET WATERMAIN. (BID ITEM 014 - INTERSECTION OF EAST LYNDE STREET AND BURLINGTON STREET). COORDINATE SHUTDOWN AND INTERRUPTION OF WATER SERVICE TO AFFECTED CONSUMERS WITH CITY OF WATERTOWN WATER DEPARTMENT.
 - CUT-IN 8" VALVE AT STA.3+22 TO ISOLATE FARWELL STREET WATERMAIN. (BID ITEM 015 - INTERSECTION OF FARWELL STREET AND BURLINGTON STREET). COORDINATE SHUTDOWN AND INTERRUPTION OF WATER SERVICE TO AFFECTED CONSUMERS WITH CITY OF WATERTOWN WATER DEPARTMENT.
 - CONSTRUCT WATERMAIN STA.0+00 TO STA.6+88 (ST. MARY STREET TO EAST LYNDE STREET).
 - CONSTRUCT WATERMAIN STA.6+88 TO END CAP AT STA.7+64 (EAST LYNDE STREET TO MAIN STREET EAST). VERIFY LOCATION OF WATER SERVICE TIE-IN LOCATION, TAP MAINS AND CONSTRUCT WATER SERVICES TO CURB STOP SHUTOFFS.
 - FLUSH, PRESSURE TEST AND DISINFECT WATERMAIN STA.0+00 TO STA.6+88 AND ACTIVATE.
 - FLUSH, PRESSURE TEST AND DISINFECT WATERMAIN STA.6+88 TO STA.7+64 AND ACTIVATE.
 - RECONNECT WATER SERVICES TO BUILDINGS.
 - AFTER WATER MAIN CONSTRUCTION, REMOVE THE VALVE BOXES ON THE VALVES AT THE INTERSECTIONS OF ST. MARY STREET/BURLINGTON STREET, FARWELL STREET/BURLINGTON STREET AND EAST LYNDE STREET/BURLINGTON STREET AND ABANDON WATER MAINS.

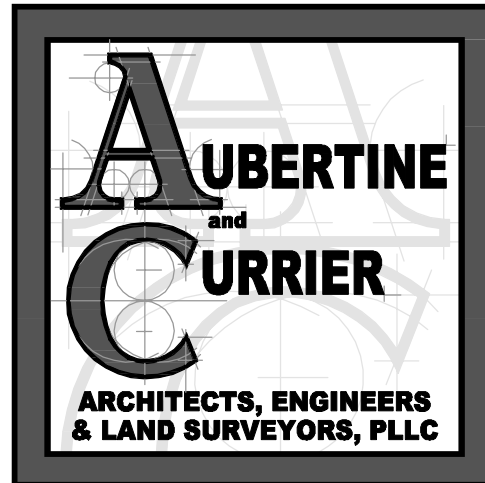


WATERMAIN PROFILE - STA.W 0+00 TO STA.W 3+68
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'
 (BURLINGTON STREET)

12/13/2024 BID DOCUMENTS FOR CONSTRUCTION



WATERMAIN PROFILE - STA.W2+85 TO STA.W3+50
 SCALE: HORIZONTAL 1" = 20'
 VERTICAL 1" = 5'
 (FARWELL STREET)

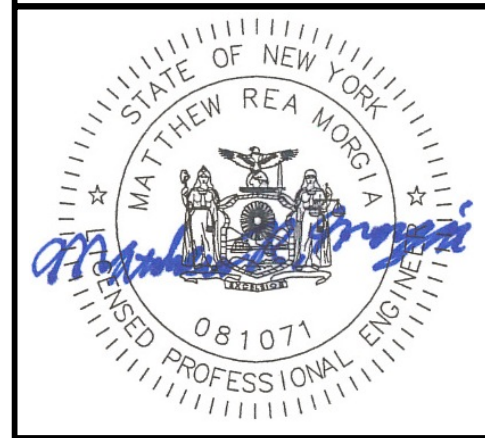


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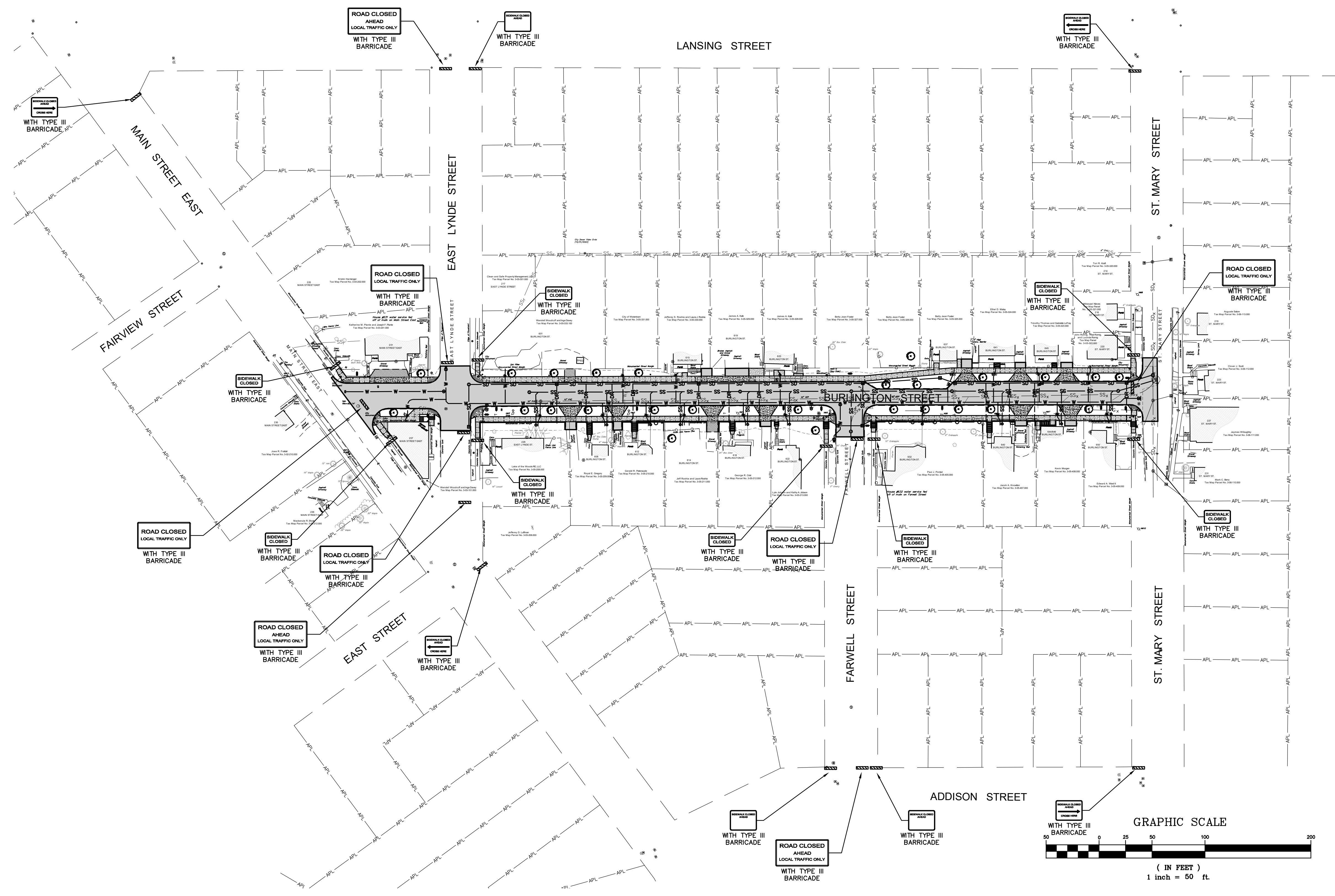


BURLINGTON STREET RECONSTRUCTION PROJECT
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

UTILITY PLAN AND PROFILE
 WATERMAIN

CU-105

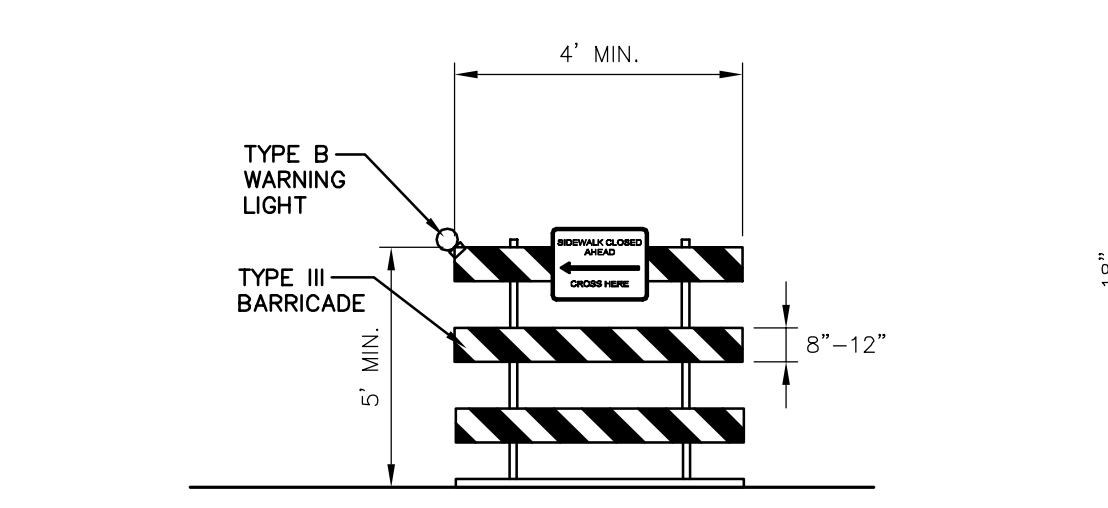
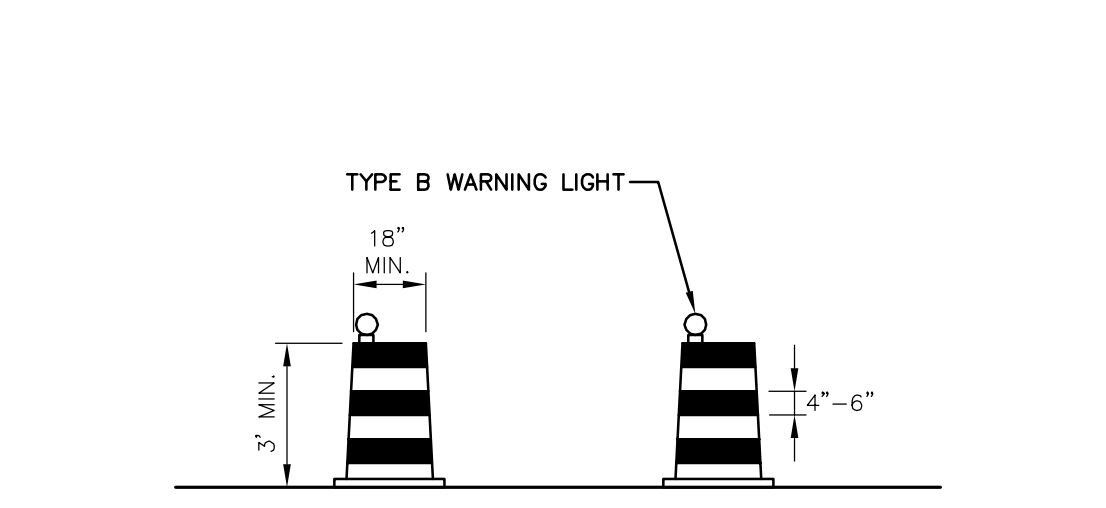
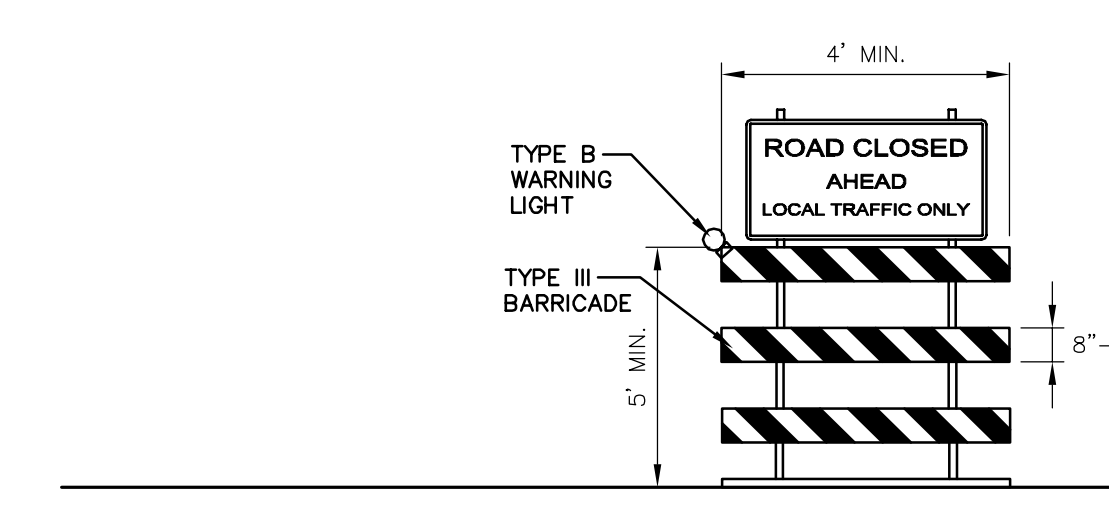
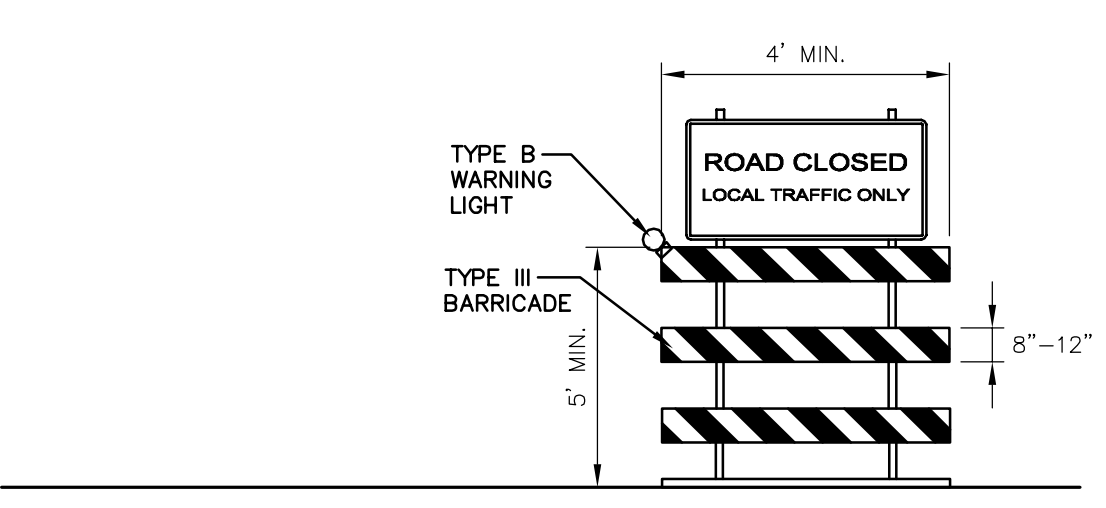
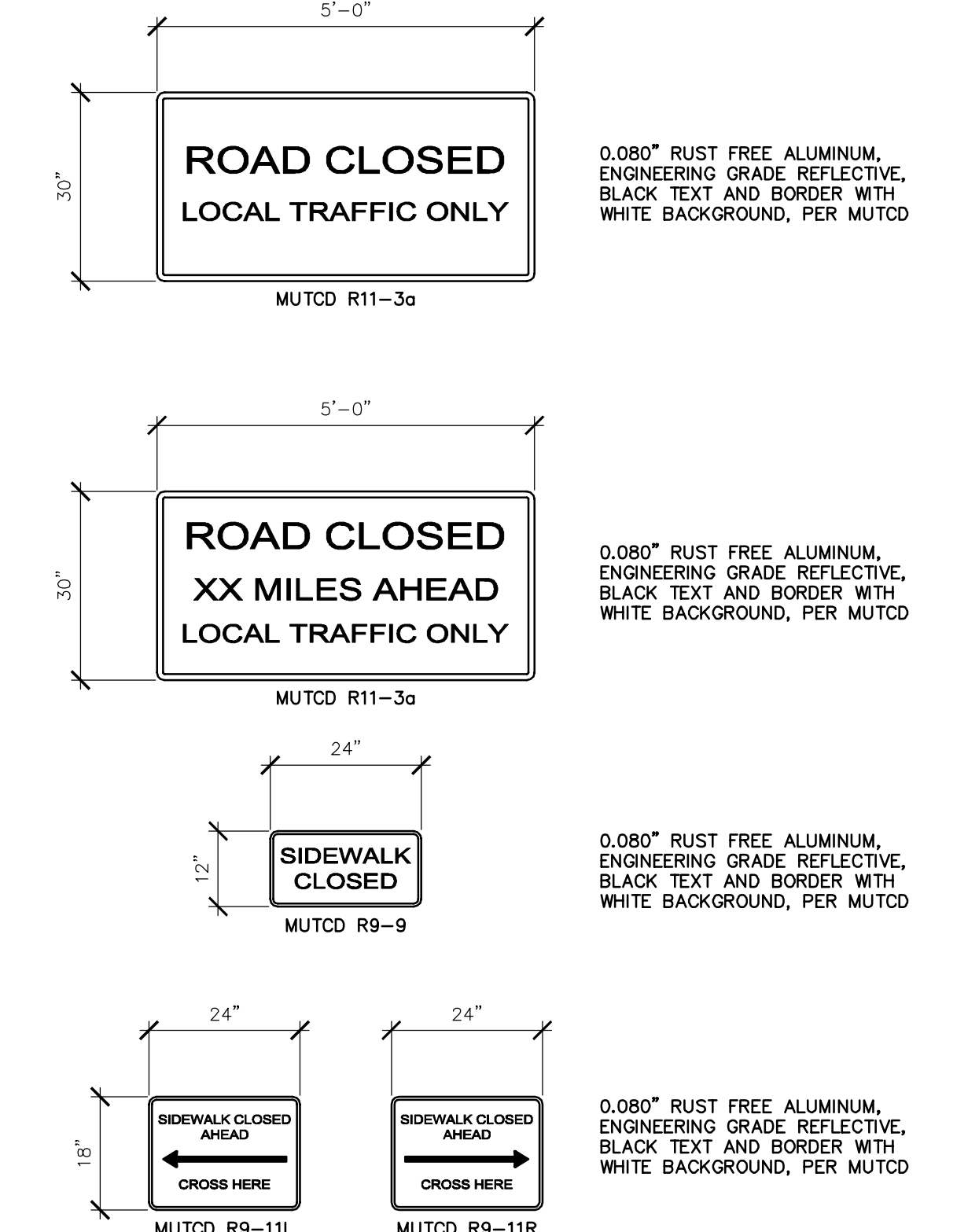


- WORK ZONE TRAFFIC CONTROL GENERAL NOTES**
1. WORK ZONE TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE MOST RECENT NYS DOT STANDARD SPECIFICATIONS - SECTION 619 WORK ZONE TRAFFIC CONTROL, THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS 2009 EDITION AND THE NEW YORK STATE SUPPLEMENT.
 2. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
 3. THE CONTRACTOR MUST SUBMIT TO THE CITY ENGINEERING, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL. A MINIMUM OF FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN.
 4. THE CONTRACTOR SHALL PROVIDE TO CITY ENGINEERING, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS.
 5. A MINIMUM OF TWO WEEK PRIOR NOTICE SHALL BE GIVEN TO CITY ENGINEERING IF THE CONTRACTOR SHALL IMPOSE A ROAD CLOSURE OF ONE LANE FOR ANY DURATION (HOURLY OR DAYS).

- SIGNS:**
1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
 2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
 3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
 4. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE RAMP, AND ONE-WAY STREETS. IN CASES WHERE LANE RESTRICTIONS REDUCE THE TRAVEL LANE TO ONE LANE, SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE ACTIVE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
 5. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
 6. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY CITY ENGINEERING.

MAINTENANCE OF TRAFFIC	
TRAFFIC SHALL BE MAINTAINED AND PROTECTED IN ACCORDANCE WITH THE M.U.T.C.D.	
OPERATION/WORK AREA	M.U.T.C.D. CHAPTER
CONSTRUCTION AND INSTALLATION OF THE TRAFFIC/PEDESTRIAN SIGNAL	6AE, 6F and 6G
ROADWAY CONSTRUCTION	6A-E, 6F and 6G

NOTE:
TYPICAL MAINTENANCE & PROTECTION OF TRAFFIC INSTALLATIONS AREA PRESENTED FOR MOST COMMON SITUATIONS WITH THE UNDERSTANDING THAT ADDITIONAL PROTECTIONS SHALL BE APPROVED WHERE SPECIAL COMPLEXITIES AND HAZARDS PREVAIL AS ORDERED BY ENGINEER



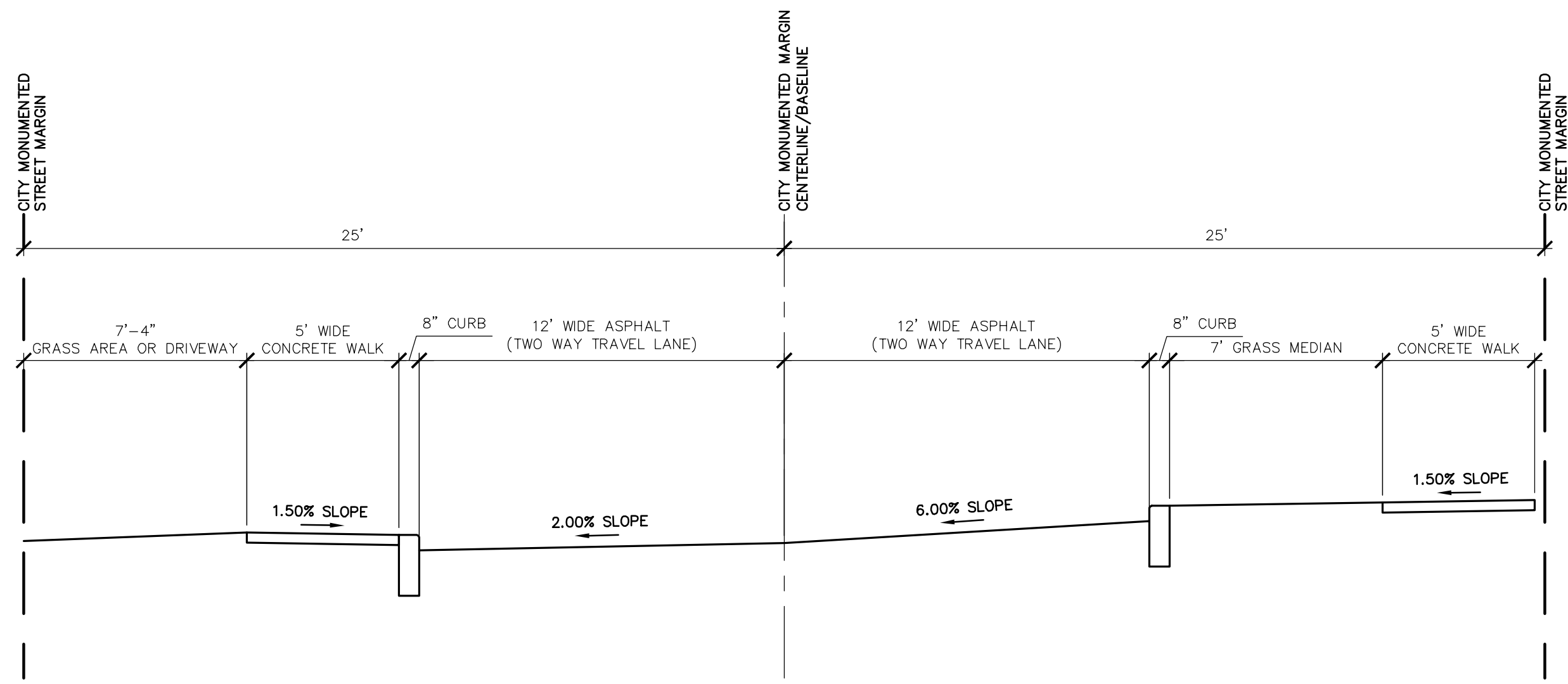
1 WORK ZONE TRAFFIC CONTROL TYPE III BARRICADE DETAIL
NOT TO SCALE

2 WORK ZONE TRAFFIC CONTROL TYPE III BARRICADE DETAIL
NOT TO SCALE

3 WORK ZONE TRAFFIC CONTROL CHANNELIZATION DEVICE DETAILS
NOT TO SCALE

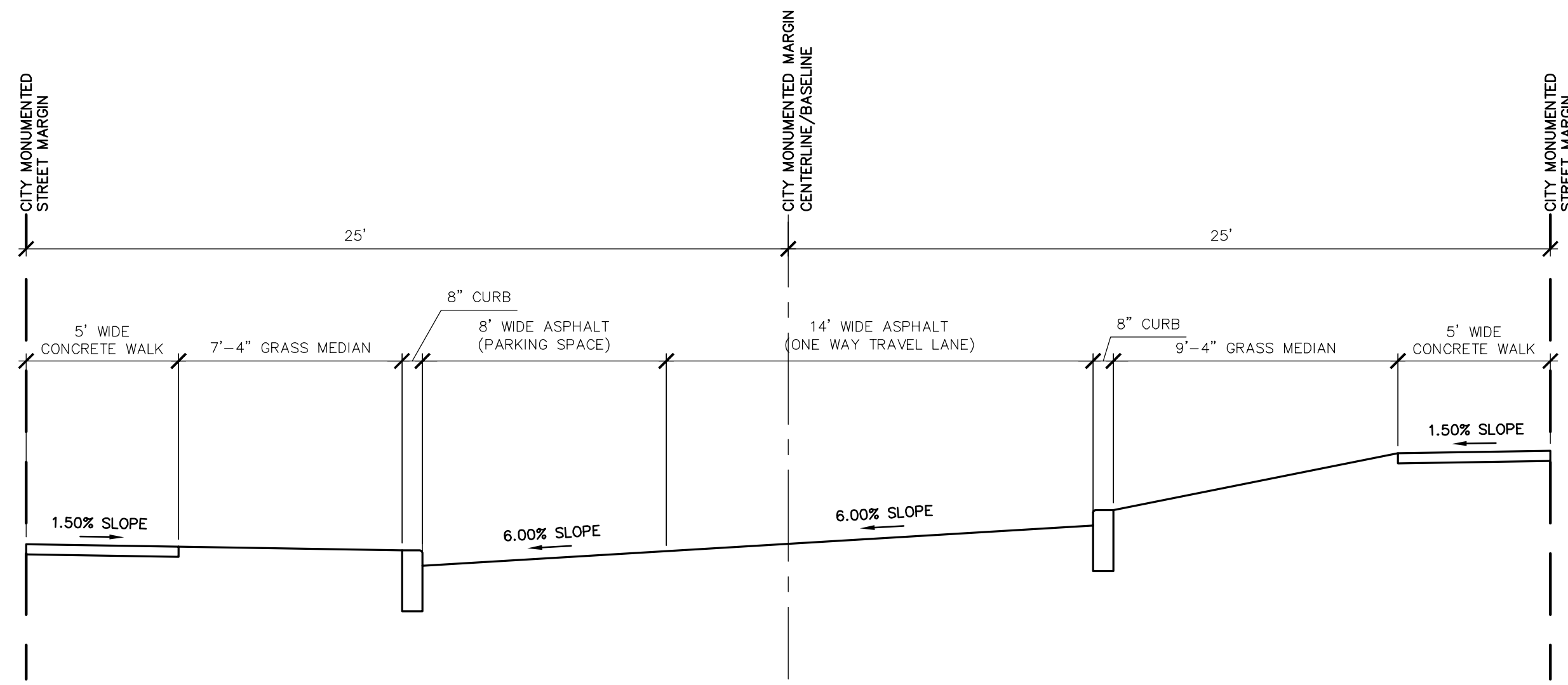
4 PEDESTRIAN SIDEWALK CONTROL TYPE III BARRICADE DETAIL
NOT TO SCALE

- NOTE:**
1. TYPE III BARRICADES SHALL BE FULL WIDTH OF THE SIDEWALK BEING CLOSED.
 2. PEDESTRIAN SIGNS THAT DISPLAYS CONTROLLING DIRECTION WITHOUT OPTIONS ON OTHER SIDE OF THE STREET SHOULD BE COVERED.



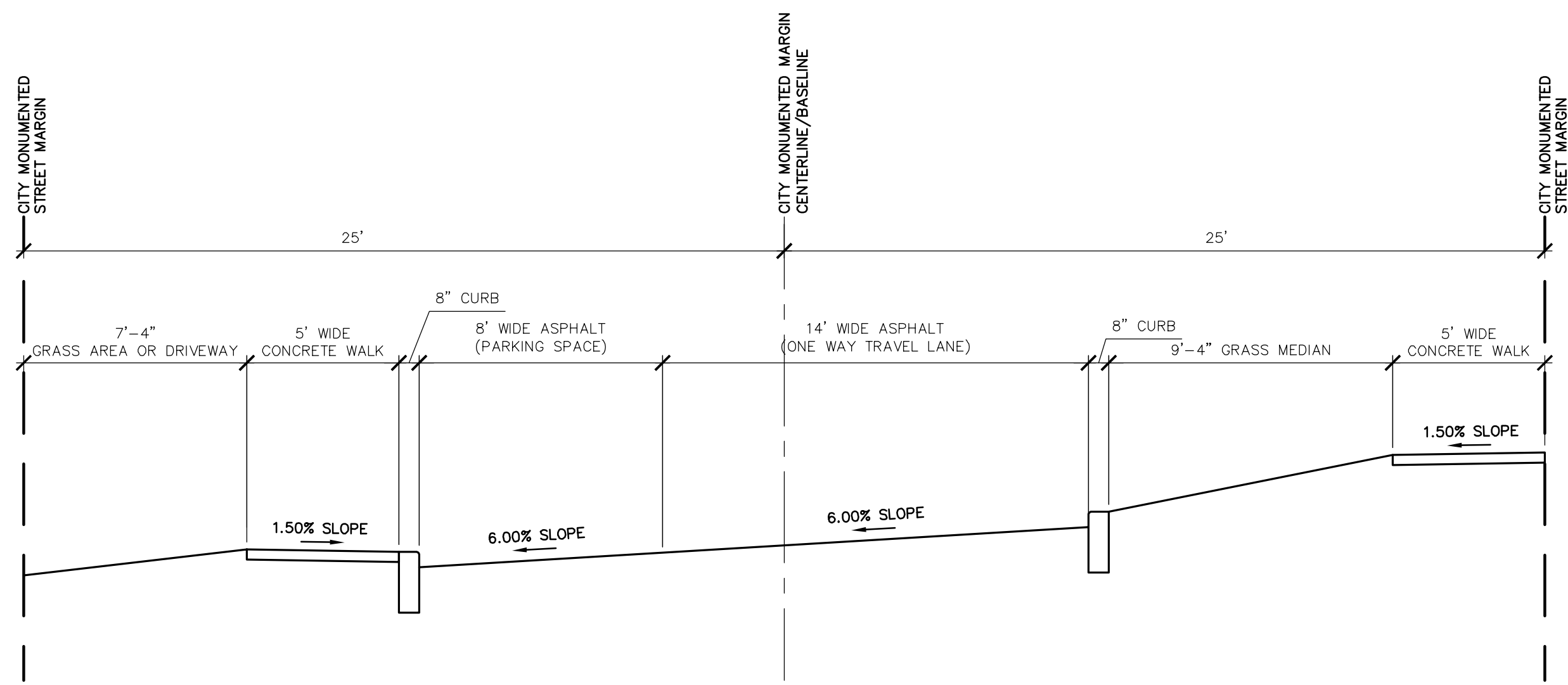
TYPICAL STREET CROSS SECTION (STA.0+55 TO STA.0+93)

SCALE: 1" = 4'



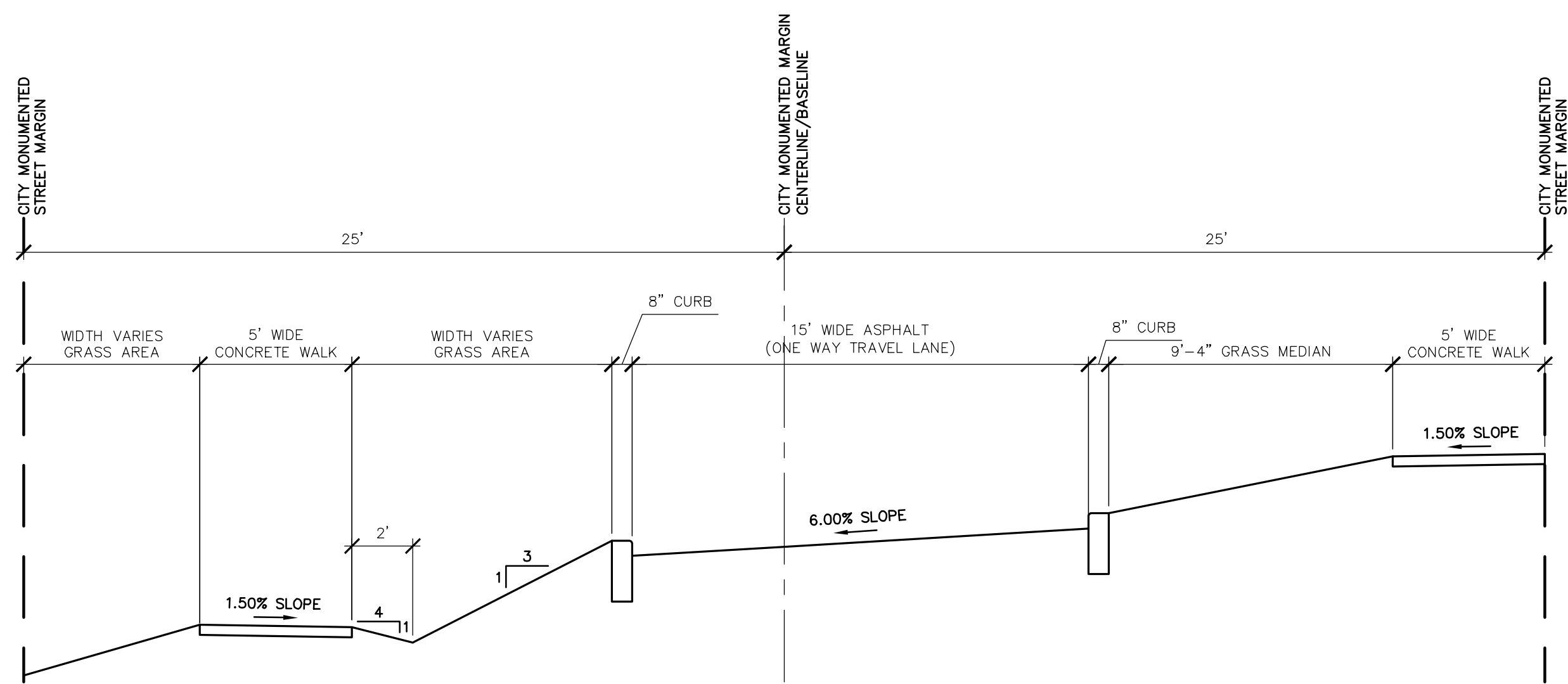
TYPICAL STREET CROSS SECTION (STA.5+90 TO STA.7+57)

SCALE: 1" = 4'



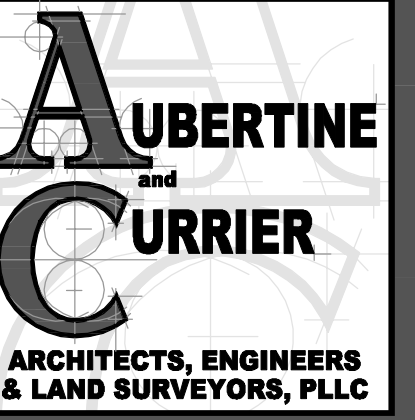
TYPICAL STREET CROSS SECTION (STA.1+45 TO STA.4+67)

SCALE: 1" = 4'



TYPICAL STREET CROSS SECTION (STA.5+17 TO STA.5+83)

SCALE: 1" = 4'



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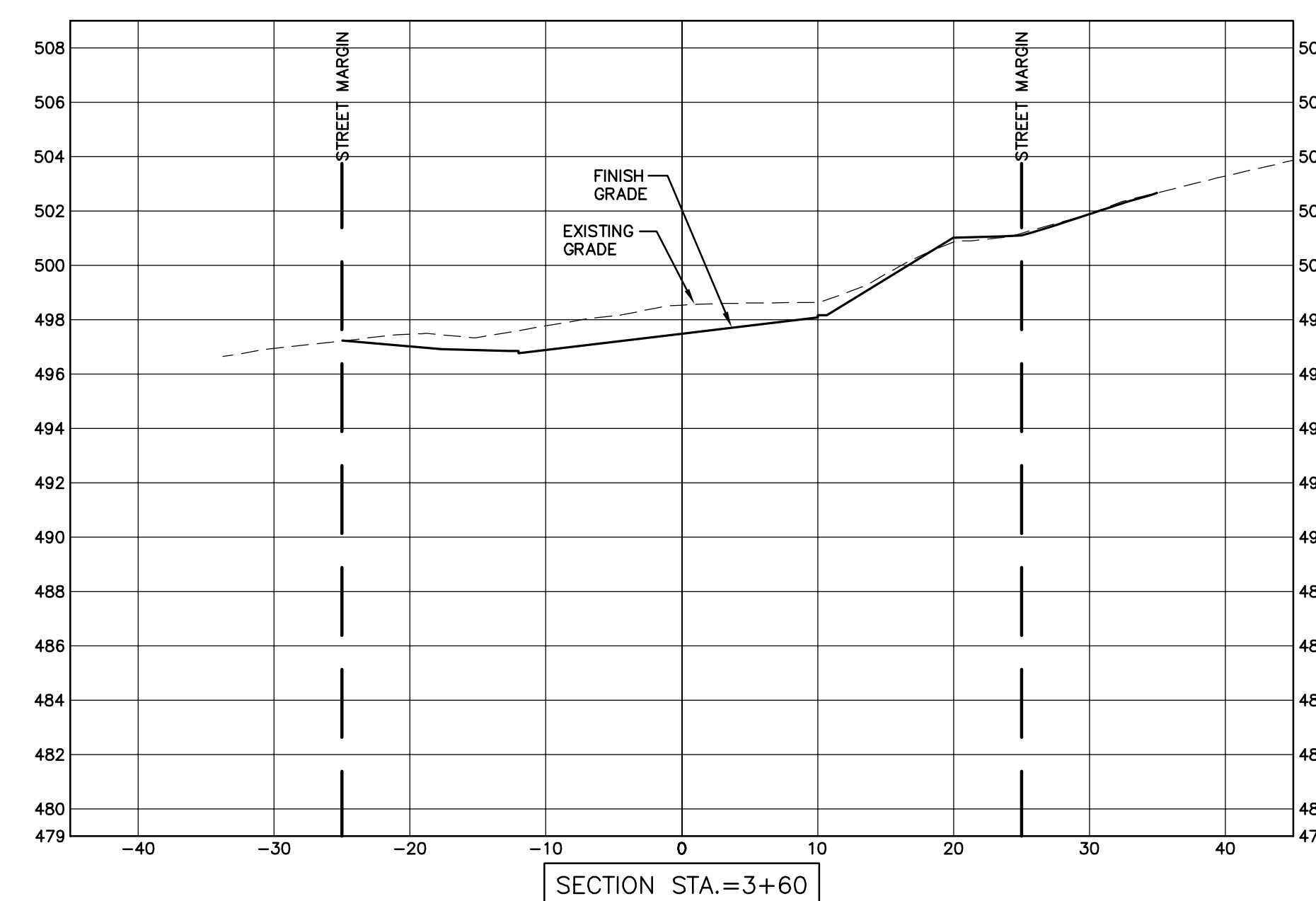
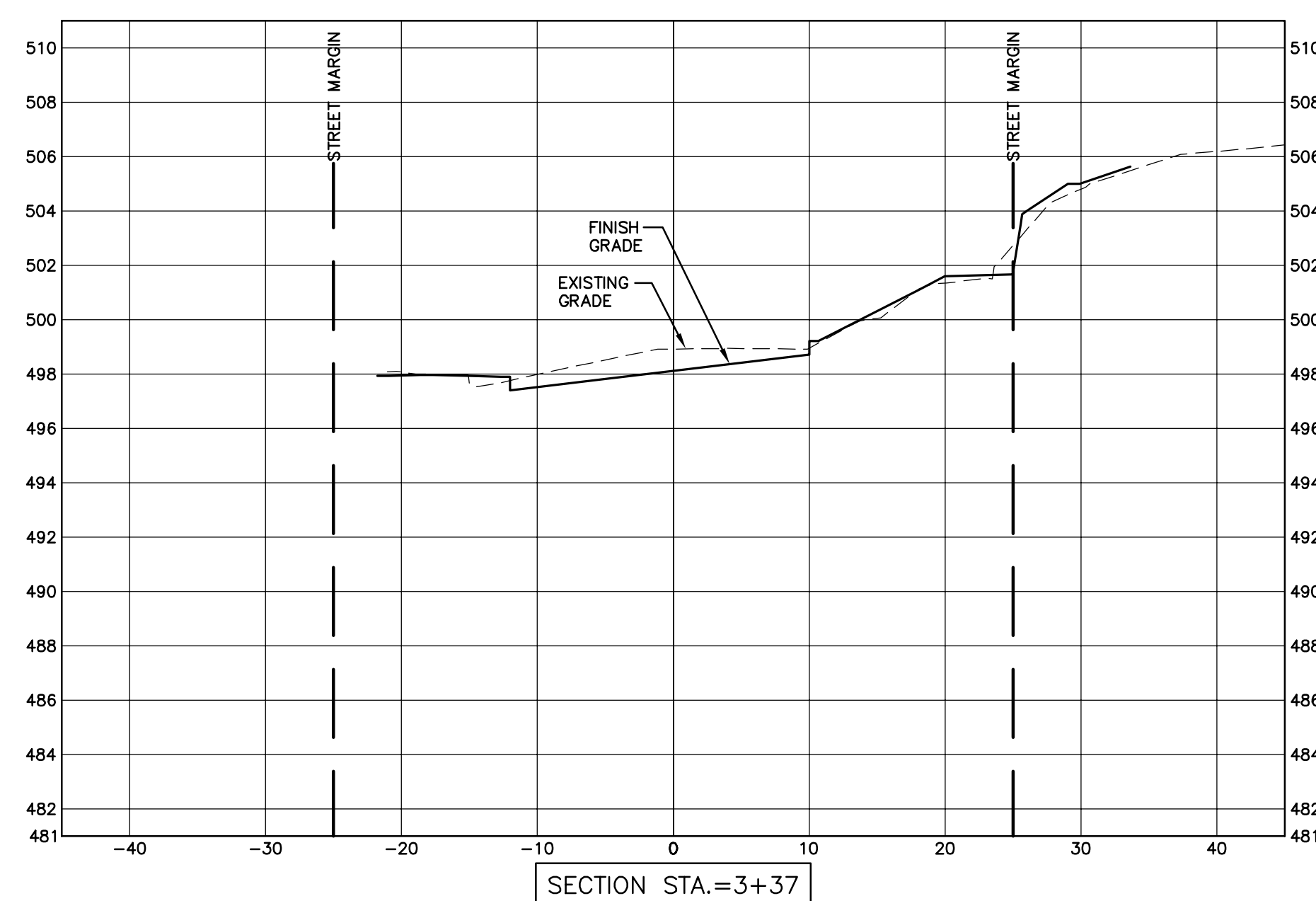
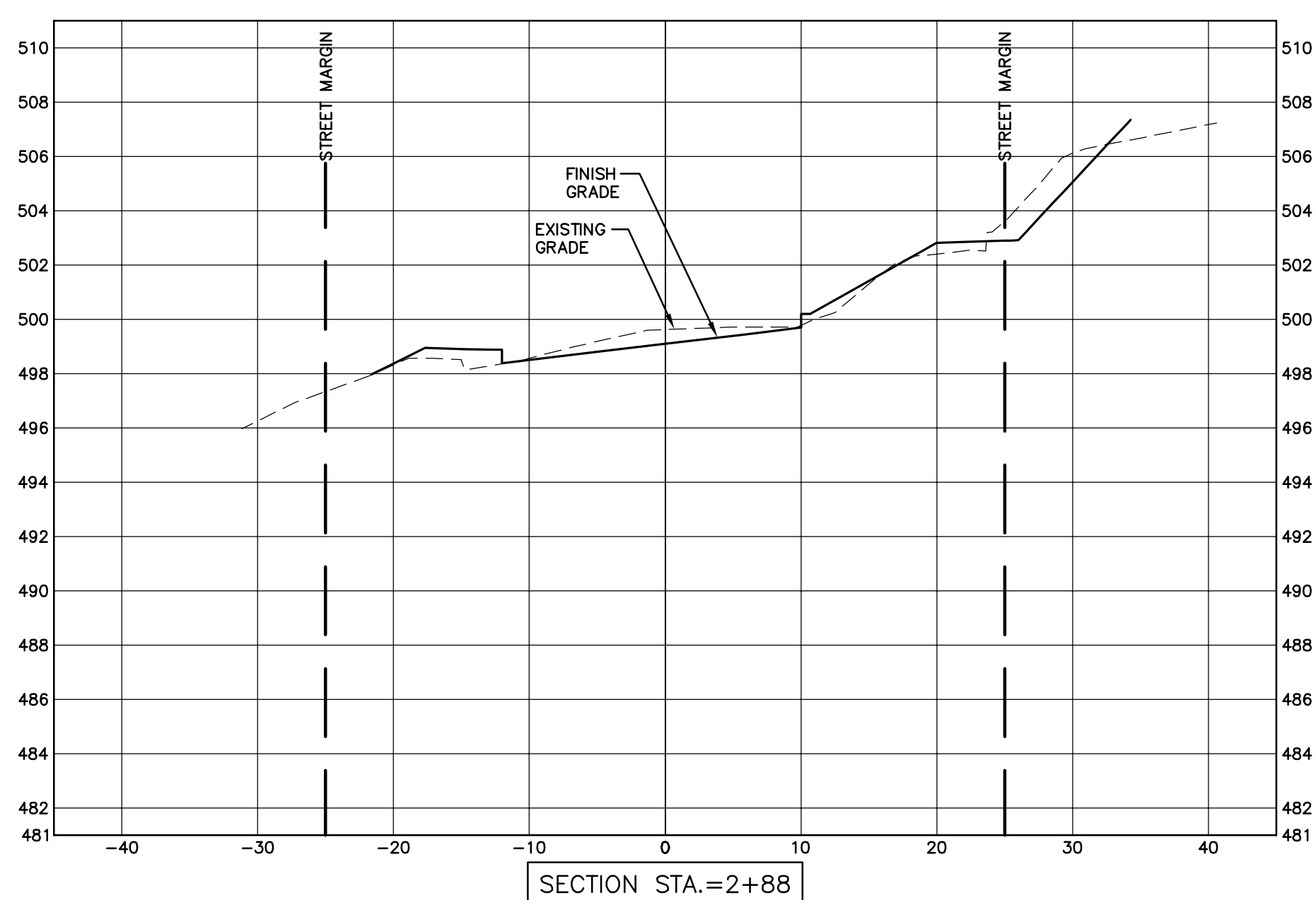
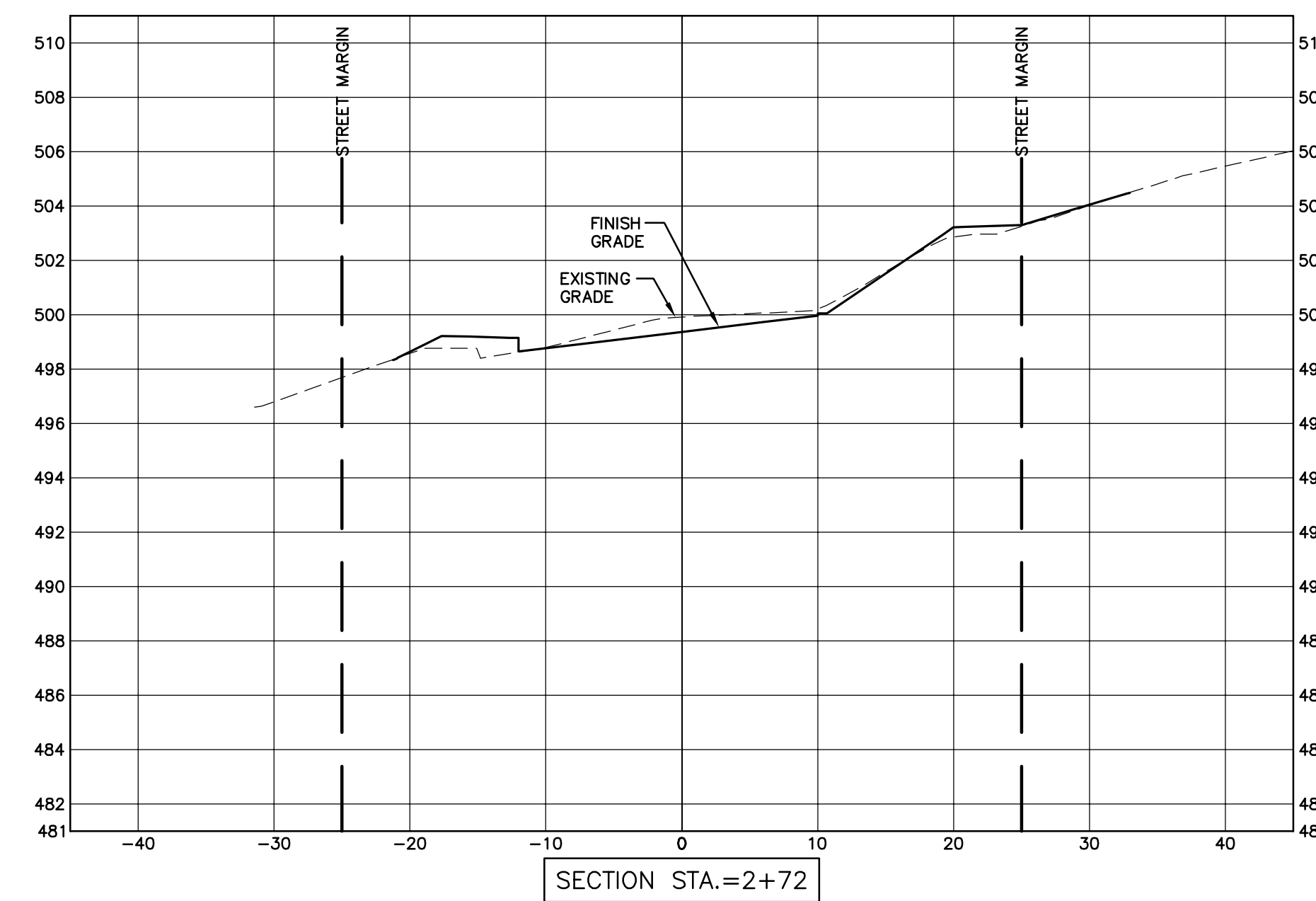
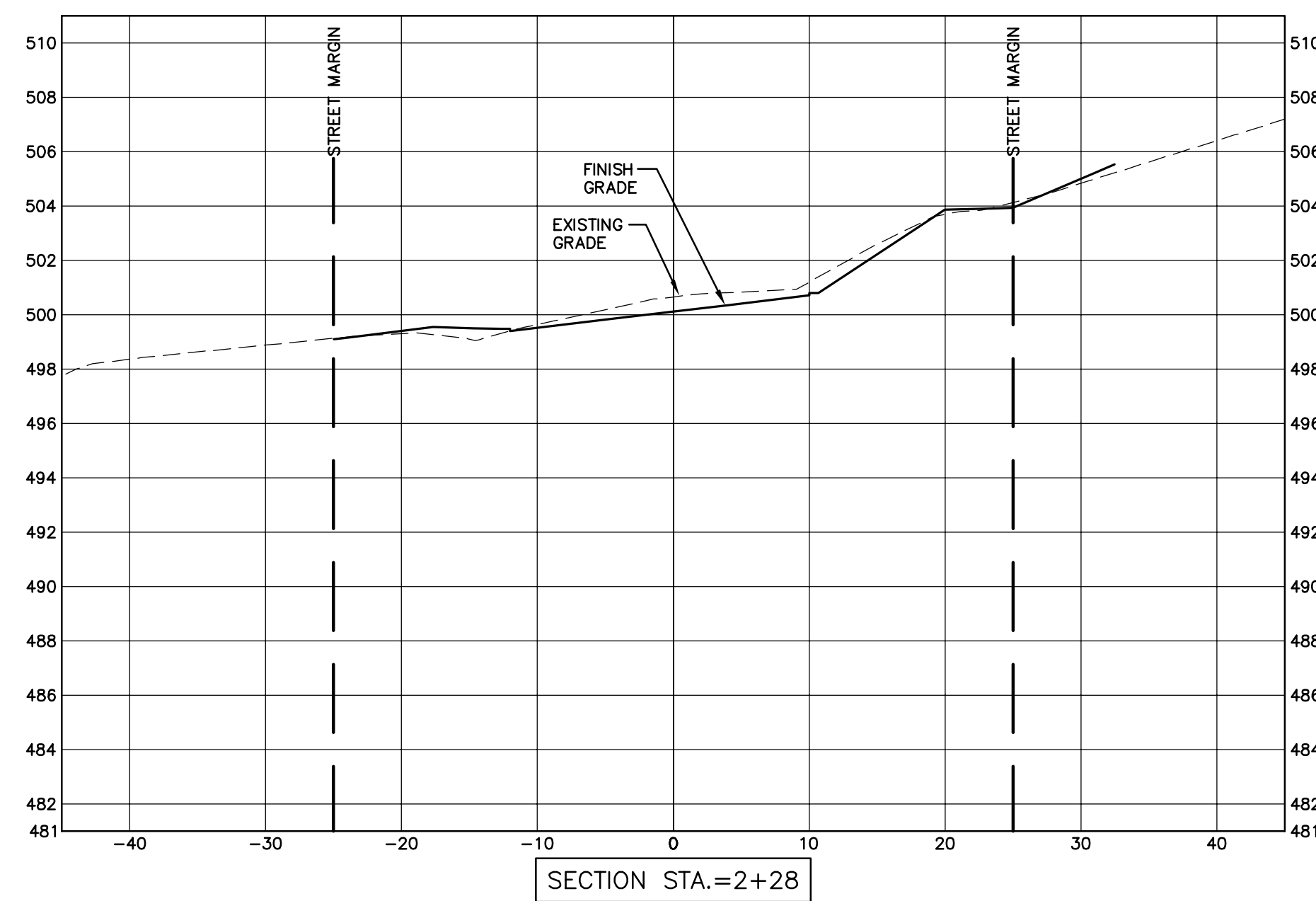
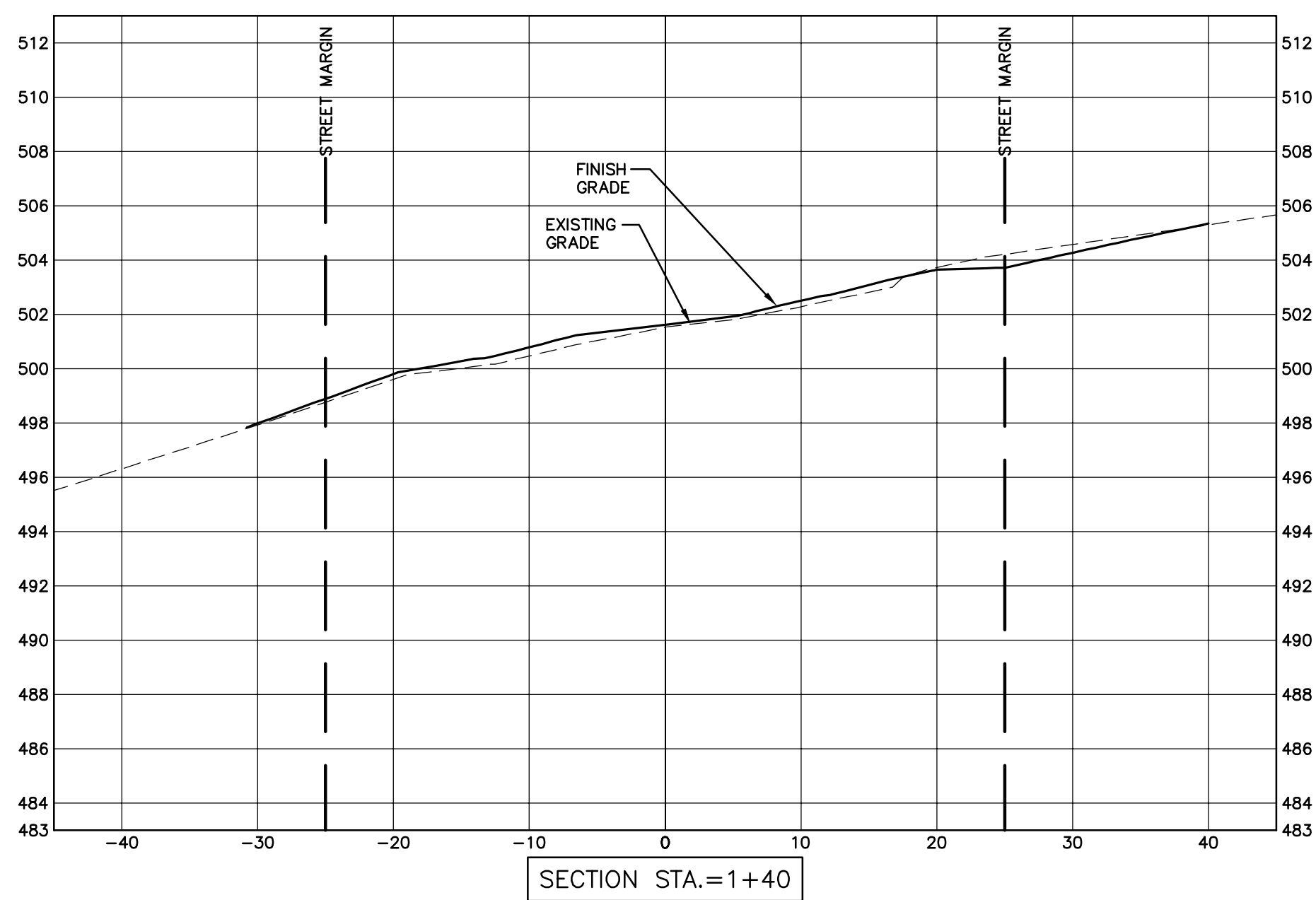
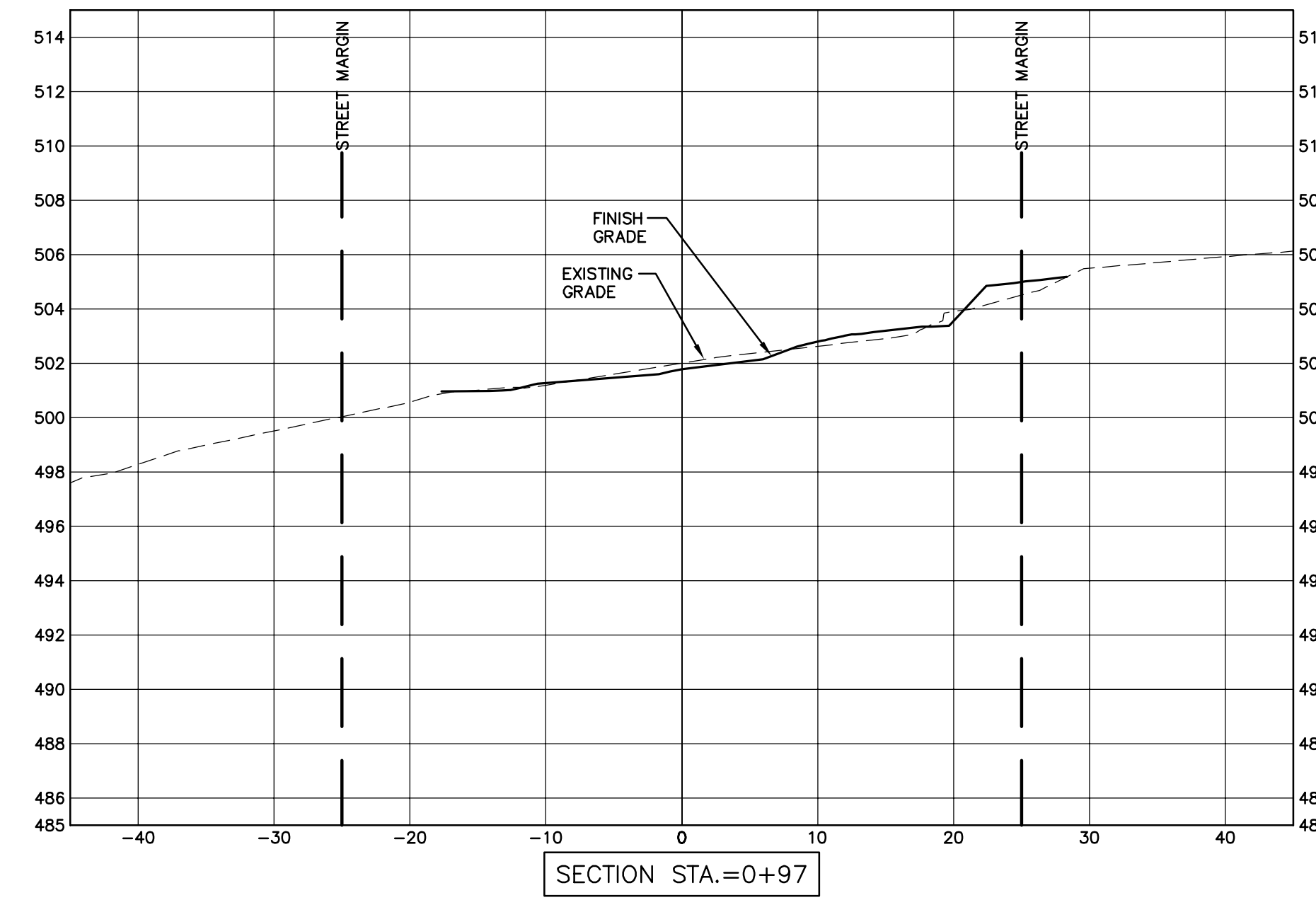
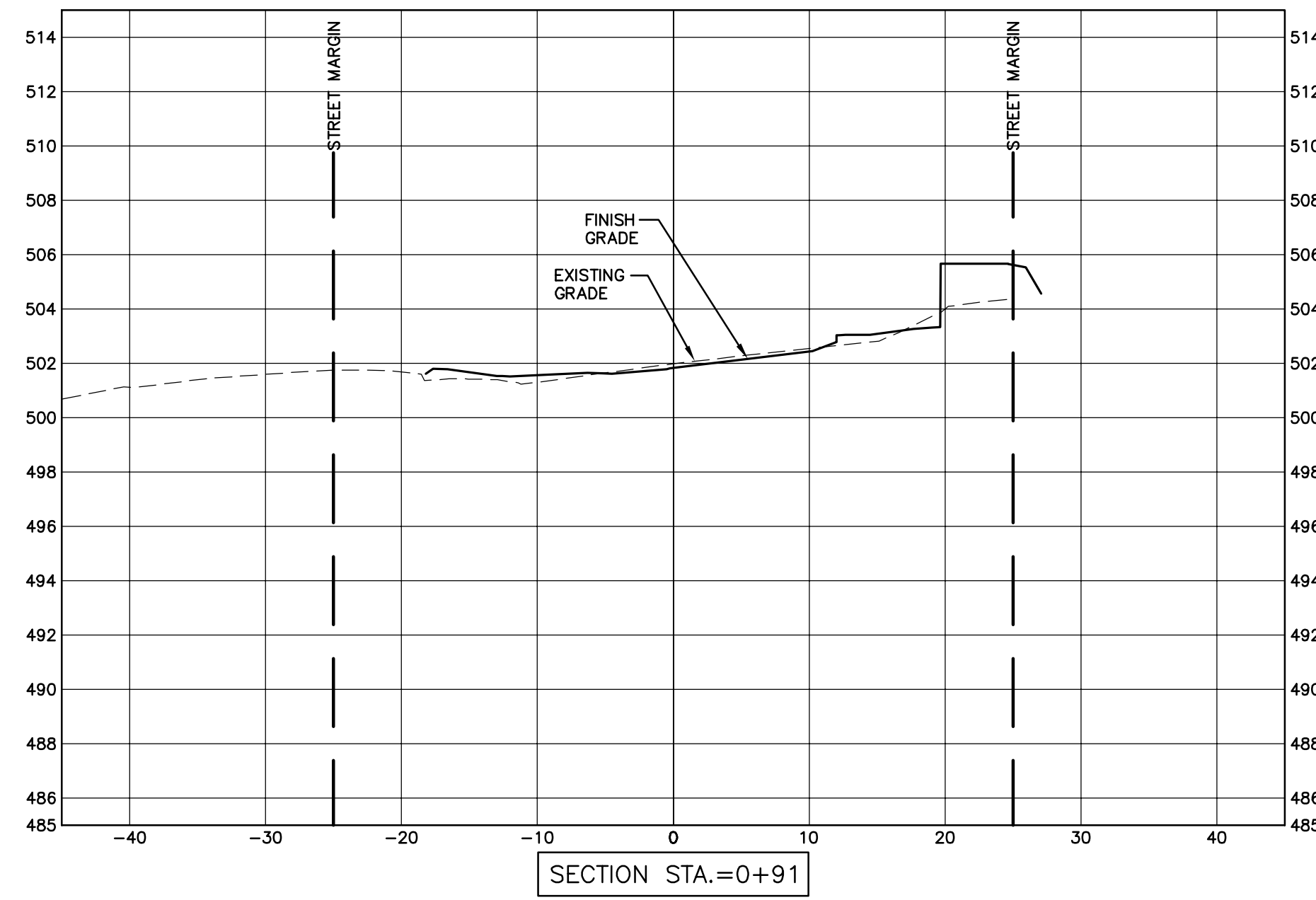
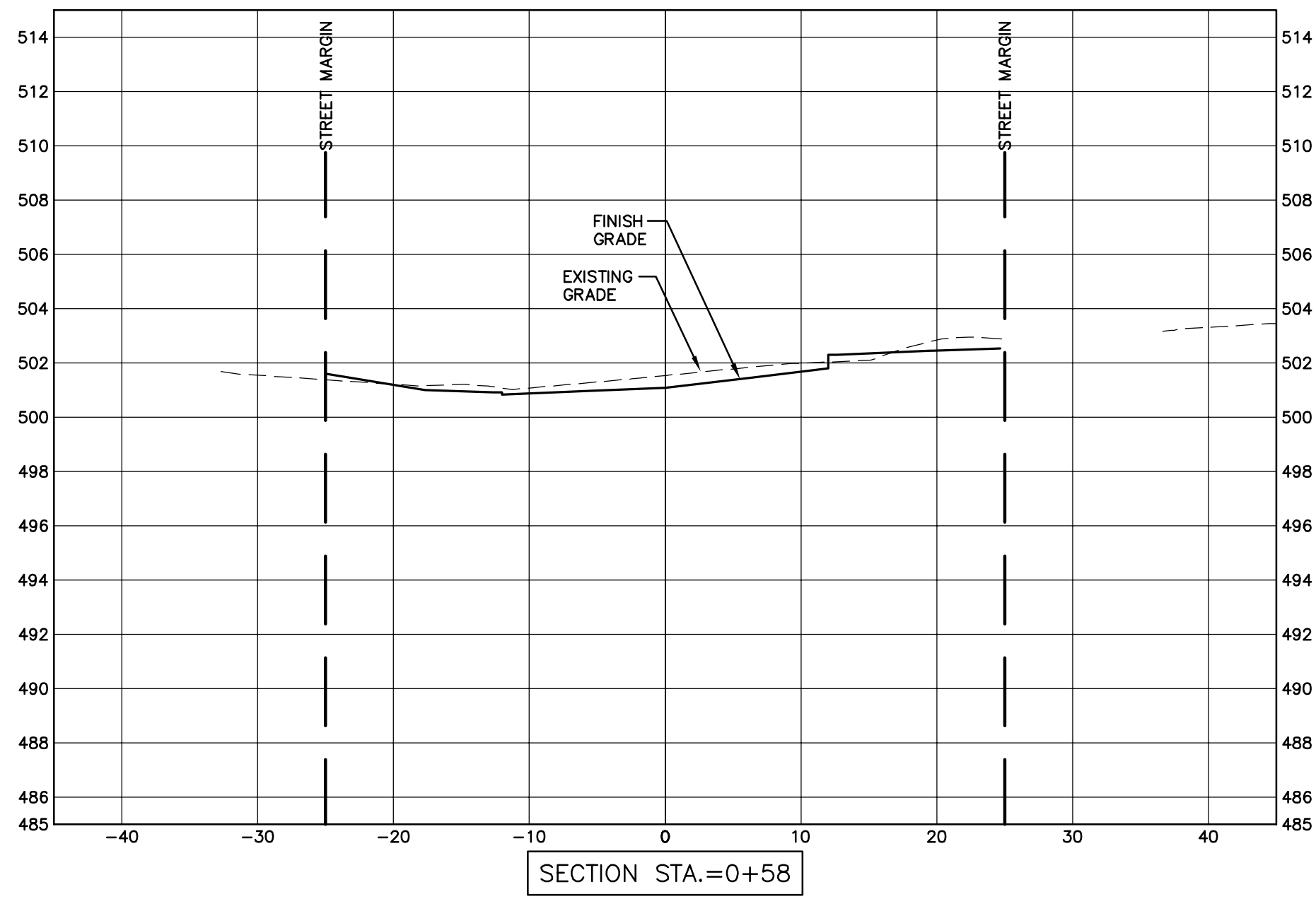


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	AS NOTED
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

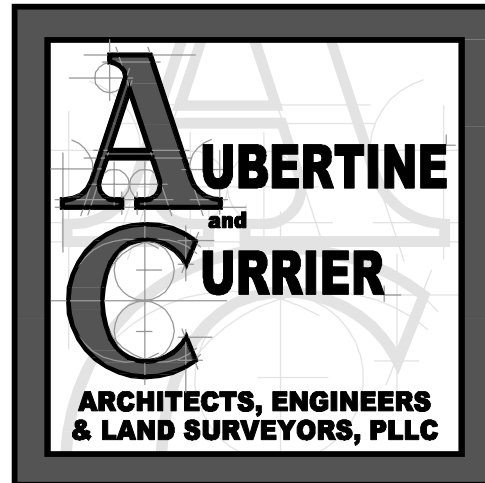
TYPICAL STREET
CROSS SECTIONS

CG-200



BURLINGTON STREET CROSS SECTIONS
 SCALE: HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

12/13/2024 BID DOCUMENTS
 FOR CONSTRUCTION

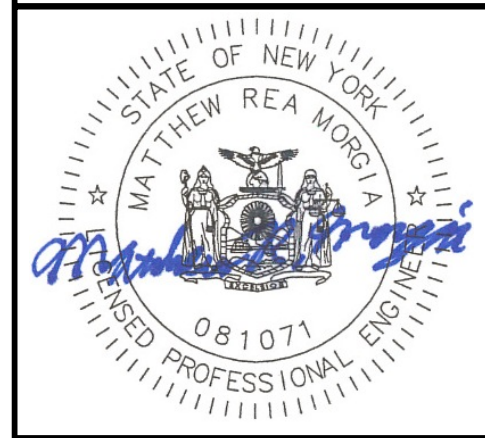


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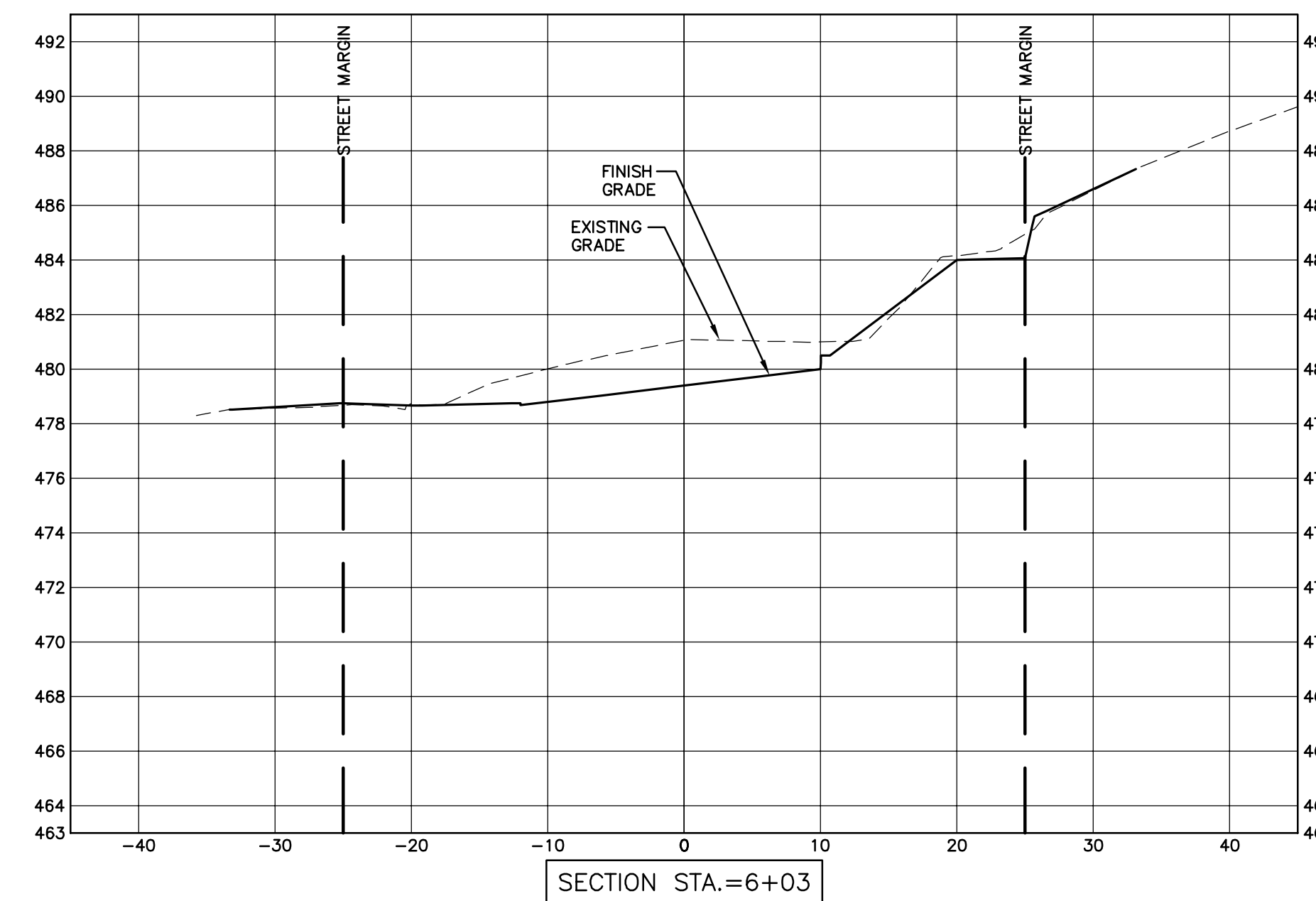
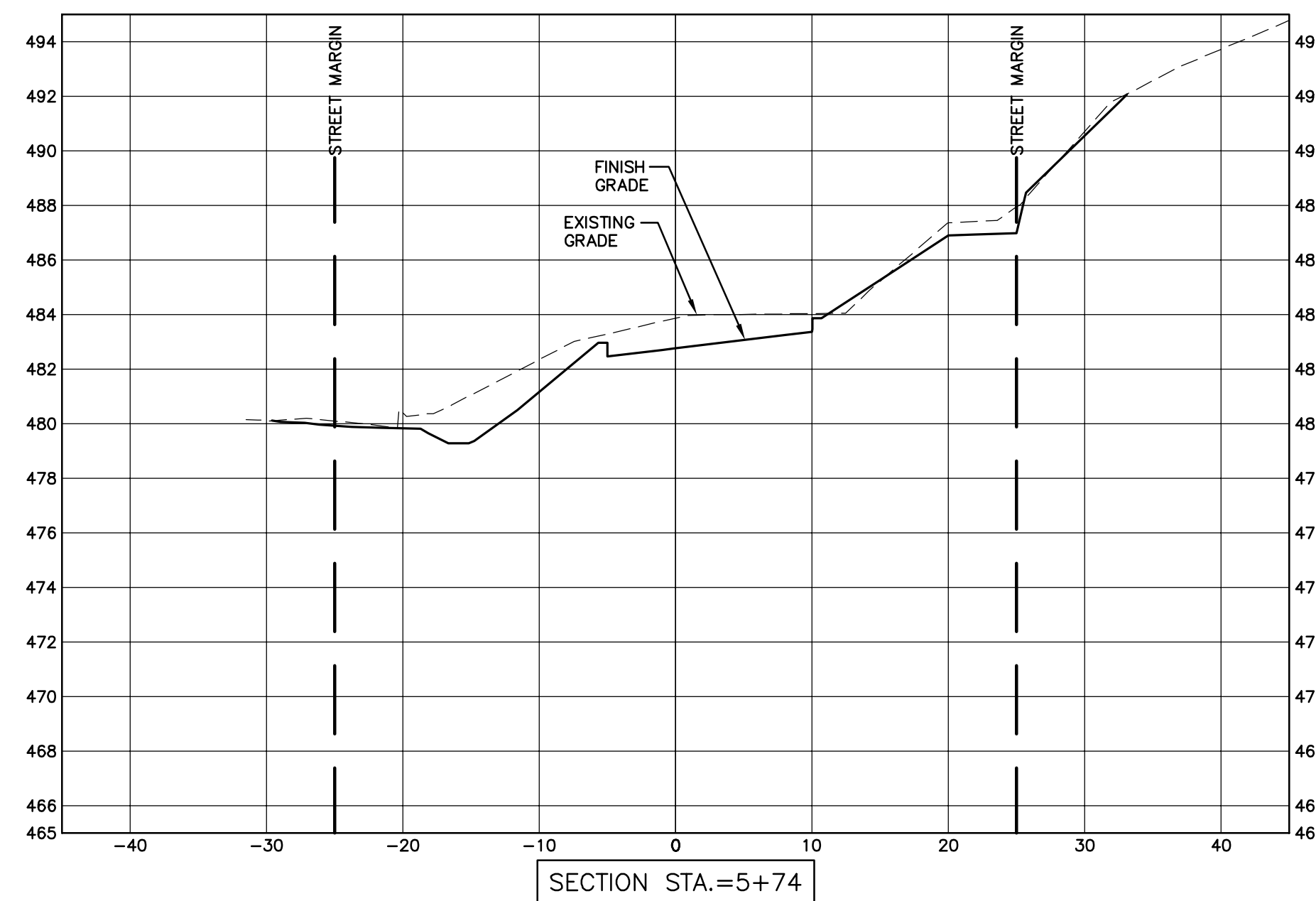
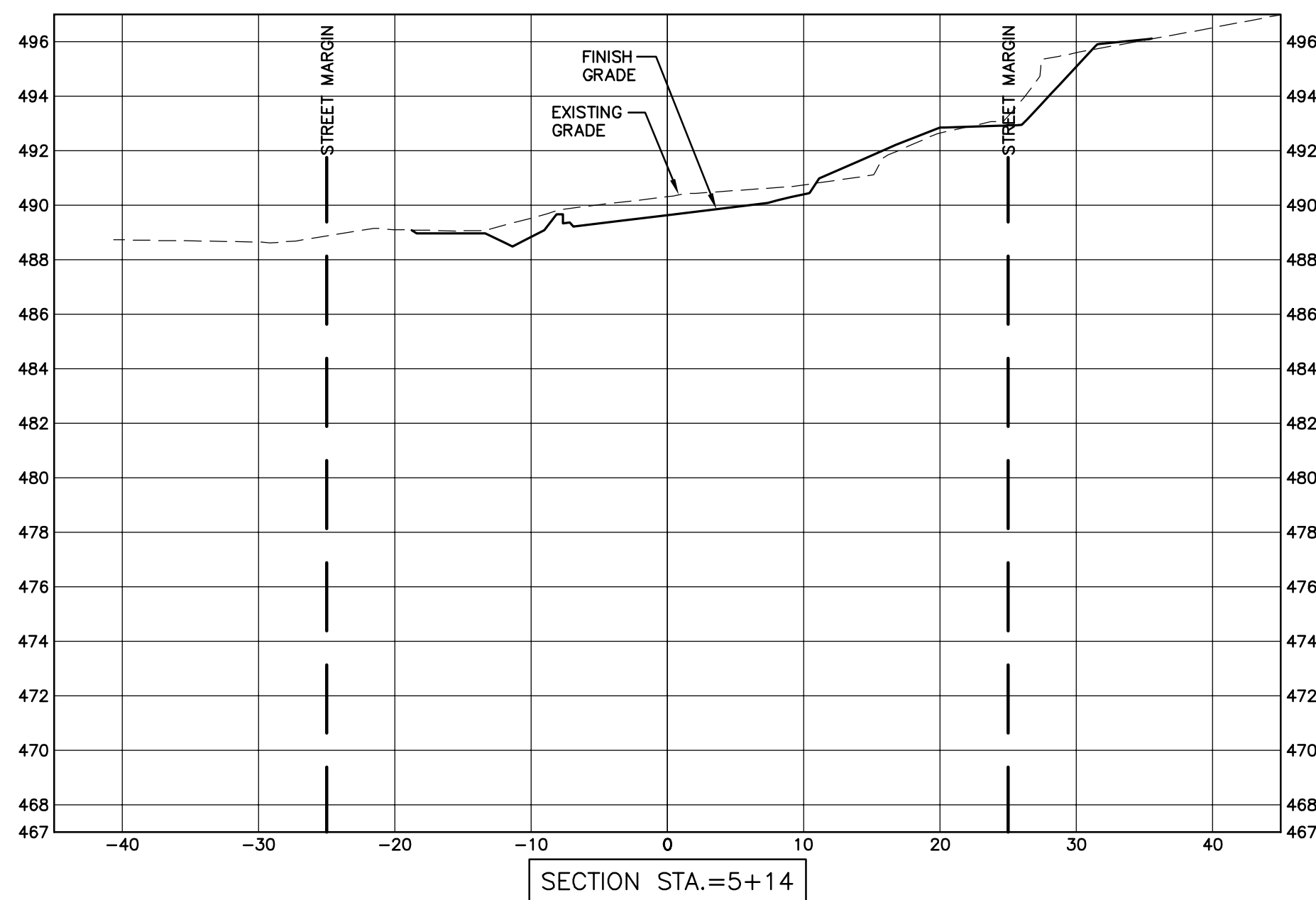
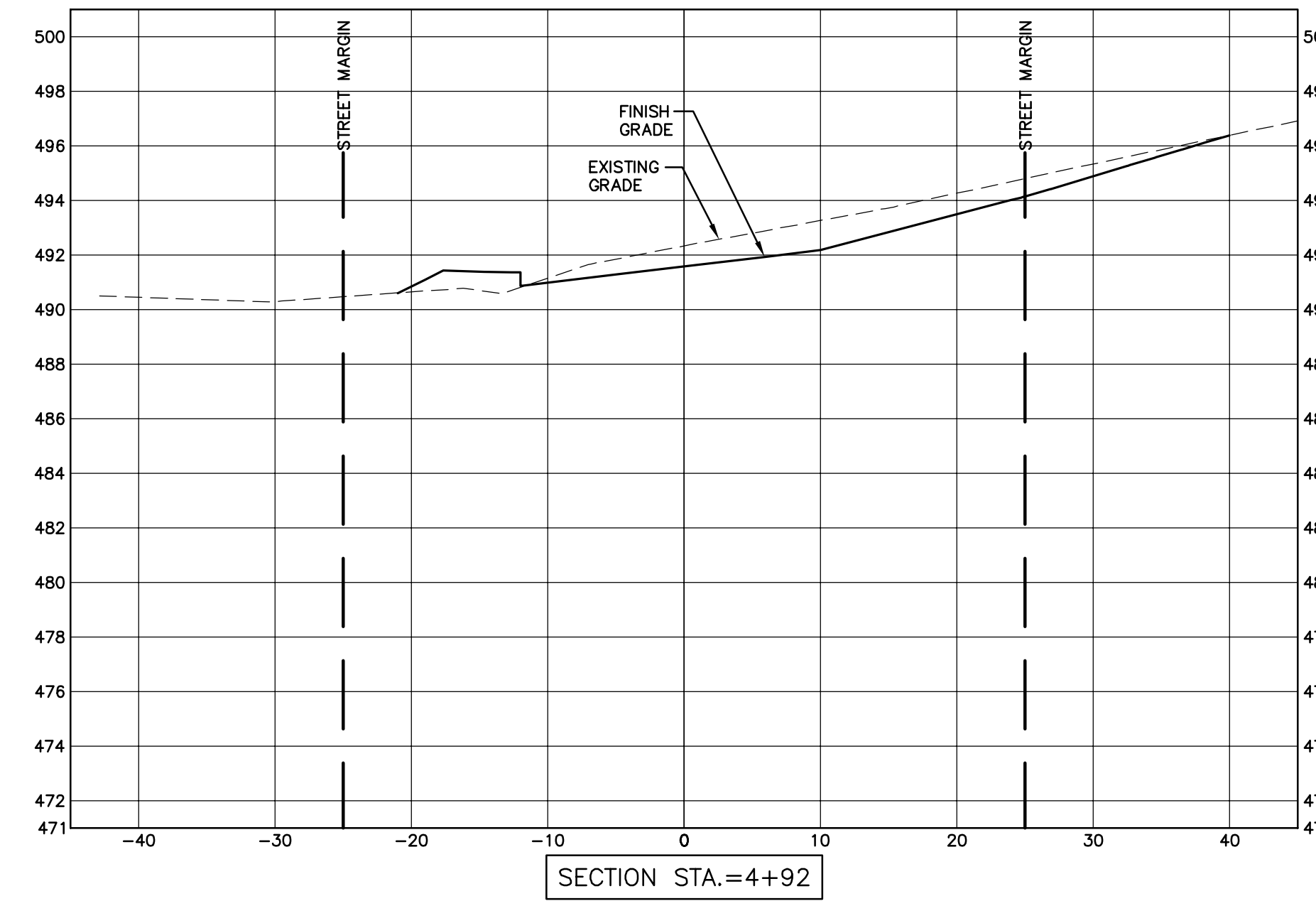
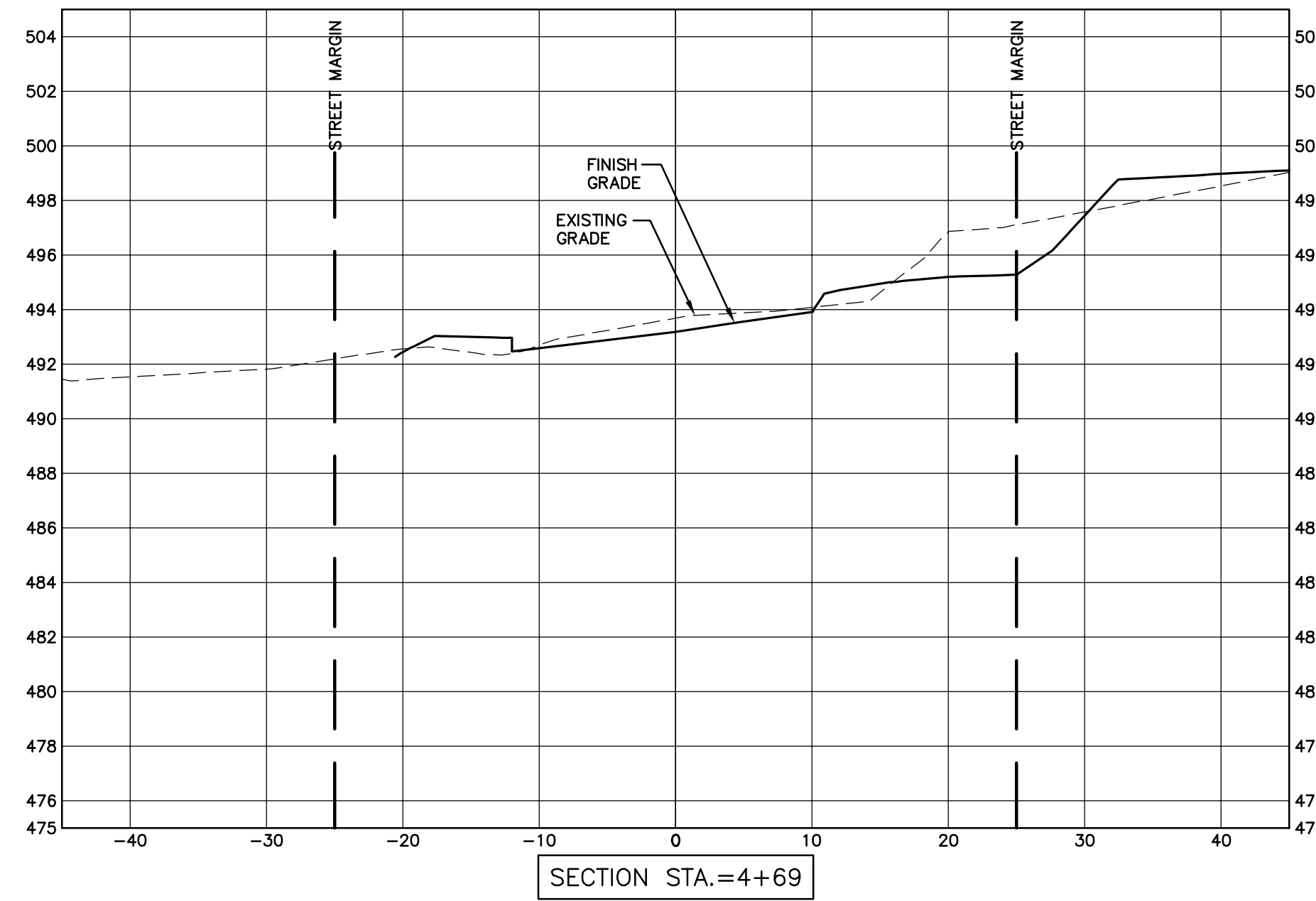
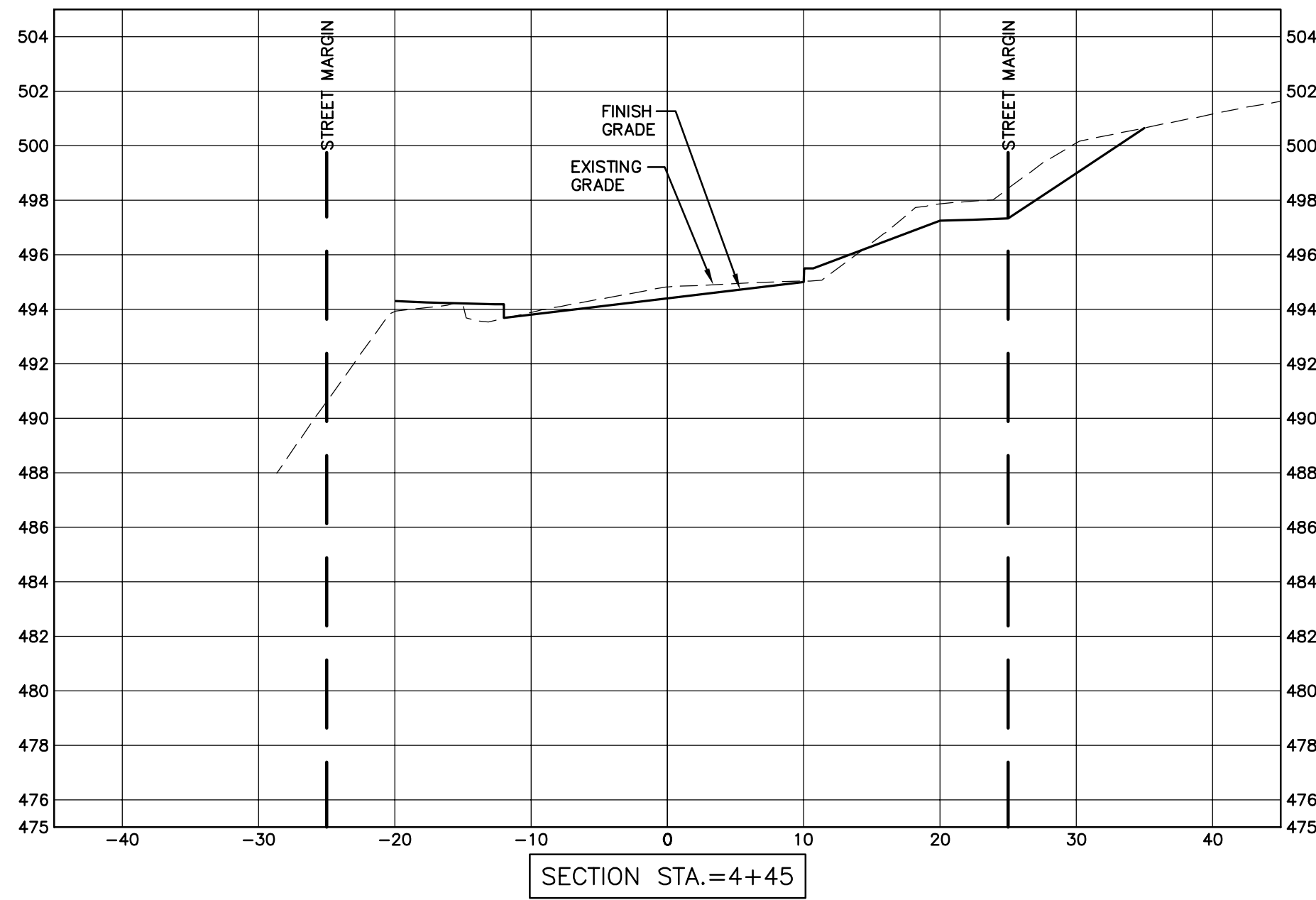
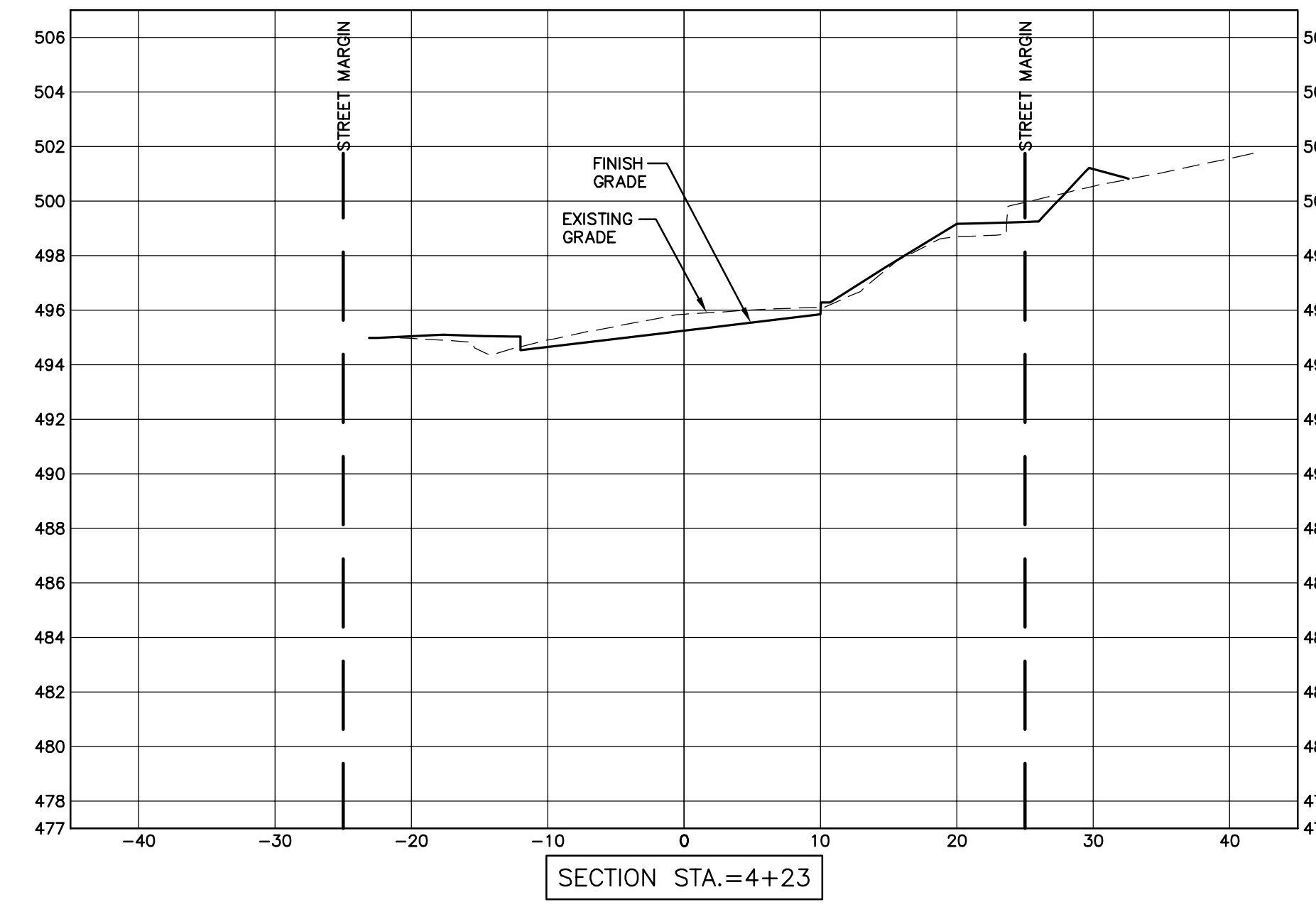
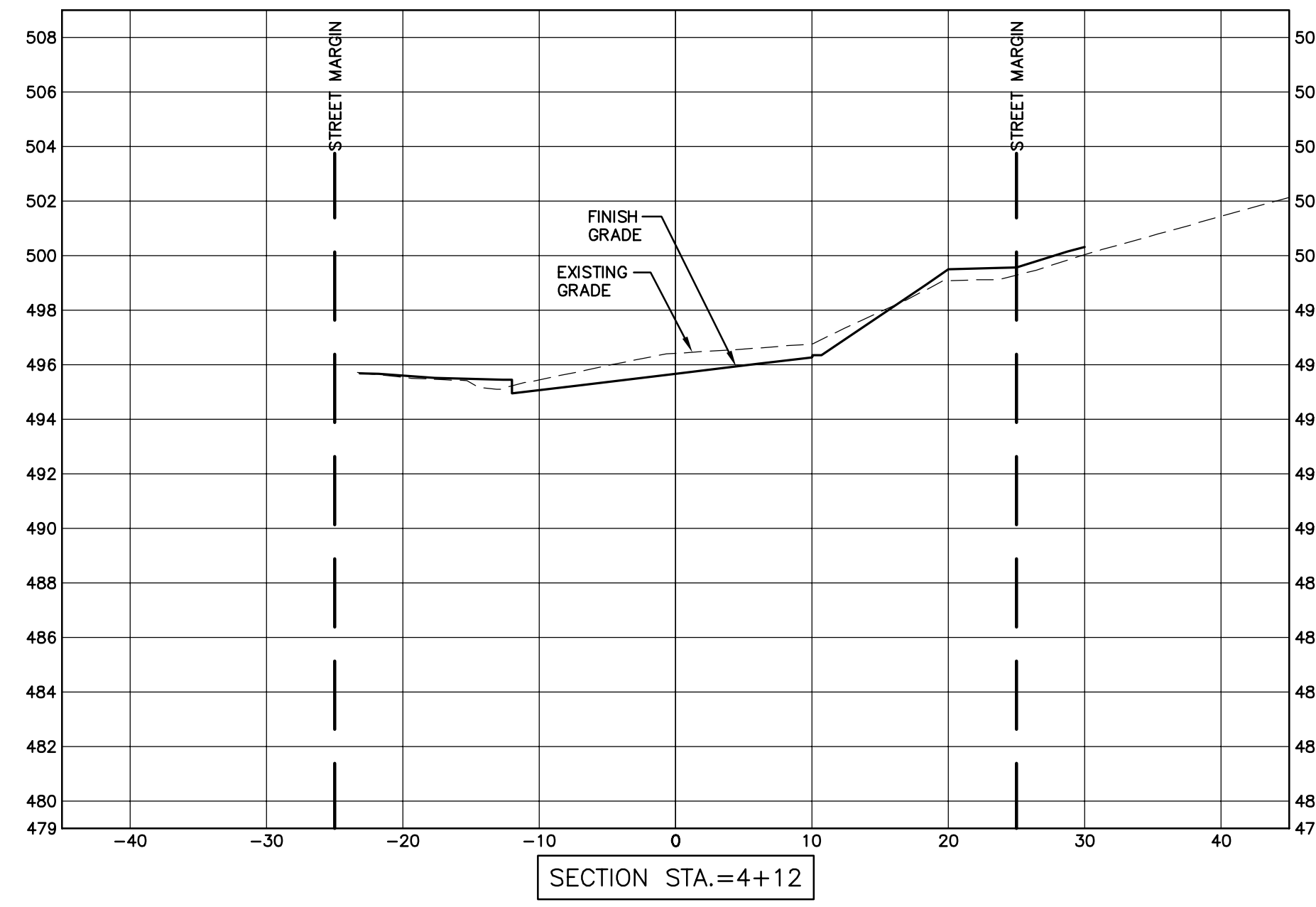
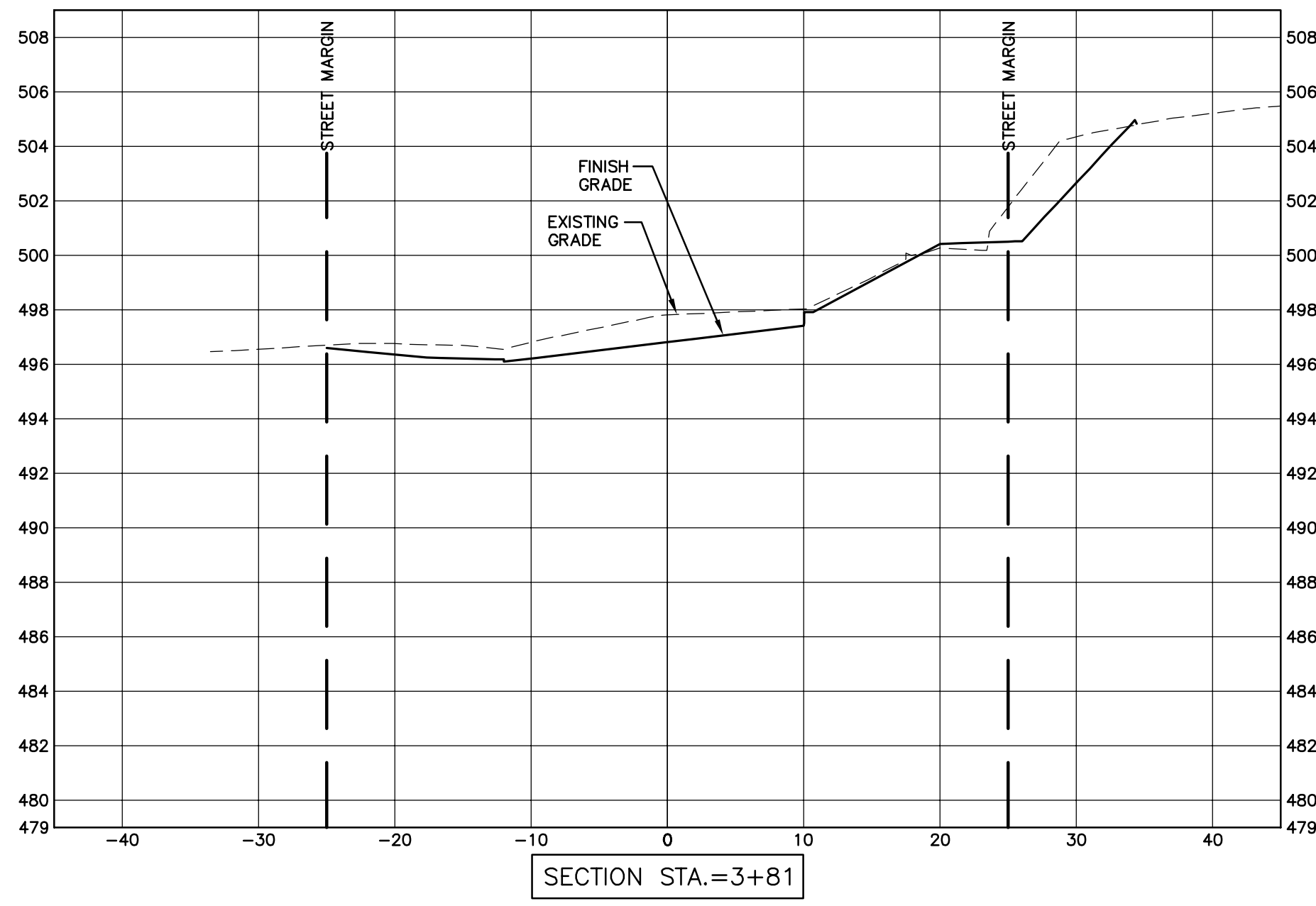


**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	AS NOTED
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

BURLINGTON STREET
 CROSS SECTIONS

CG-201



BURLINGTON STREET CROSS SECTIONS
 SCALE: HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

12/13/2024 BID DOCUMENTS
 FOR CONSTRUCTION

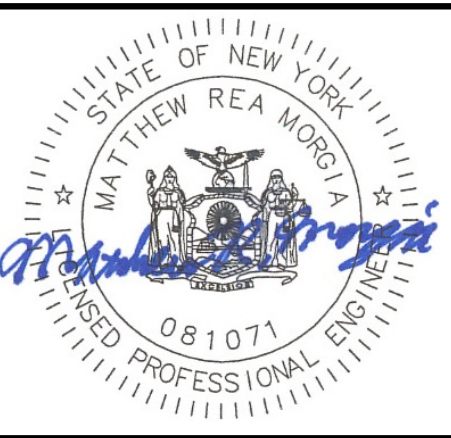


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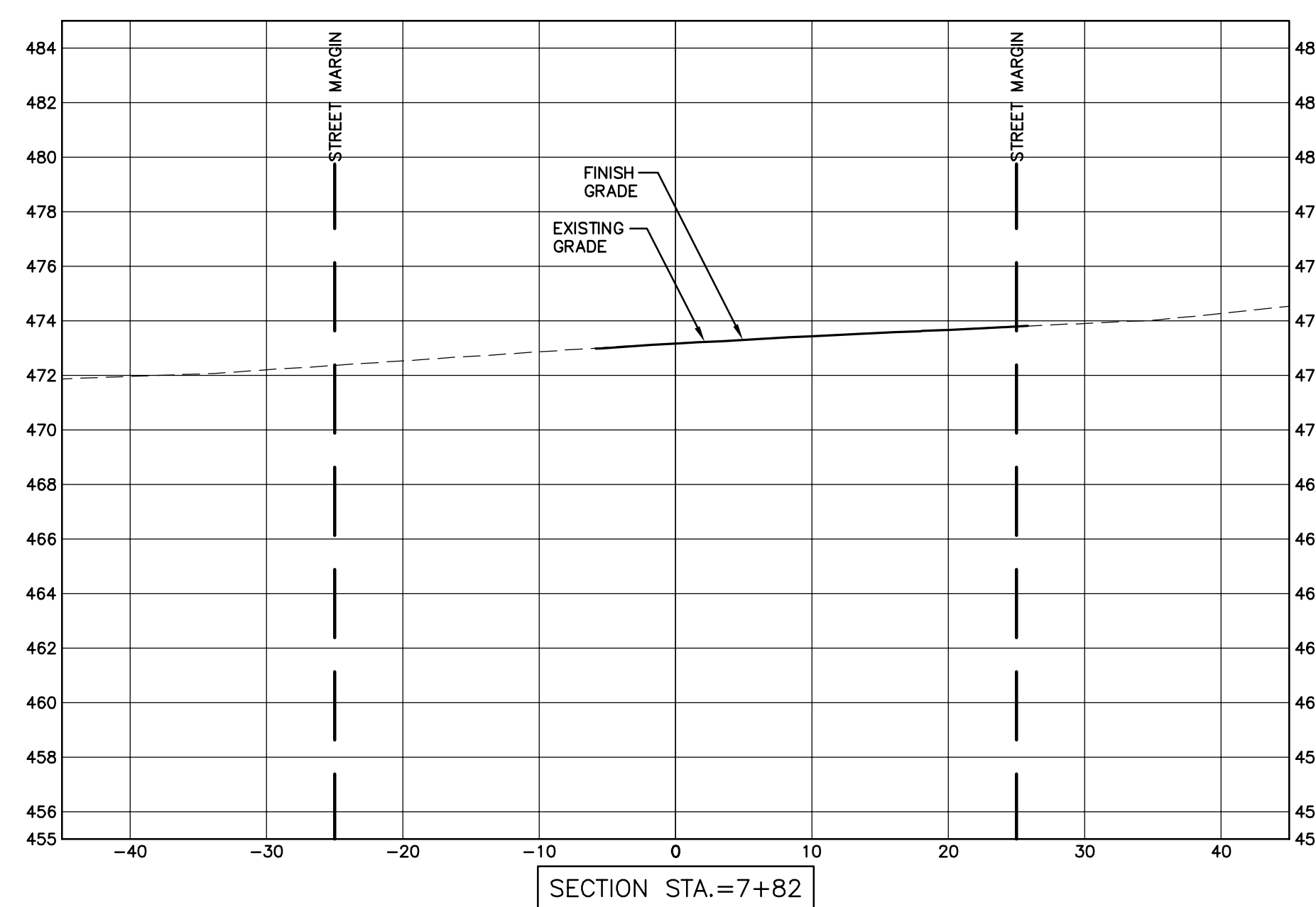
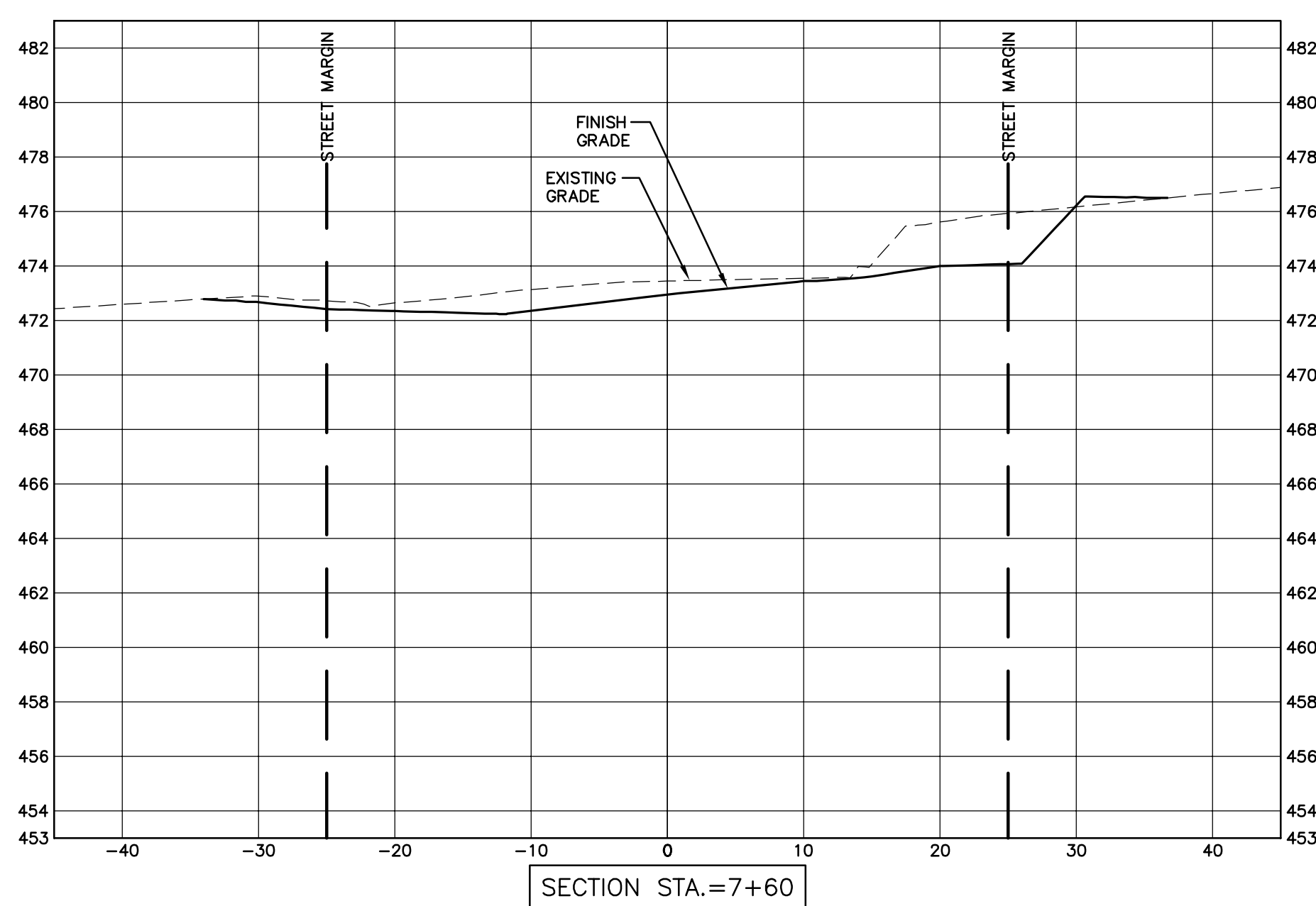
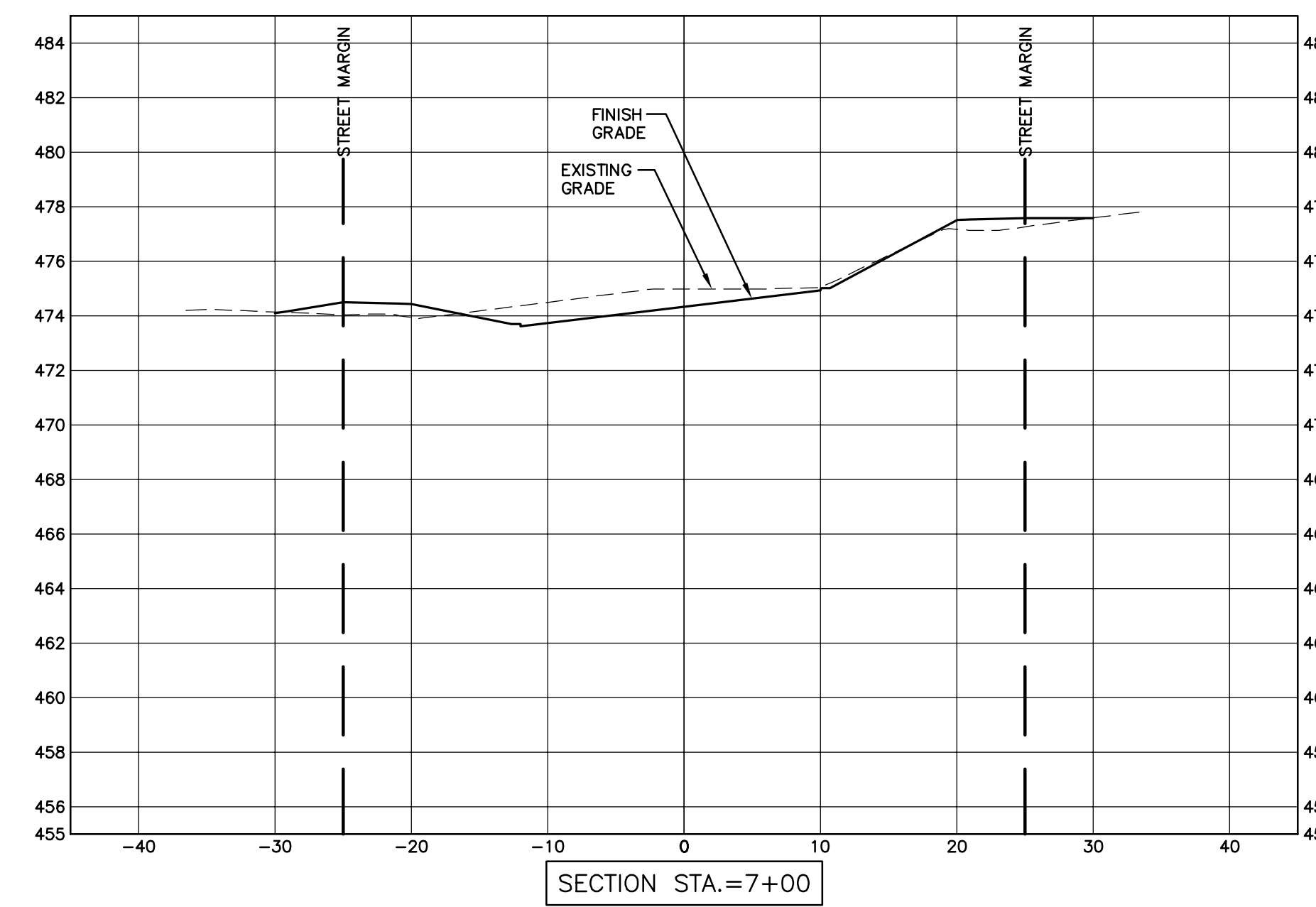
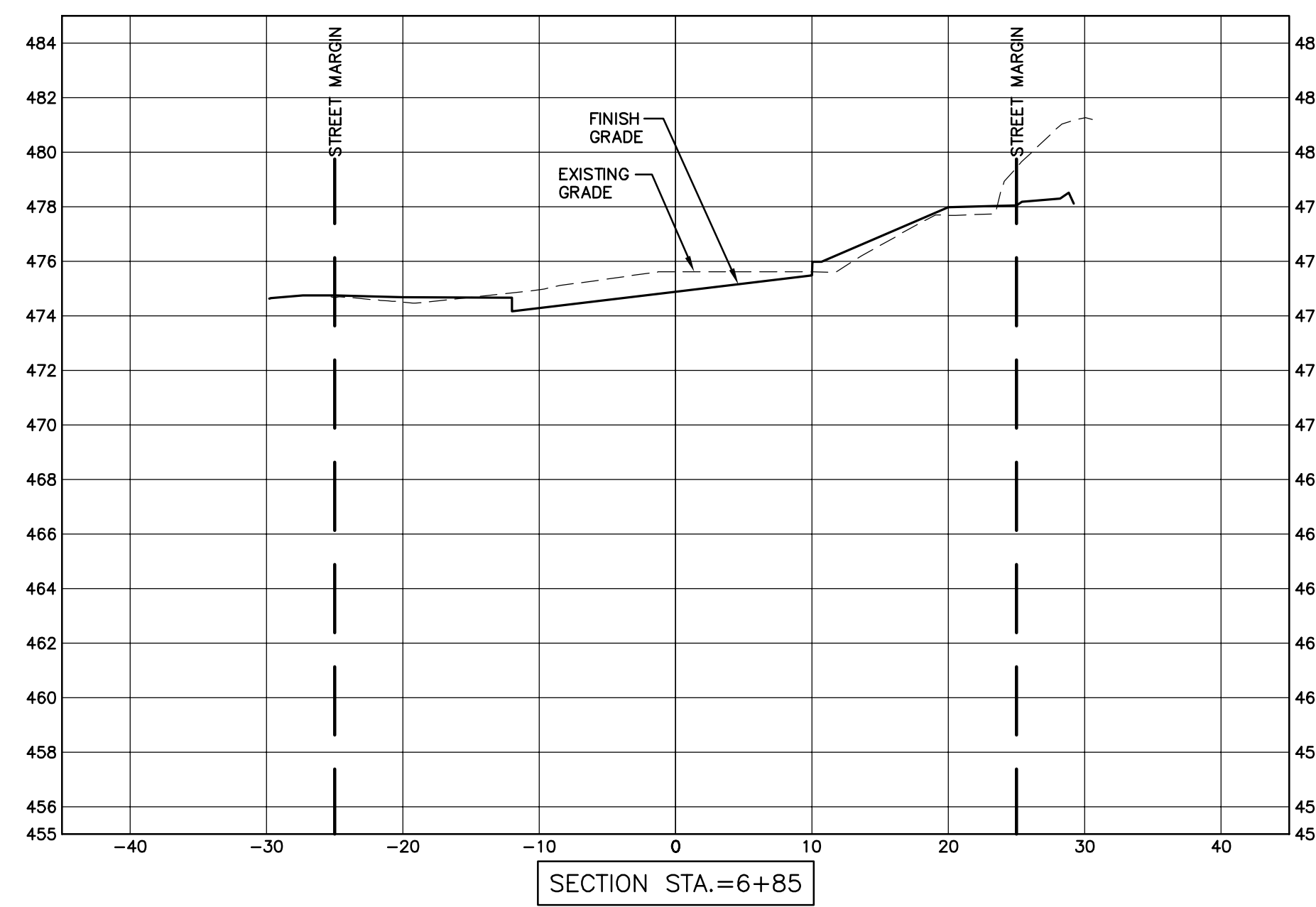
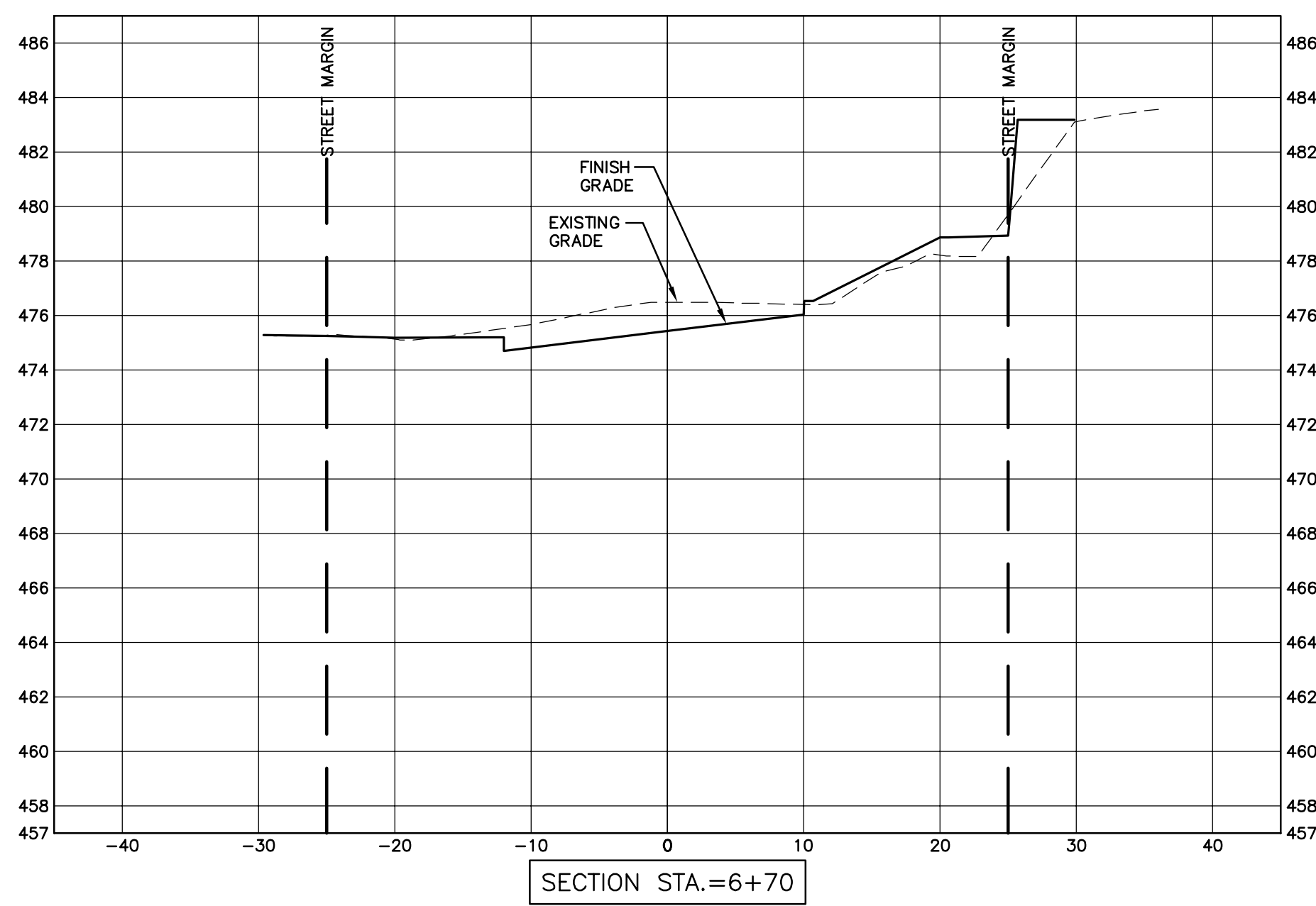
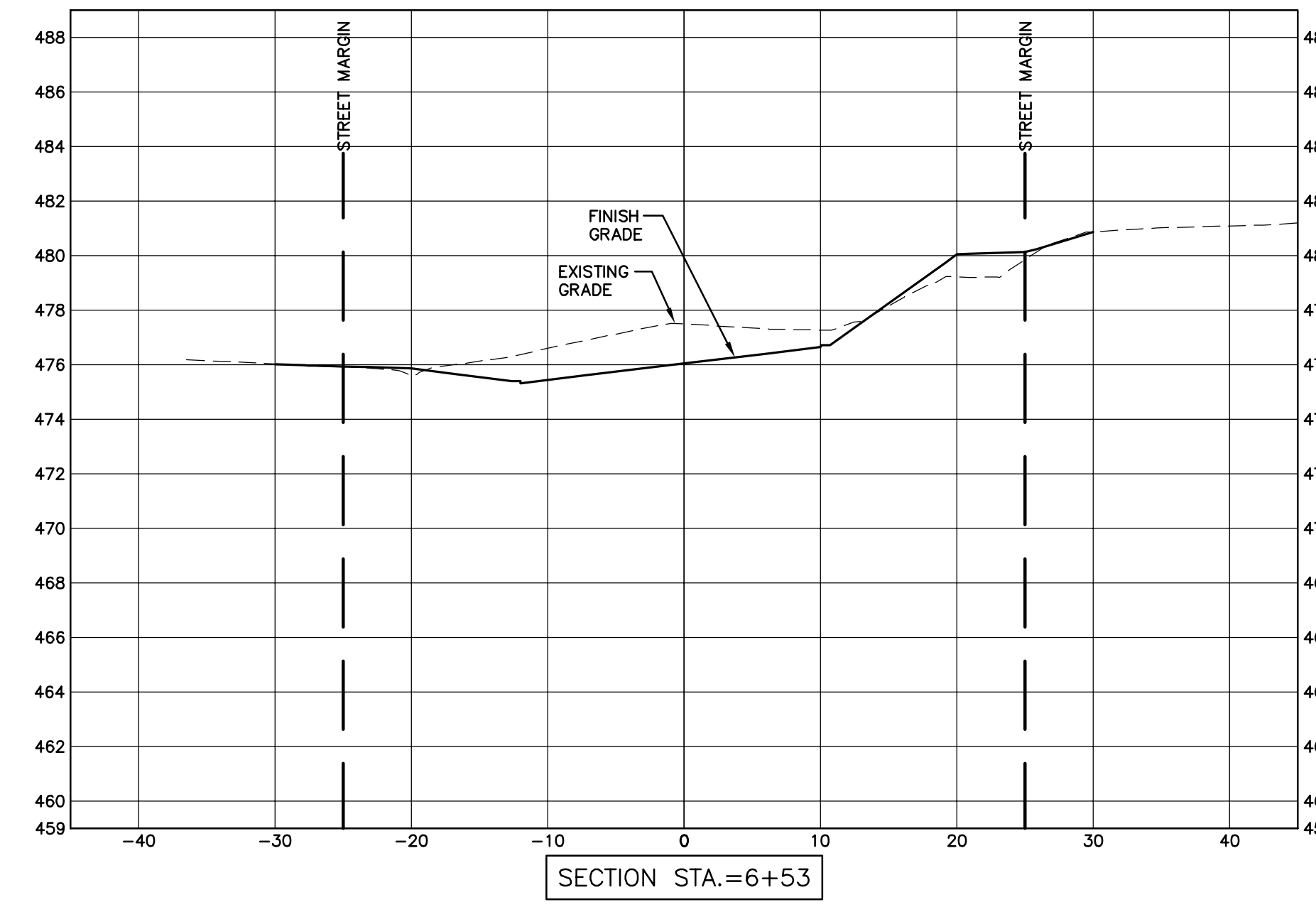
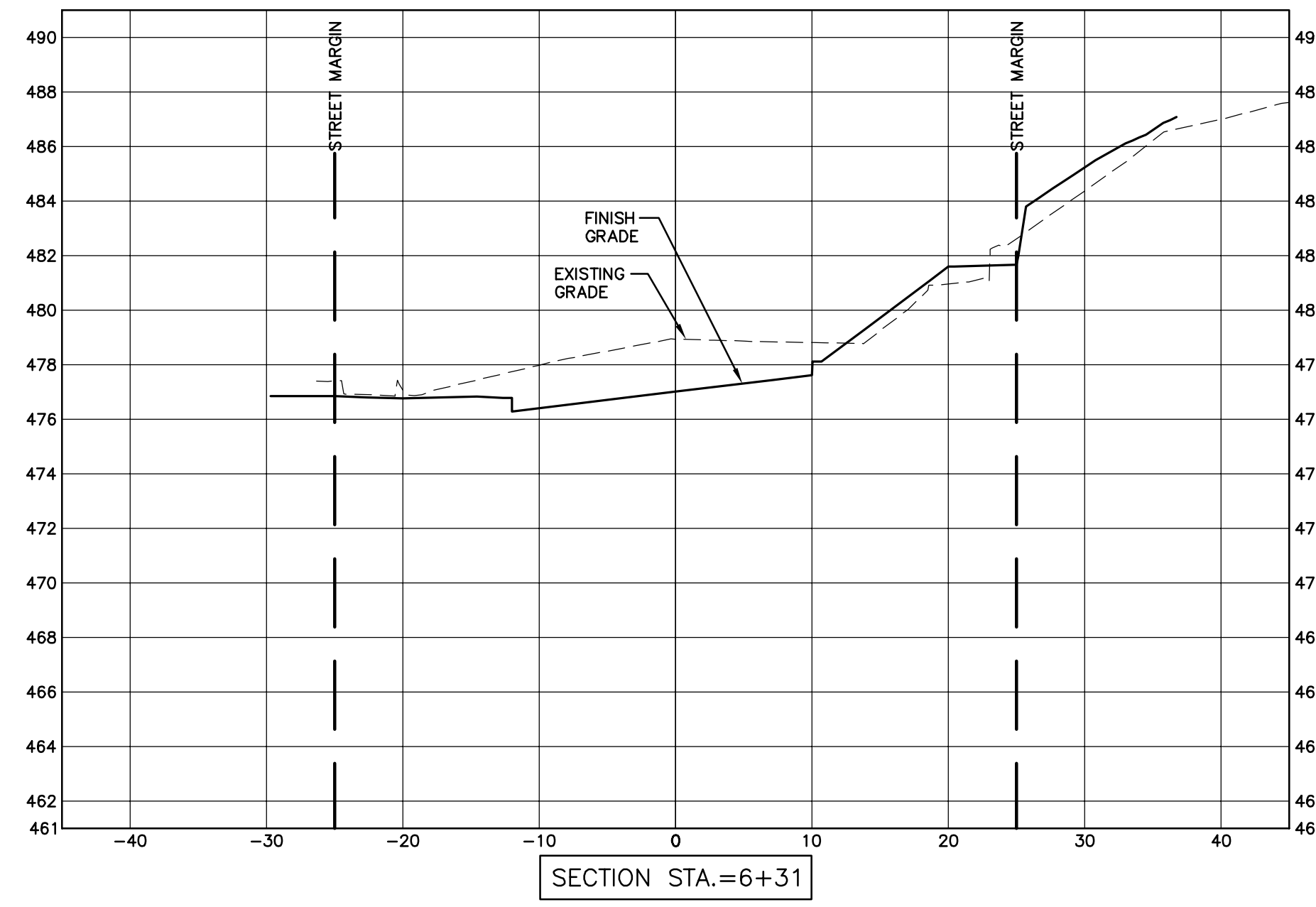
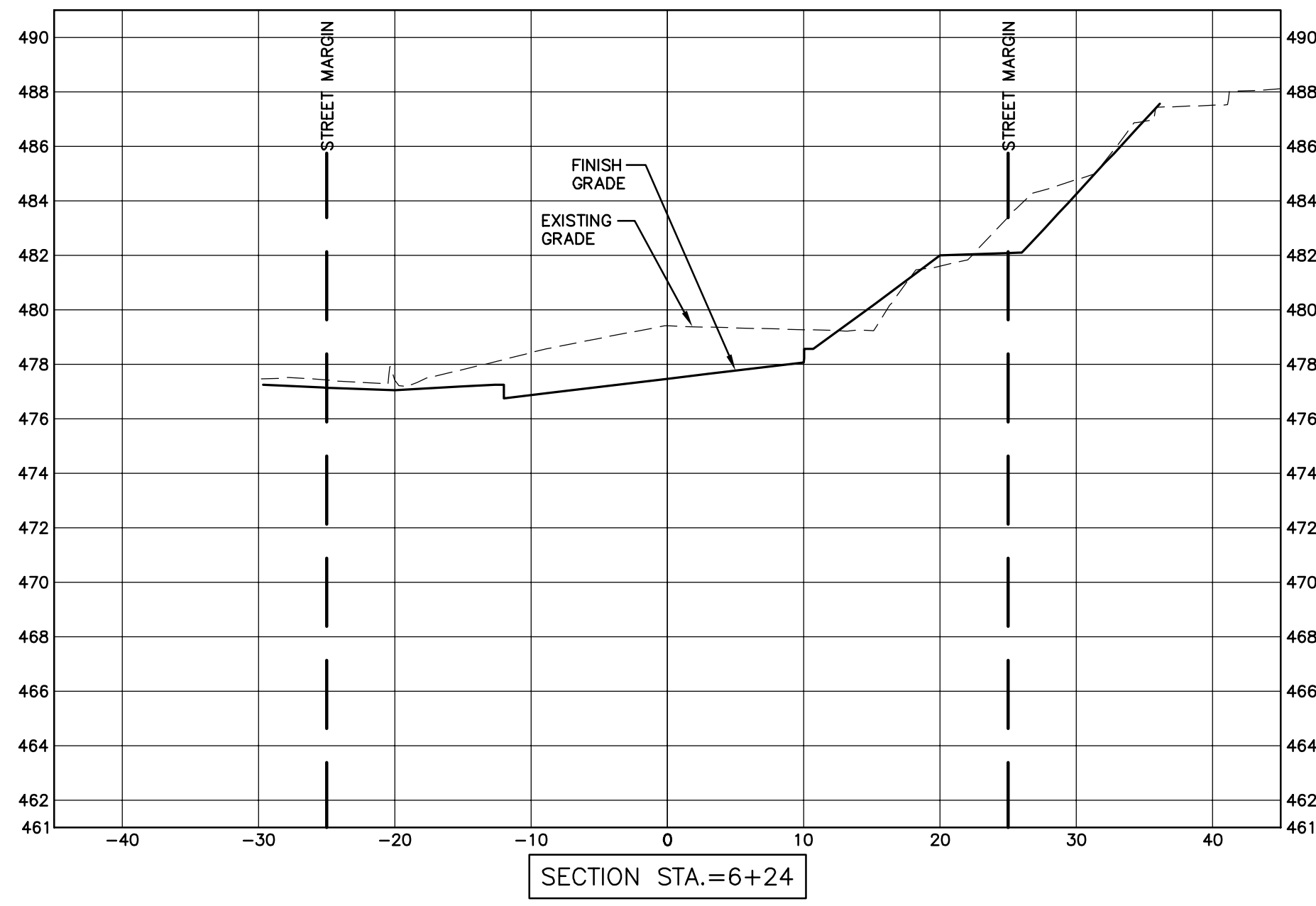


**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

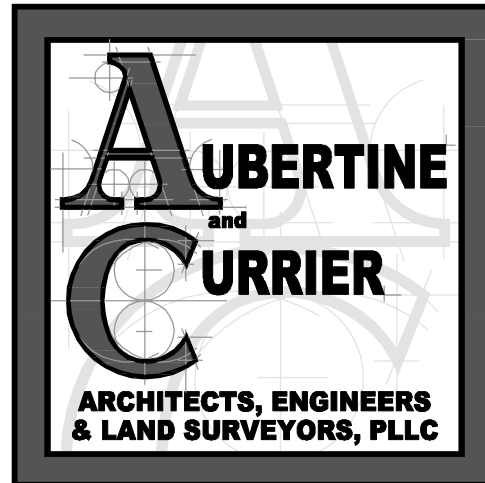
PROJECT NO:	2023-052
SCALE:	AS NOTED
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

BURLINGTON STREET
 CROSS SECTIONS

CG-202



BURLINGTON STREET CROSS SECTIONS
 SCALE: HORIZONTAL 1" = 10'
 VERTICAL 1" = 5'

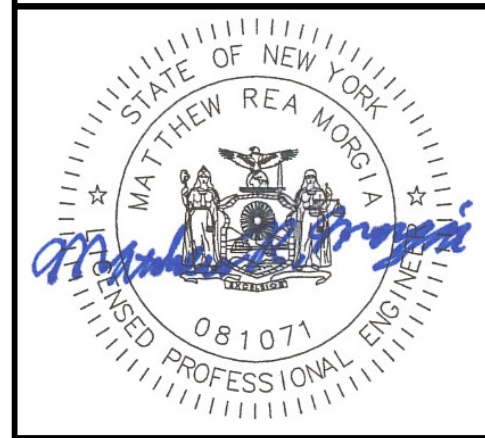


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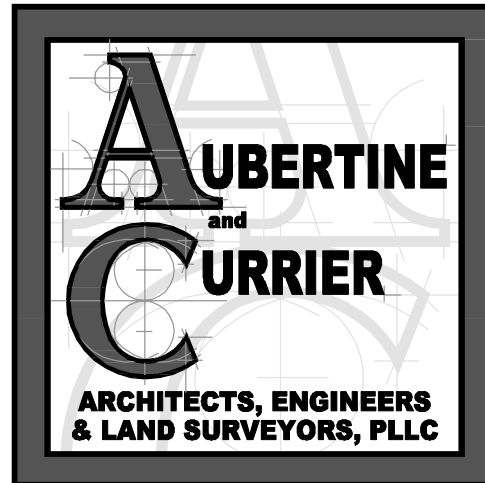
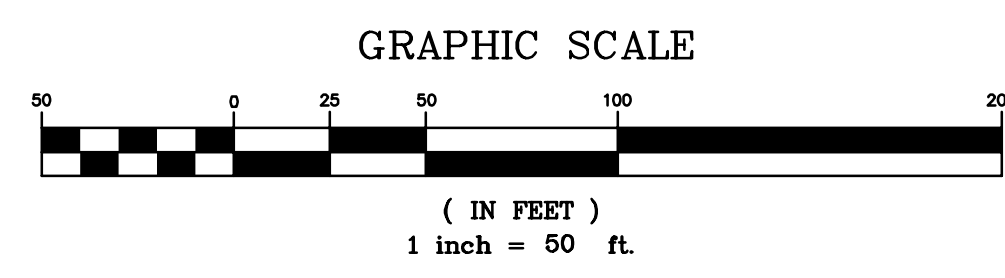
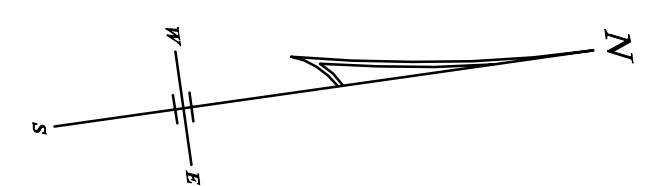
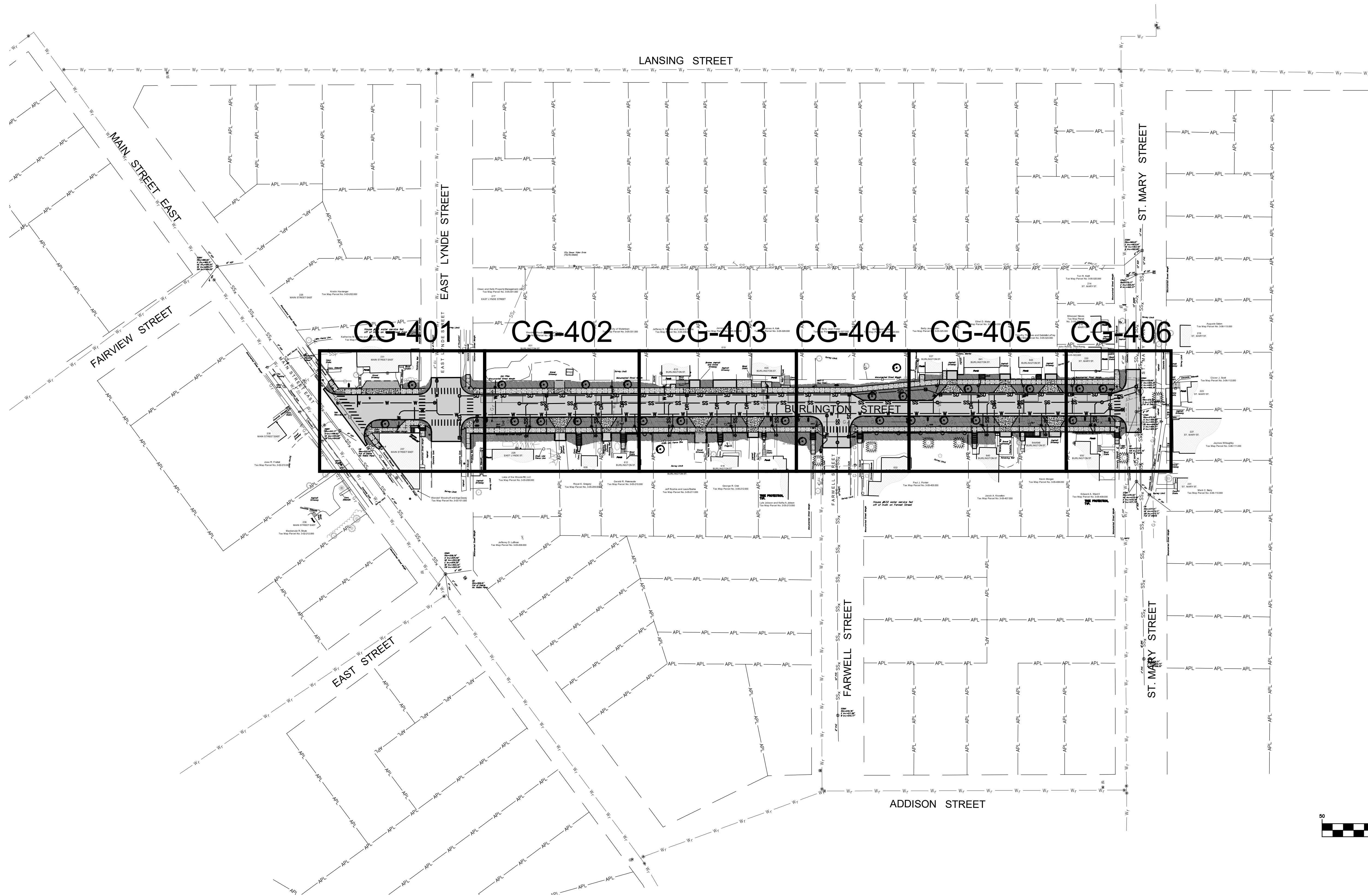


**BURLINGTON STREET
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DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

BURLINGTON STREET
 CROSS SECTIONS

CG-203

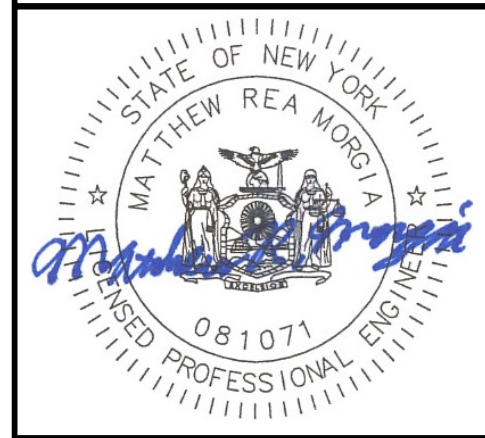


NYS WBE/DBE Certified
SBA Woman Owned
Small Business (WOSB)

aubertinecurrier.com

522 Bradley Street
Watertown, New York 13601
Phone: (315)782-2005

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**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=50'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

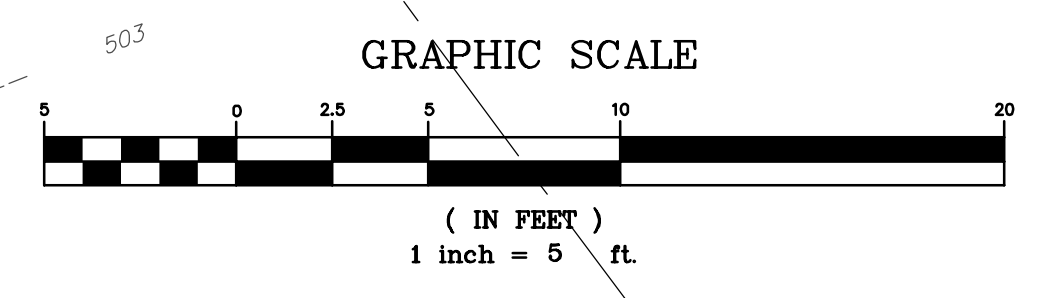
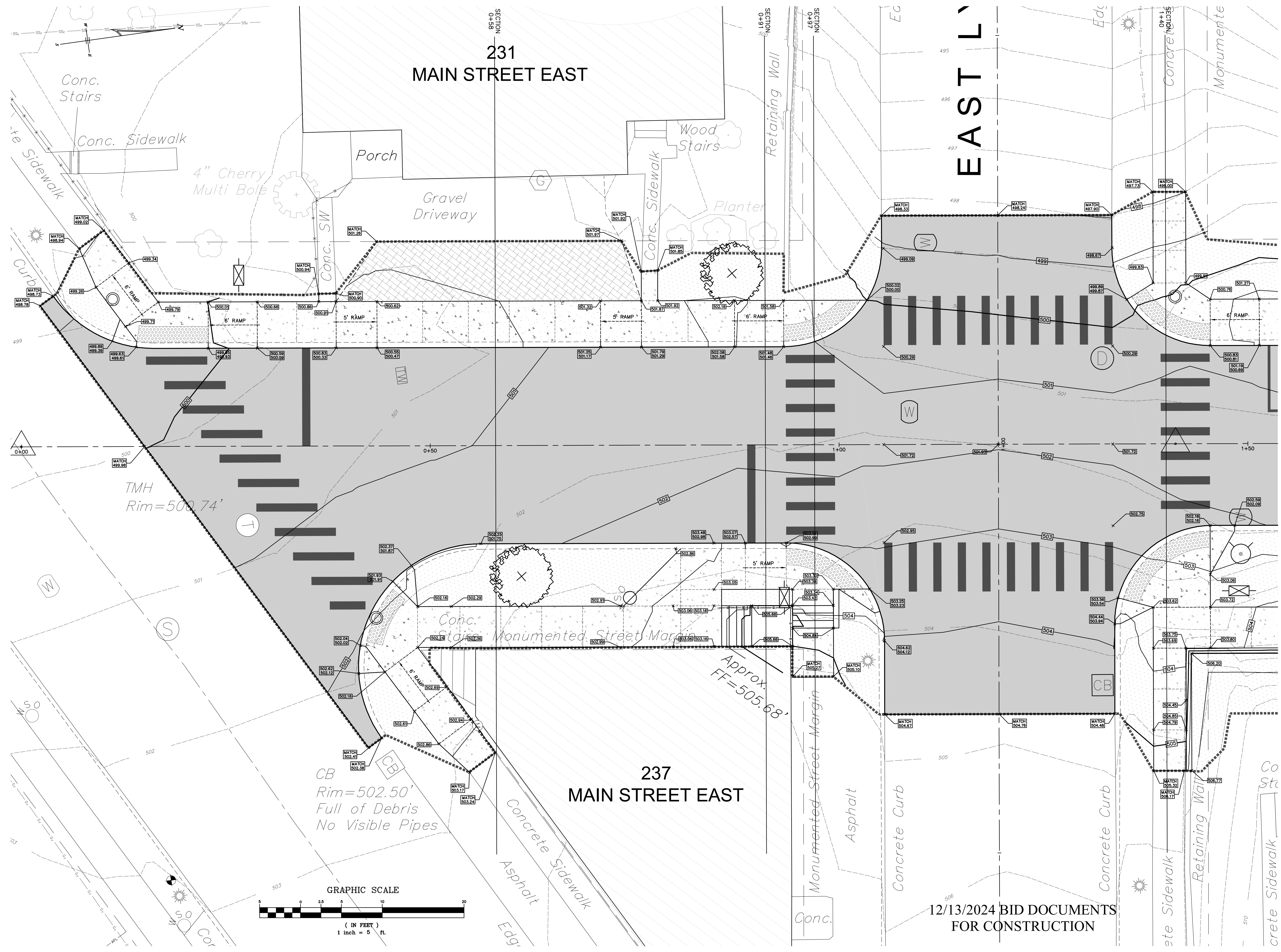
OVERALL
ENLARGED GRADING PLAN
KEY MAP

CG-400

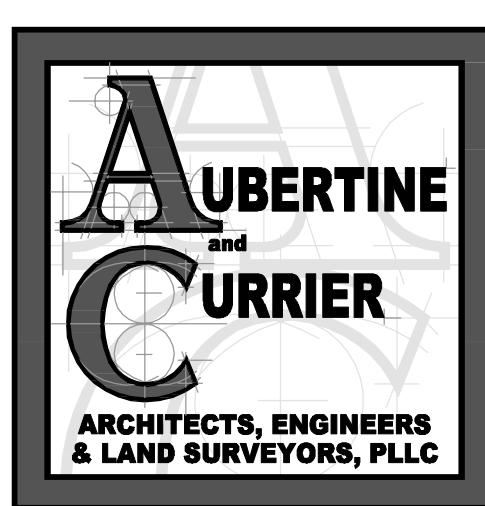
231
MAIN STREET EAST

237
MAIN STREET EAST

EAST L'



MATCH LINE - REFER TO SHEET CG-402

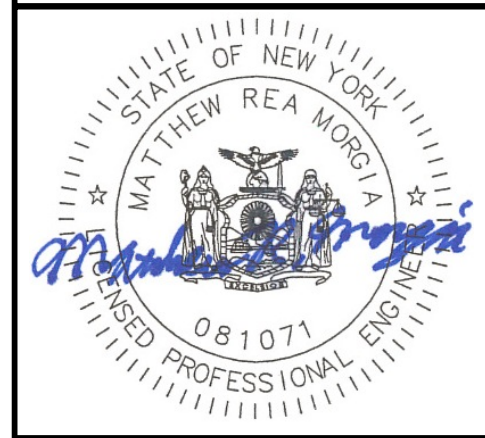


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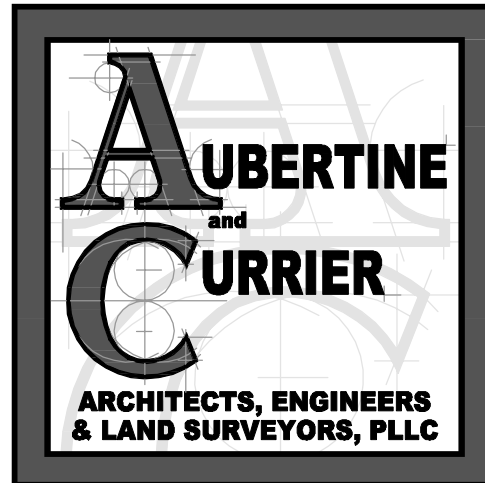
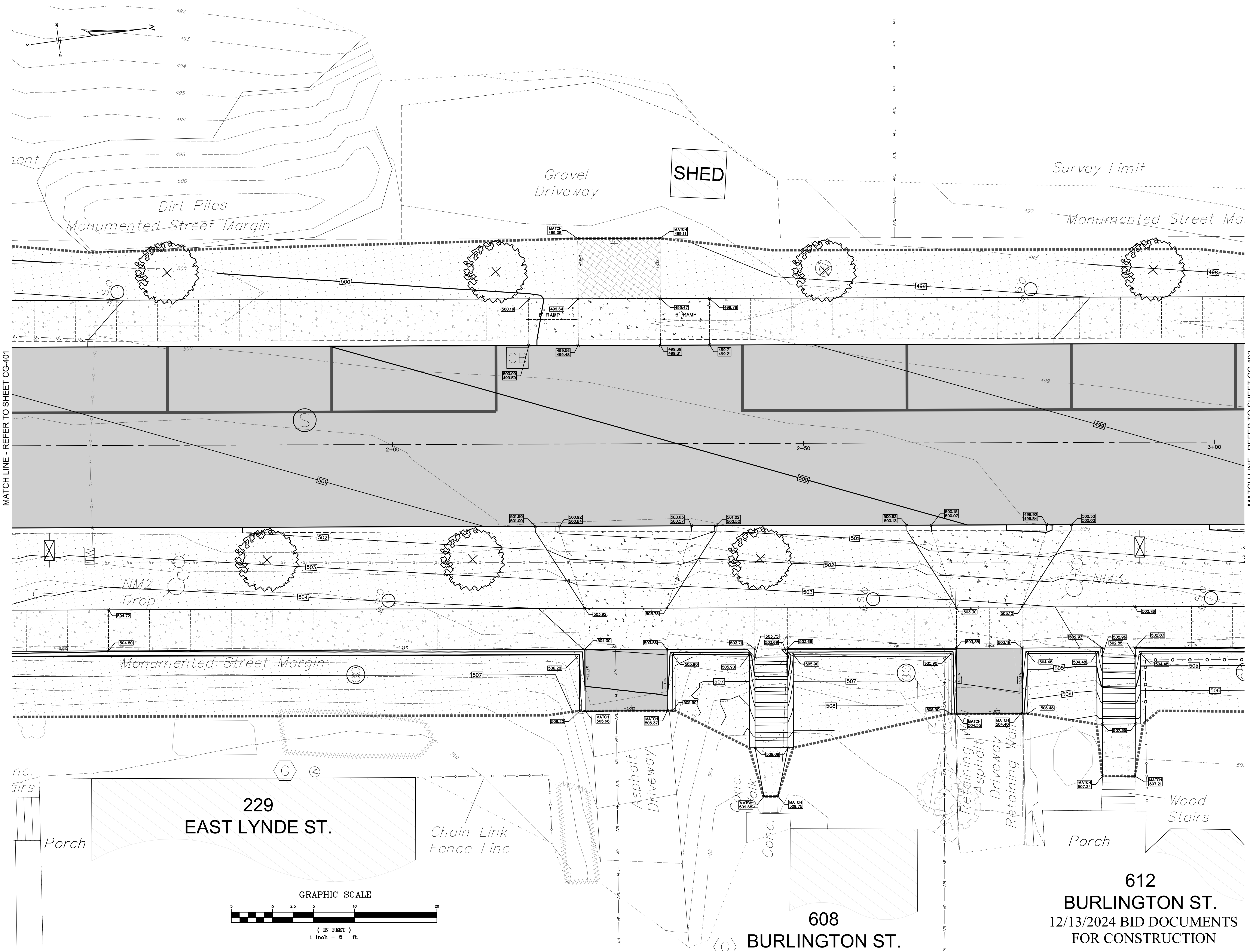
**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=5'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

ENLARGED GRADING PLAN

CG-401

12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION

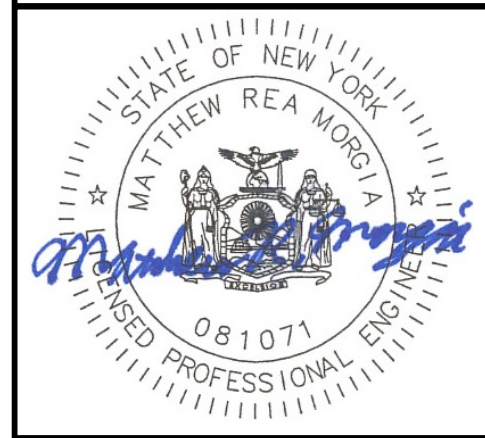


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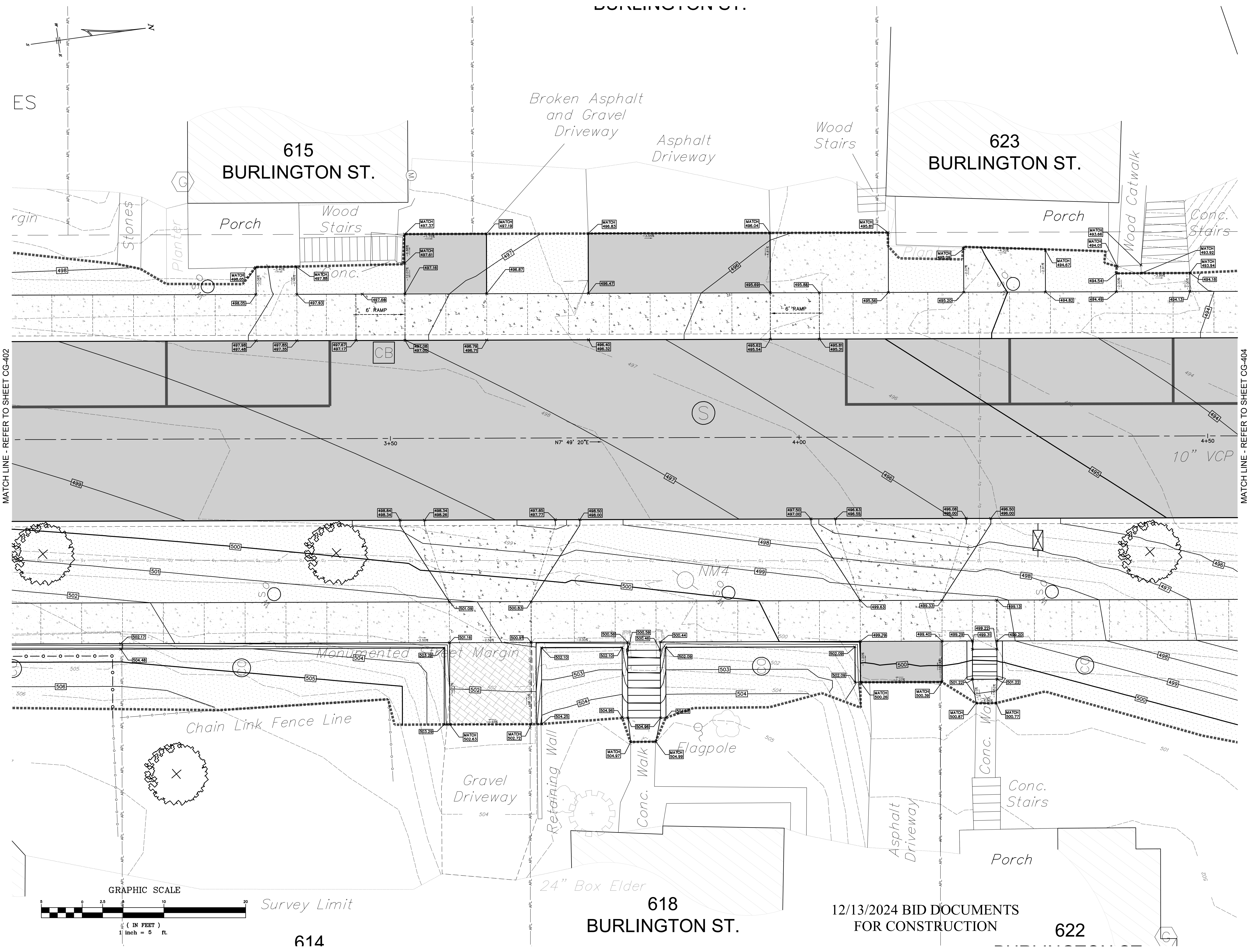
**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=5'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

ENLARGED GRADING PLAN

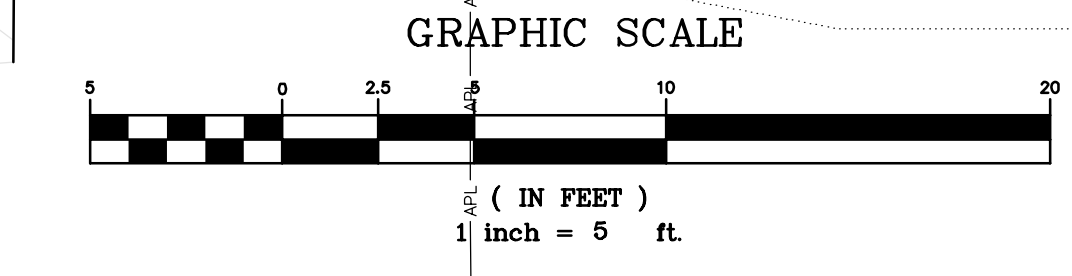
CG-402

612
BURLINGTON ST.
12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



MATCH LINE - REFER TO SHEET CG-402

MATCH LINE - REFER TO SHEET CG-404



Survey Limit

614

618 BURLINGTON ST.

12/13/2024 BID DOCUMENTS FOR CONSTRUCTION

622

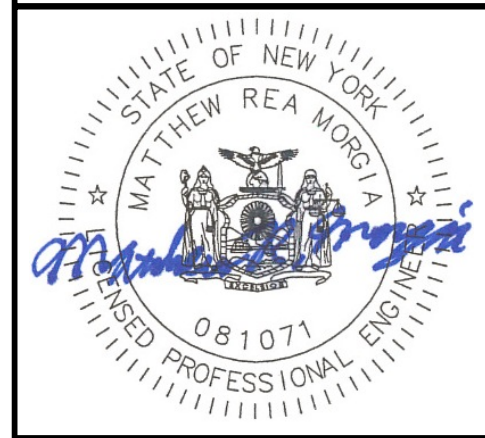


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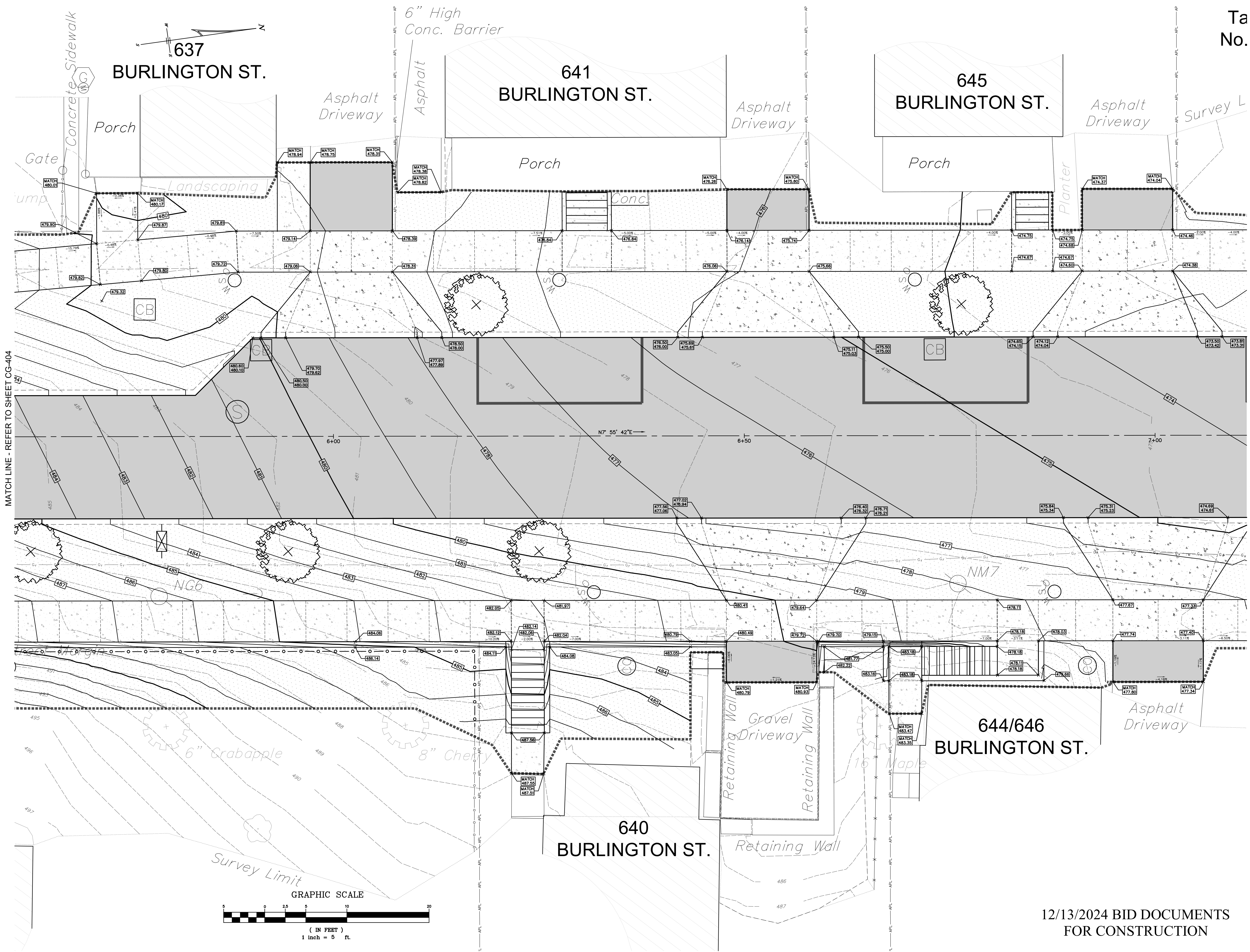


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	1"=20'
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/03/2024 12/13/2024

ENLARGED GRADING PLAN

CG-403



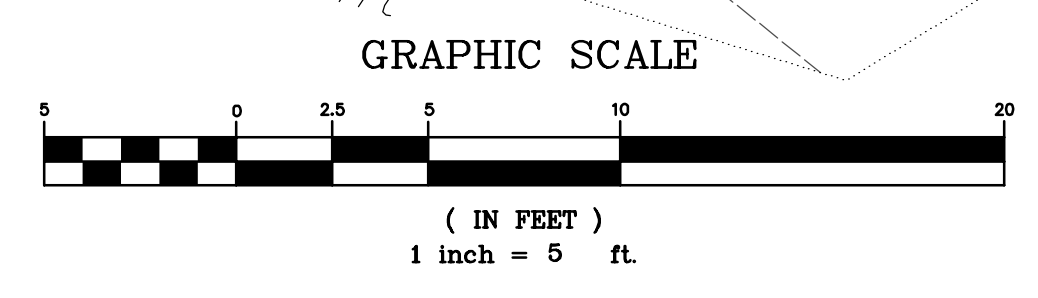
637
BURLINGTON ST.

641
BURLINGTON ST.

645
BURLINGTON ST.

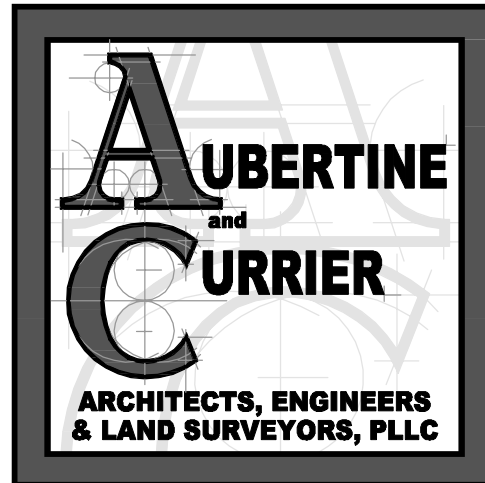
640
BURLINGTON ST.

644/646
BURLINGTON ST.



12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION

Ta
No.

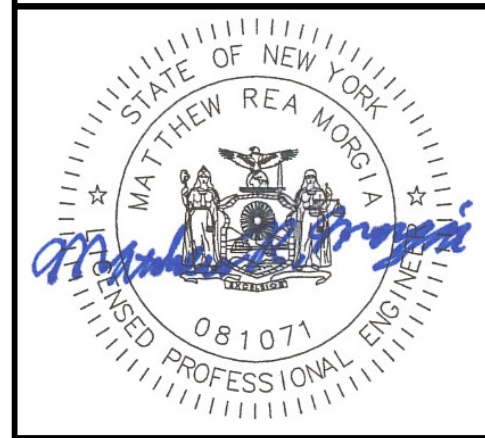


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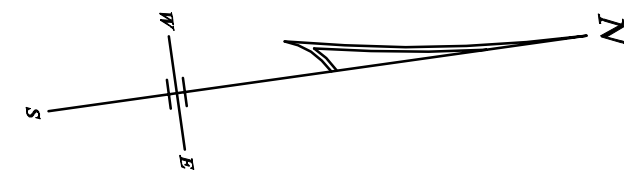


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: 1"=5'
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/30/2024
12/13/2024

ENLARGED GRADING PLAN

CG-405

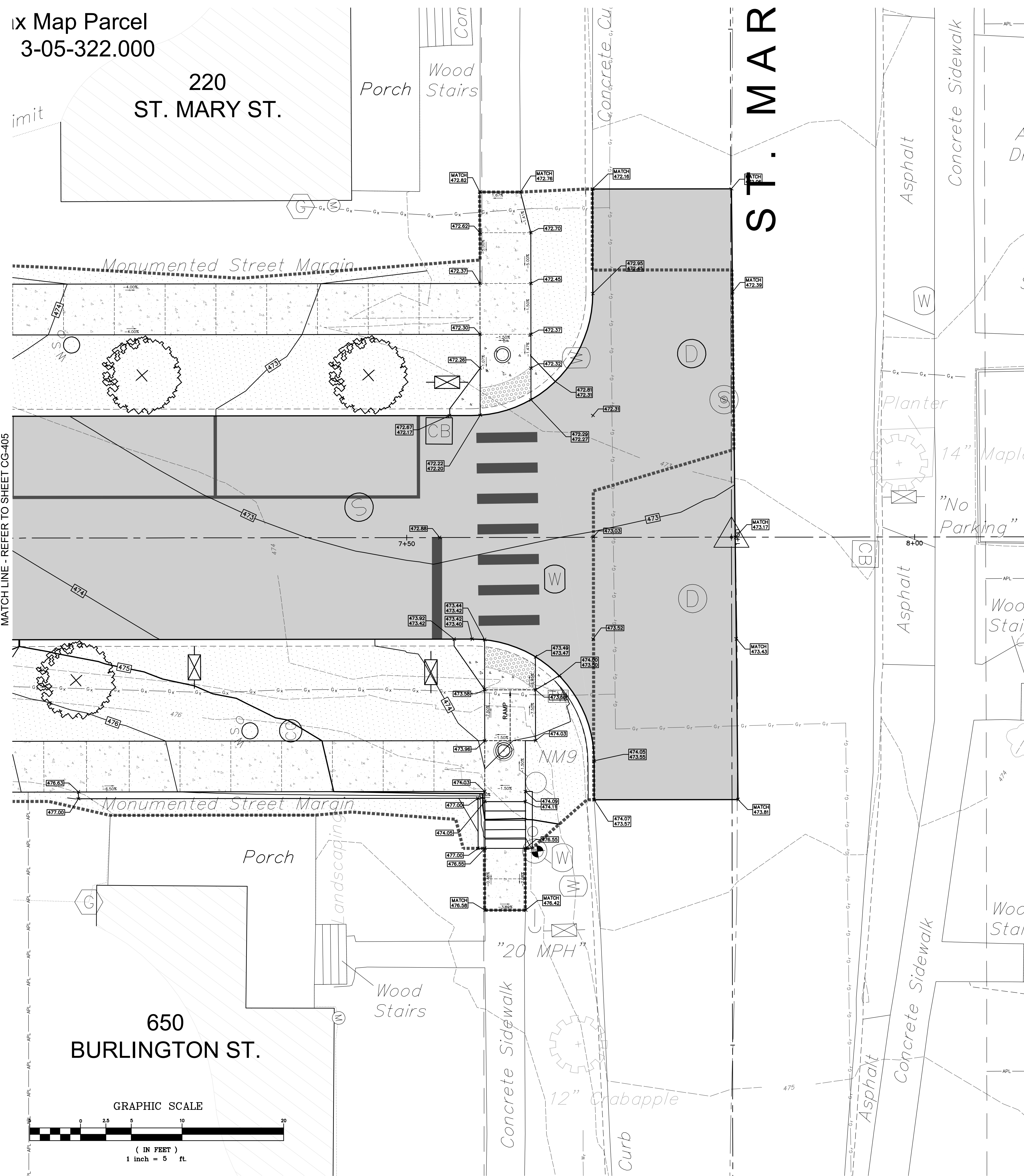


ix Map Parcel
3-05-322.000

220
ST. MARY ST.

Porch
Wood Stairs

ST. MAR



MATCH LINE - REFER TO SHEET CG-405

650
BURLINGTON ST.

Porch

Landscaping

Wood Stairs

Concrete Sidewalk
"20 MPH"

Curb

12" Crabapple

Asphalt
Concrete Sidewalk

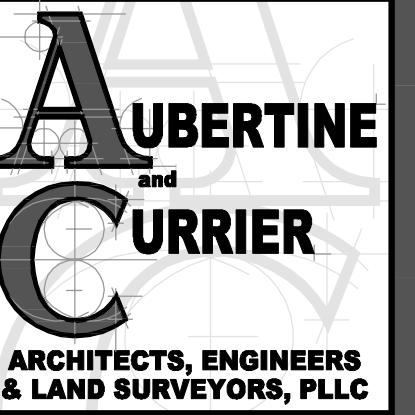
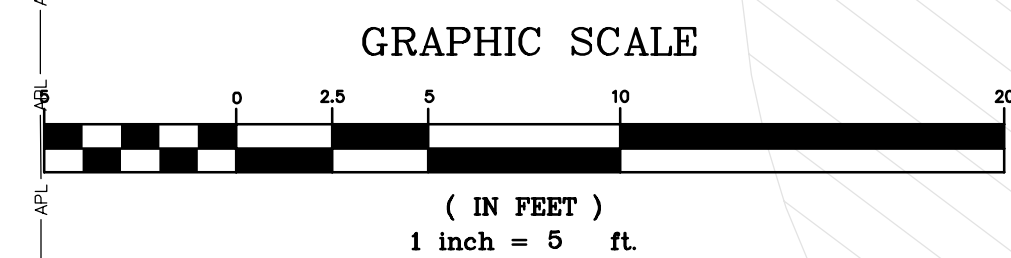
Wood Stair

Asphalt

Planter

14" Maple
"No Parking"

Wood Stair



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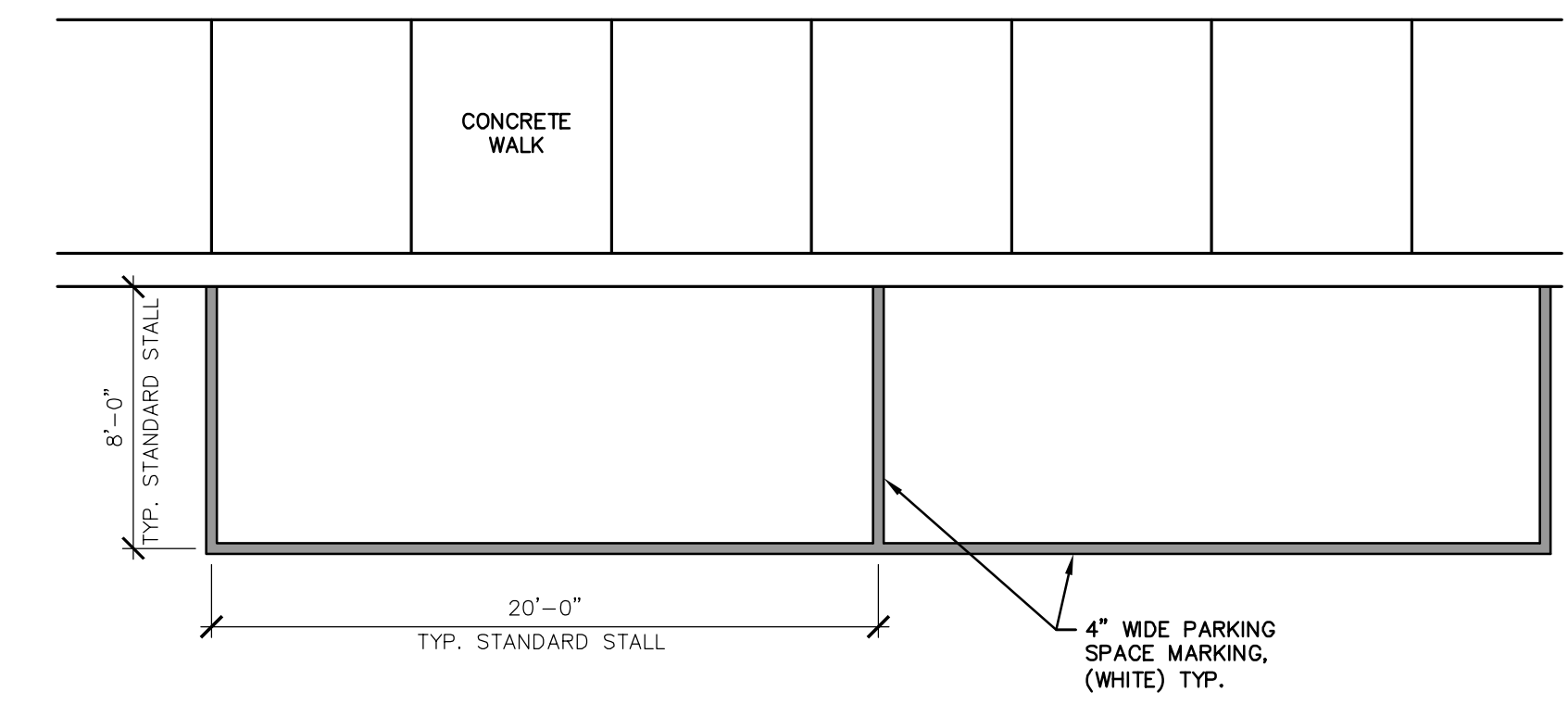
**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
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DRAWN BY:	TFT
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ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

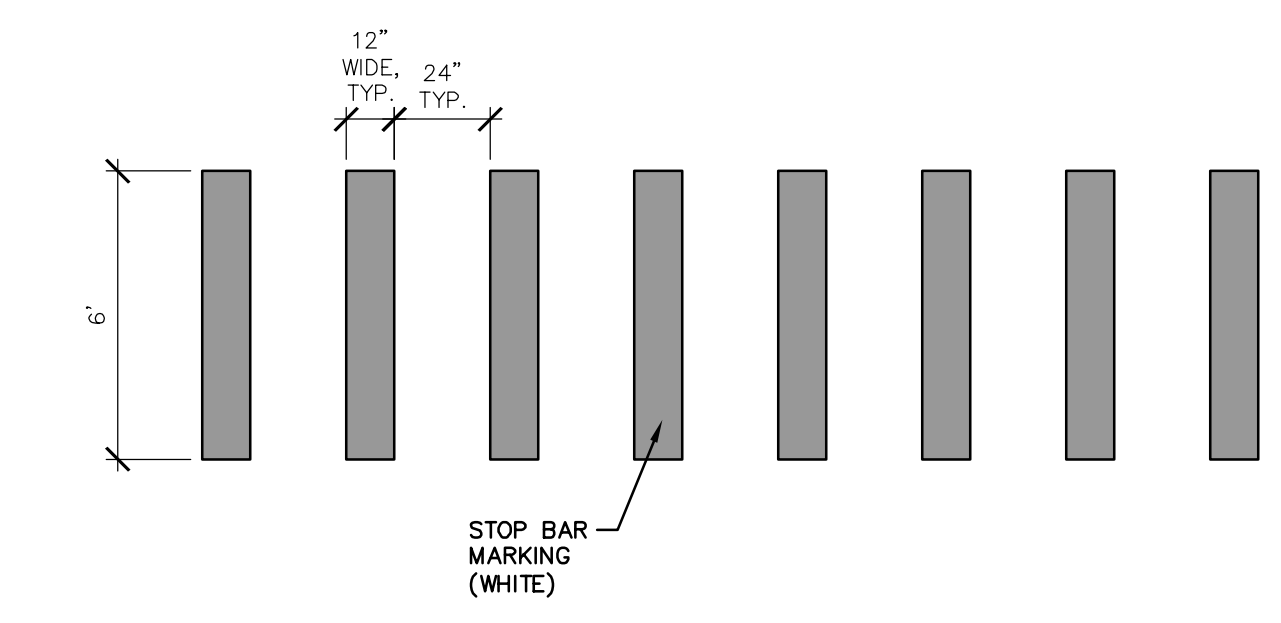
ENLARGED GRADING PLAN

CG-406

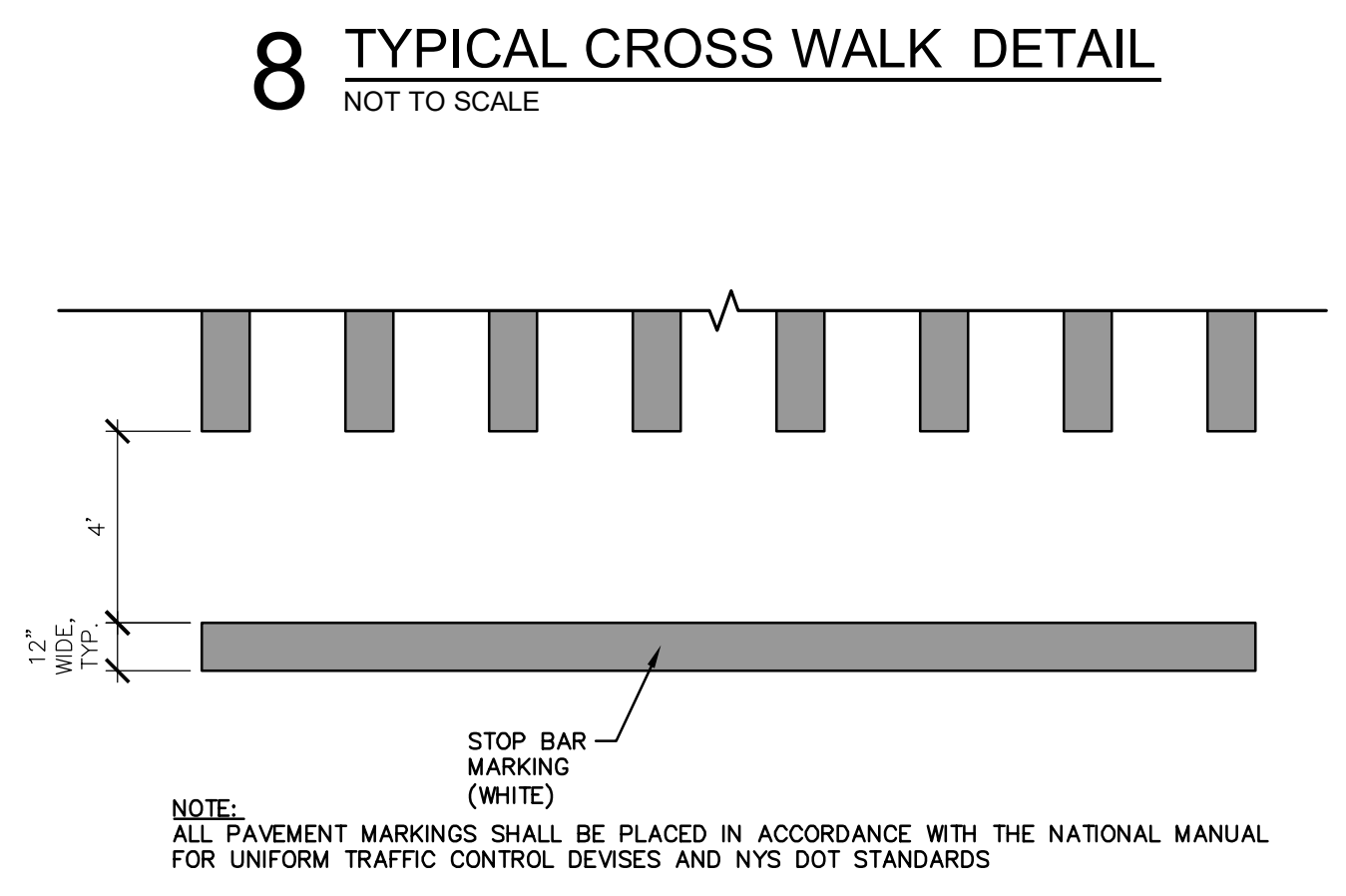
12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



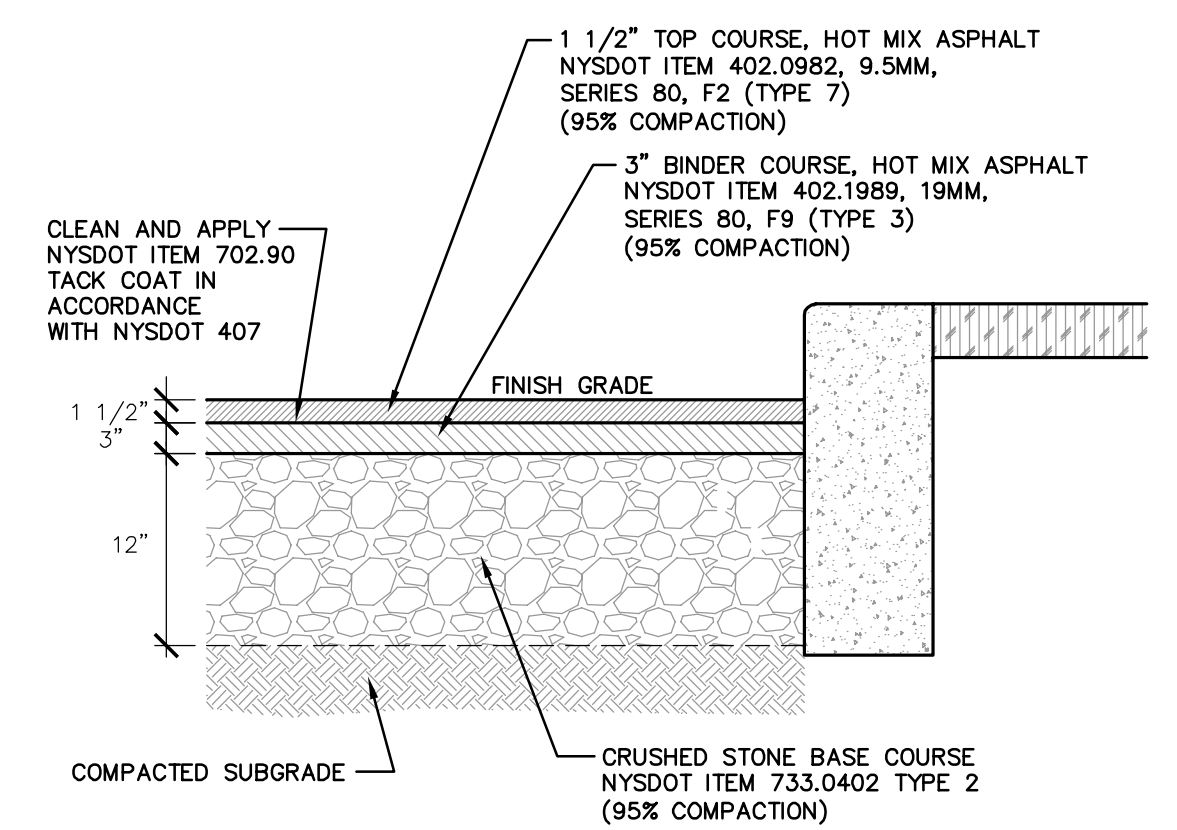
7 TYPICAL PARKING STALL MARKINGS DETAIL
NOT TO SCALE



8 TYPICAL CROSS WALK DETAIL
NOT TO SCALE

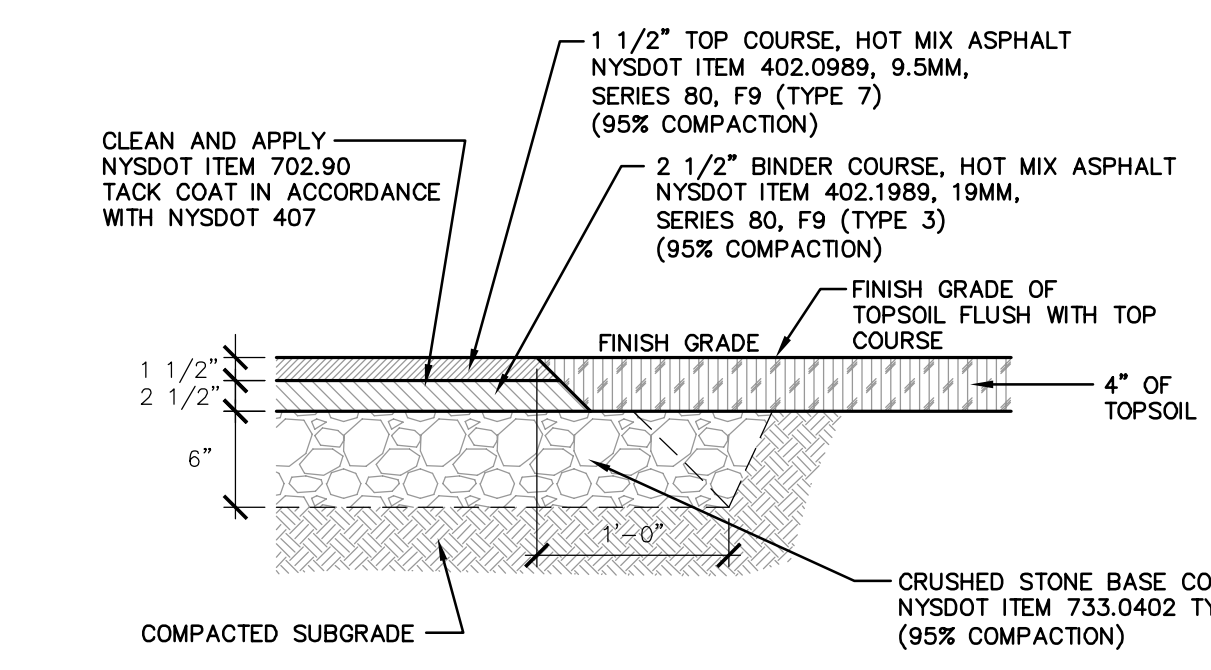


9 TYPICAL STOP BAR DETAIL
NOT TO SCALE



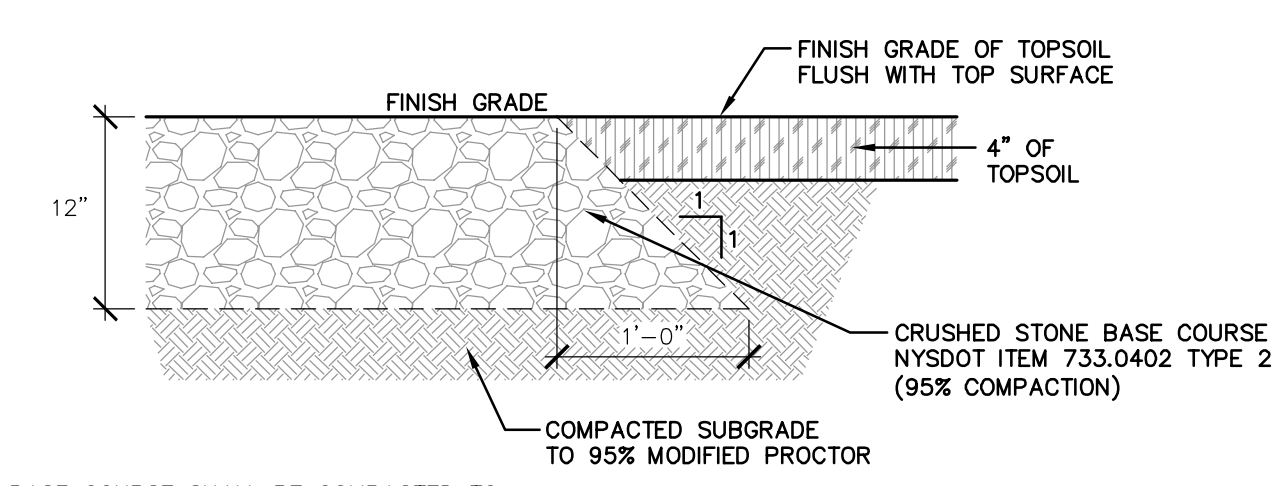
- NOTES:**
1. ALL HMA COMPACTION WILL BE TO 95% MADMTD (MIXTURE'S AVERAGE DAILY MAXIMUM THEORETICAL DENSITY) PER NYS DOT SPECIFICATIONS FOR HMA COMPACTION 402-3.07
 2. BASE COURSE SHALL BE COMPACTED TO 95% MODIFIED PROCTOR MAXIMUM DENSITY.
 3. FIELD VERIFICATION OF COMPACTION SHALL BE BY NUCLEAR DENSITY TESTING METHODS

1 TYPICAL ASPHALT STREET PAVEMENT DETAIL
NOT TO SCALE



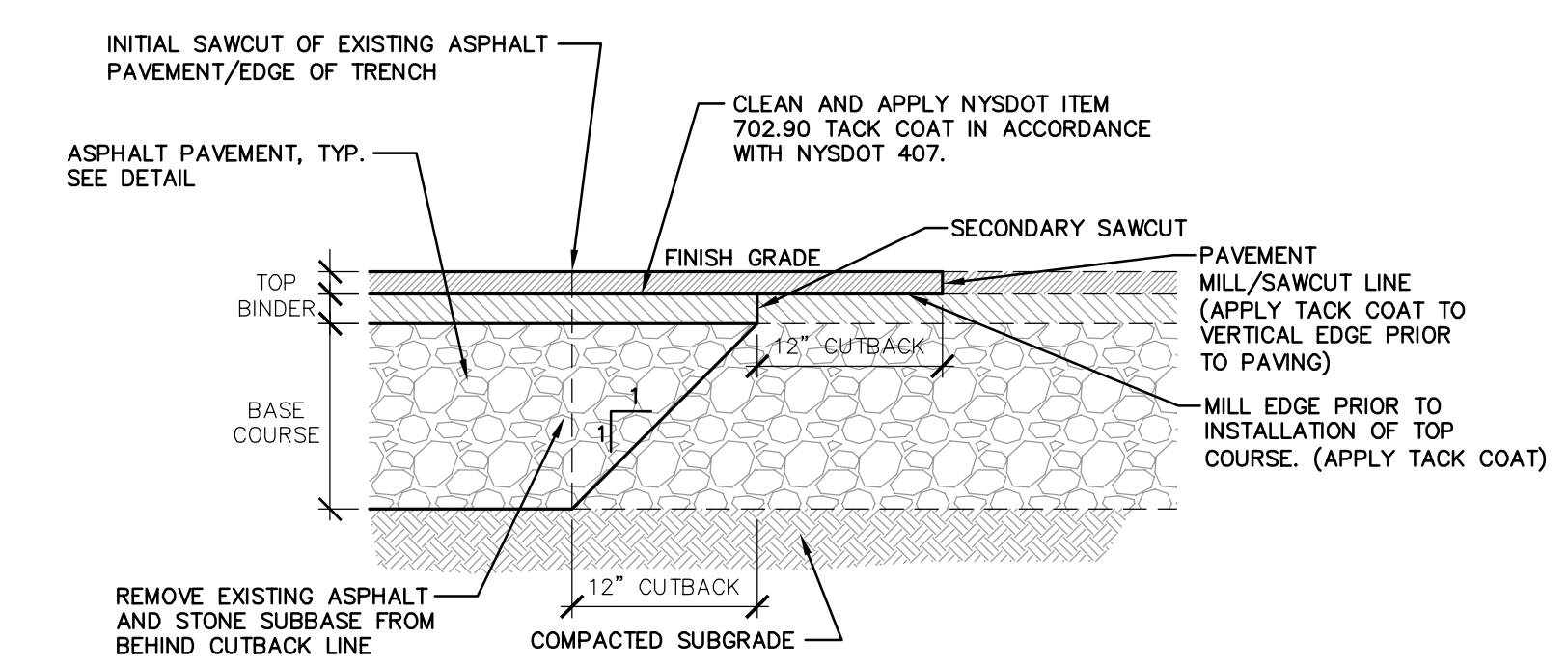
- NOTES:**
1. ALL HMA COMPACTION WILL BE TO 95% MADMTD (MIXTURE'S AVERAGE DAILY MAXIMUM THEORETICAL DENSITY) PER NYS DOT SPECIFICATIONS FOR HMA COMPACTION 402-3.07
 2. BASE COURSE SHALL BE COMPACTED TO 95% MODIFIED PROCTOR MAXIMUM DENSITY.
 3. FIELD VERIFICATION OF COMPACTION SHALL BE BY NUCLEAR DENSITY TESTING METHODS

2 TYPICAL ASPHALT DRIVEWAY PAVEMENT DETAIL
NOT TO SCALE

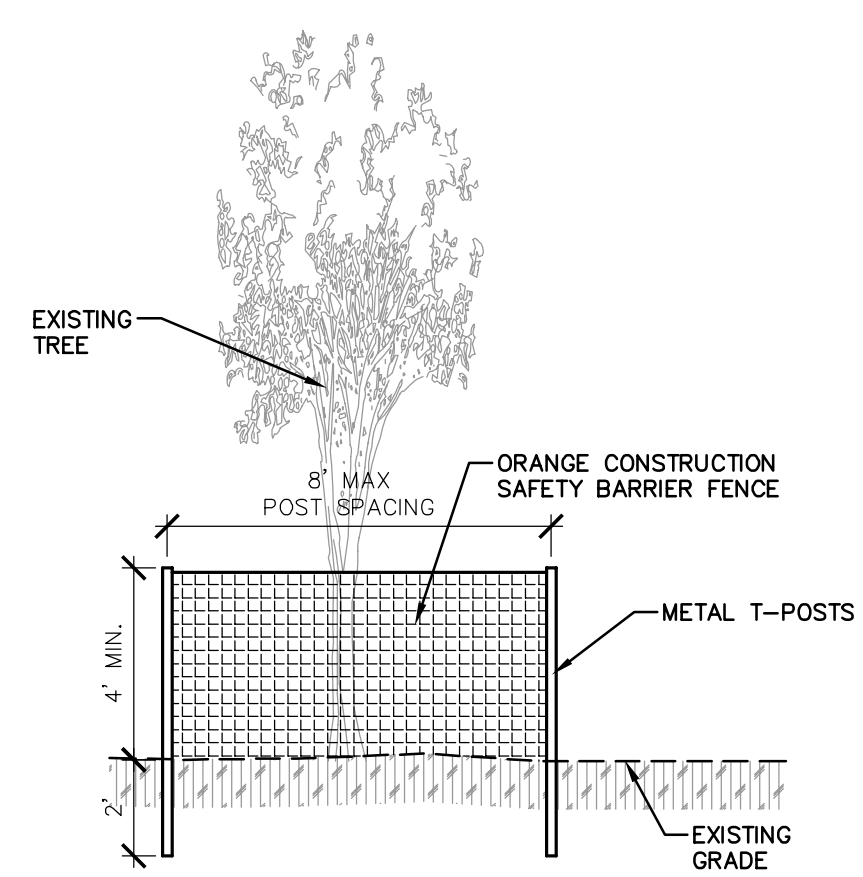


- NOTES:**
1. STONE BASE COURSE SHALL BE COMPACTED TO 95% MODIFIED PROCTOR MAXIMUM DENSITY.
 2. FIELD VERIFICATION OF COMPACTION SHALL BE BY NUCLEAR DENSITY TESTING METHODS

3 TYPICAL CRUSHED STONE SURFACE DRIVEWAY DETAIL
NOT TO SCALE

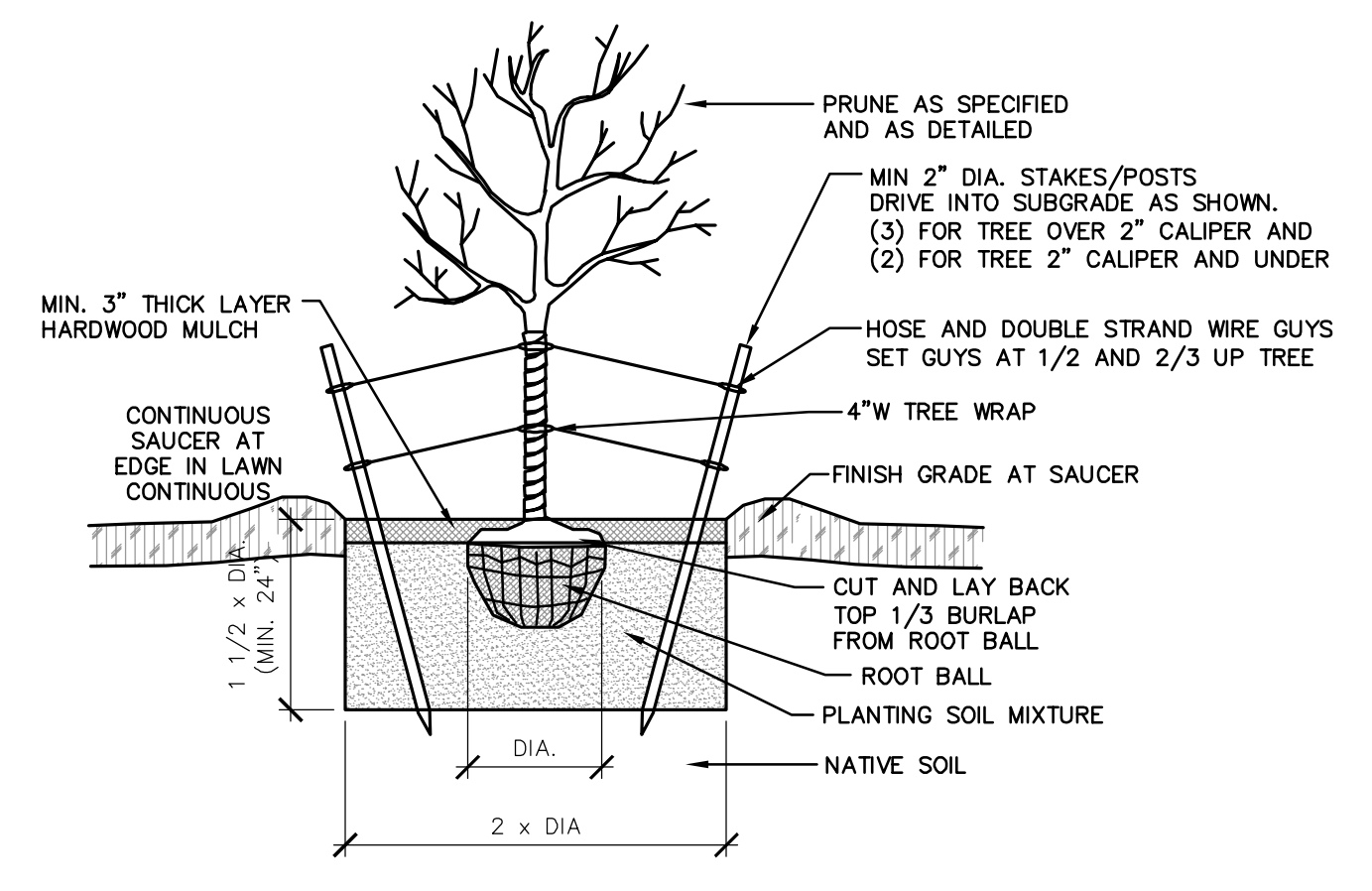


4 TYPICAL ASPHALT PAVEMENT JOINT DETAIL
NOT TO SCALE (CONNECTION AT EXISTING PAVEMENT)

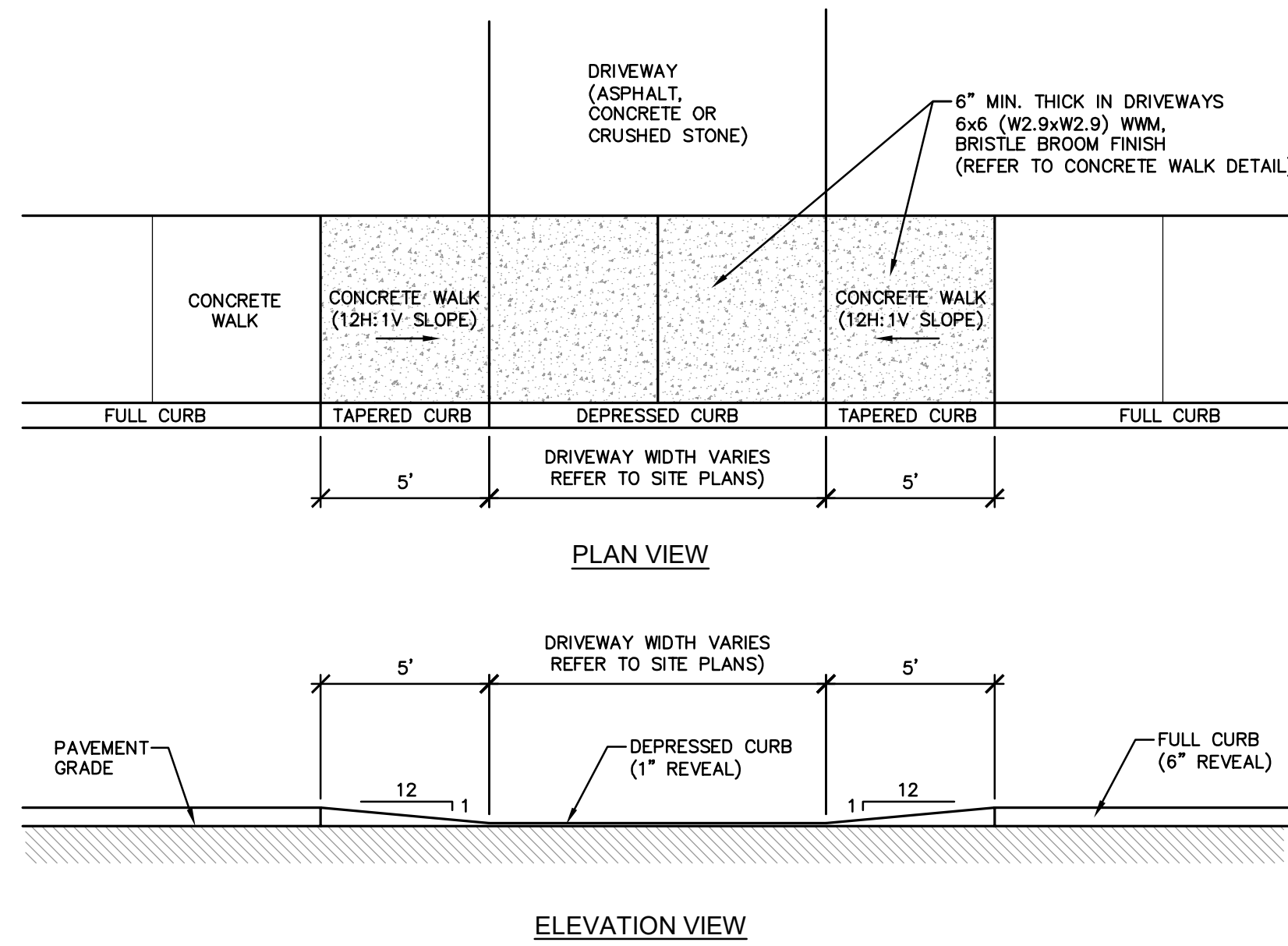


- NOTES:**
1. TREE PROTECTION FENCING SHALL BE INSTALLED PRIOR TO THE START OF ANY CONSTRUCTION.
 2. FOR NEWLY PLANTED TREES AND TREES WITH A CROWN SPREAD OF EIGHT (8) FEET OR LESS, A MINIMUM OF 8'x8' SQUARE TREE PROTECTION FENCE SHALL BE INSTALLED.
 3. FOR TREES WITH A CROWN SPREAD OF OVER EIGHT (8) FEET, THE TREE PROTECTION FENCE SHALL EXTEND TO THE TREE'S DRIPLINE OR AT A DISTANCE PRESCRIBED BY THE CITY ARBORIST.
 4. THERE SHALL BE NO STORAGE OF MATERIAL WITHIN BOUNDARIES OF TREE PROTECTION FENCING.
 5. TREE PROTECTION FENCING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION.

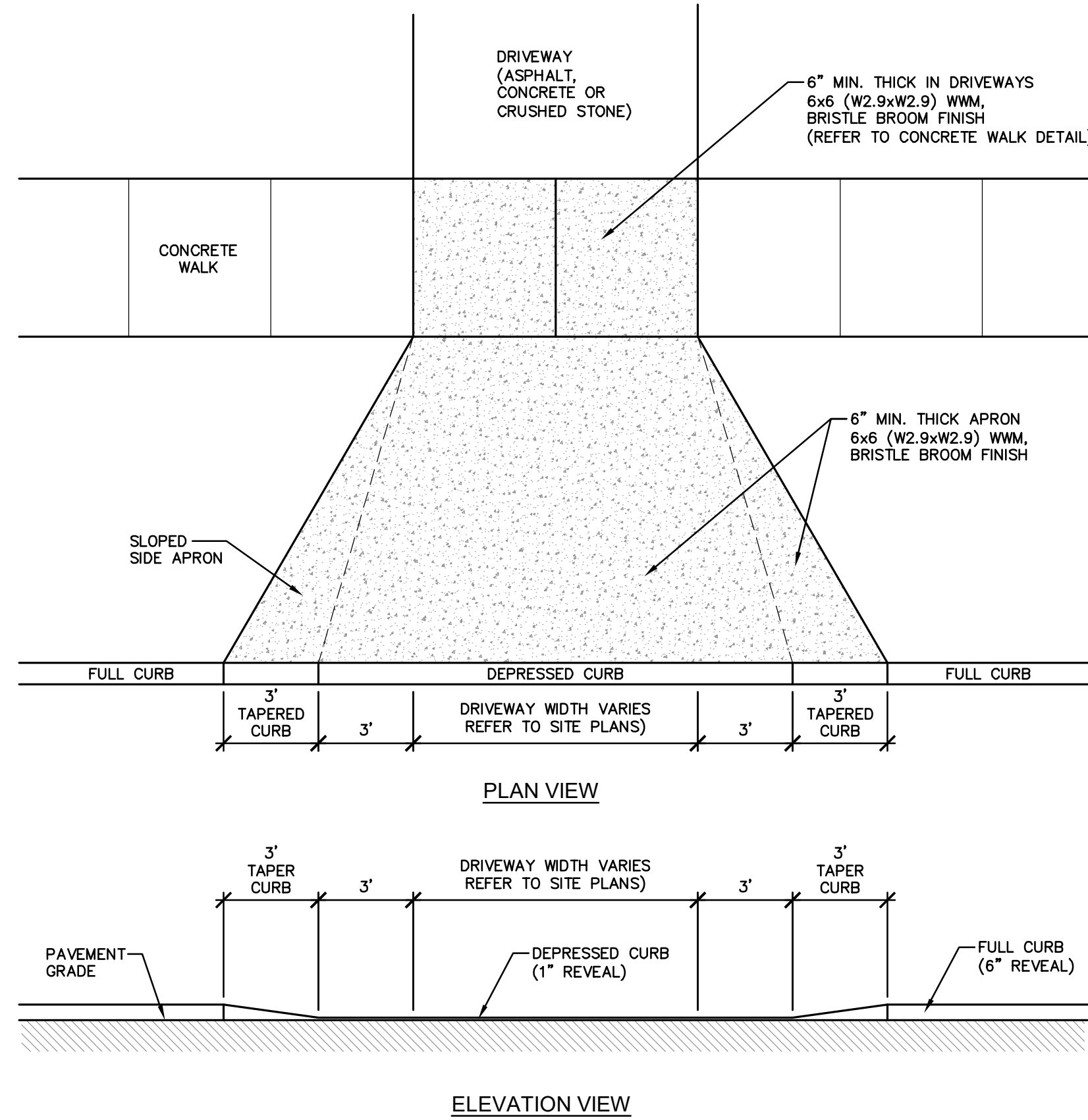
5 TYPICAL TREE PROTECTION DETAIL
NOT TO SCALE



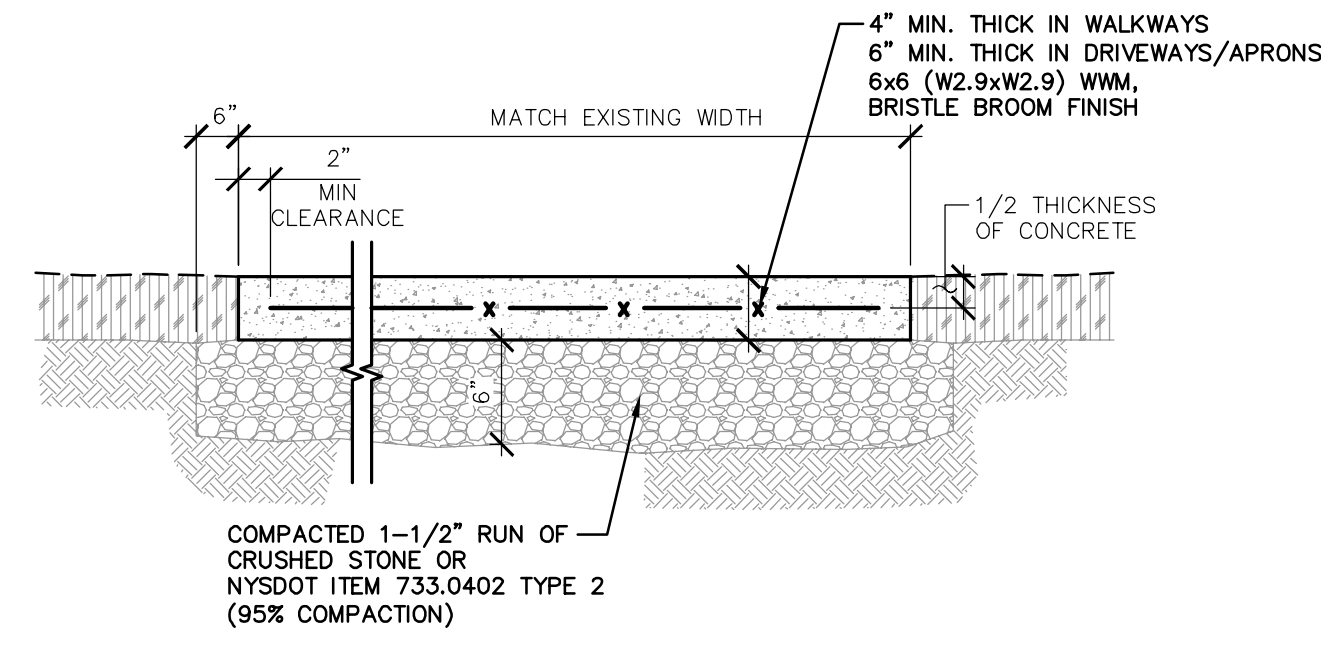
6 TYPICAL TREE PLANTING DETAIL
NOT TO SCALE



1 TYPICAL DRIVEWAY APRON DETAIL
NOT TO SCALE (WITH NO GRASS MEDIUM)

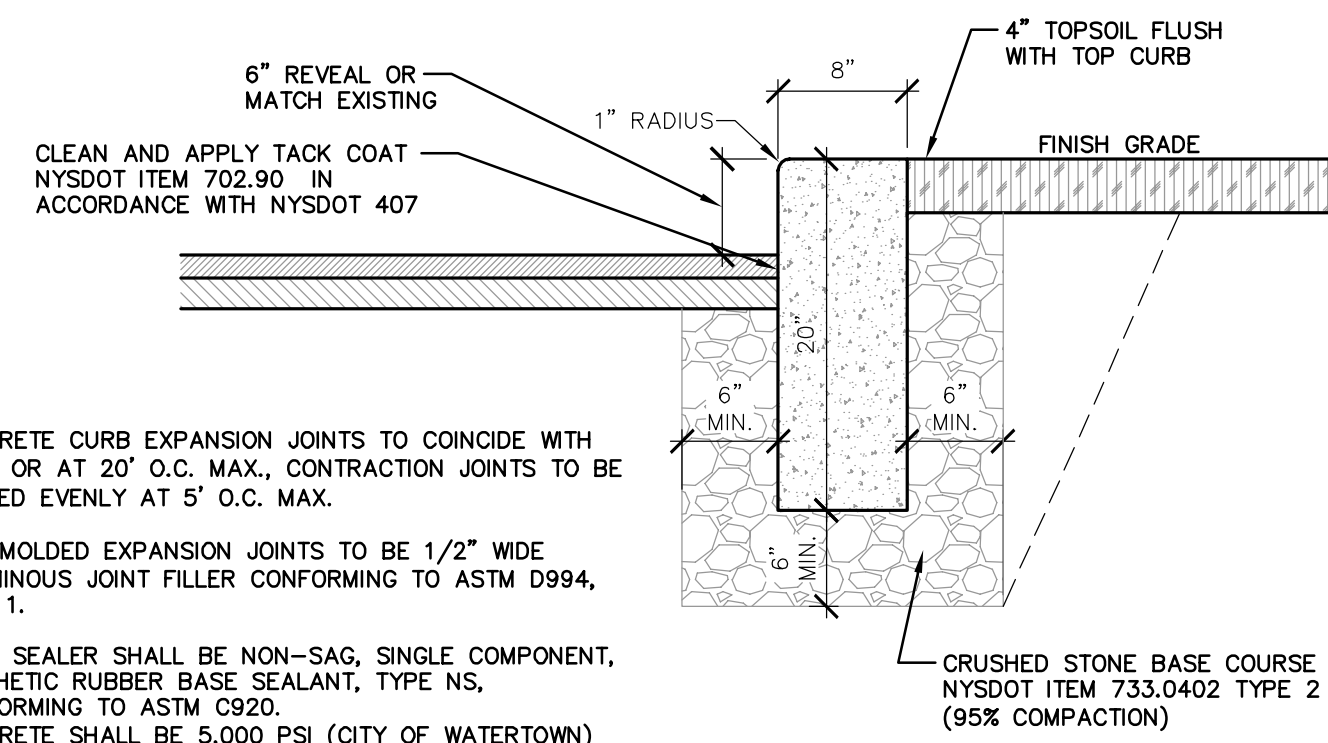


2 TYPICAL DRIVEWAY APRON DETAIL
NOT TO SCALE (WITH GRASS MEDIUM)



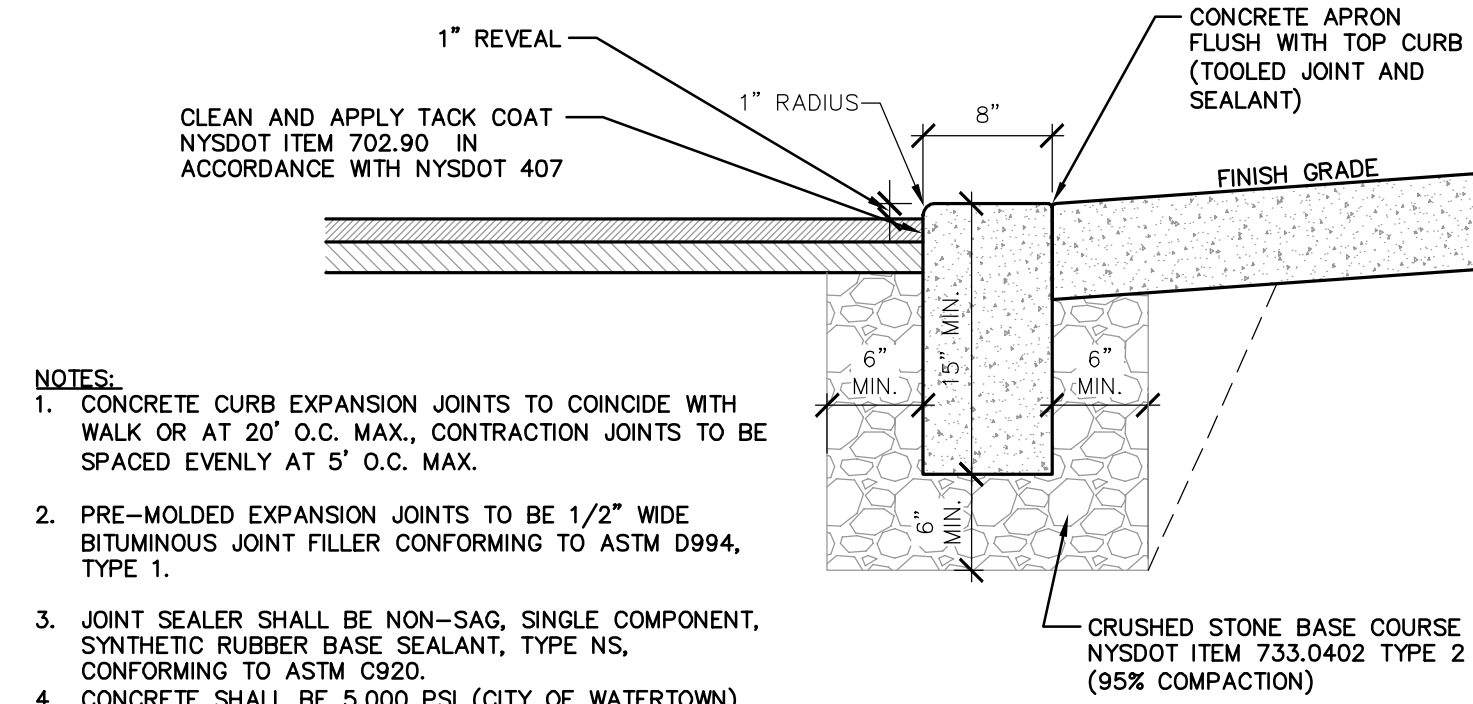
- NOTES:**
- CONCRETE WALK EXPANSION JOINTS TO COINCIDE AT 20' O.C. MAX.
 - CONTRACTION JOINTS TO BE SPACED EVENLY AT 4' TO 6' O.C. MAX. BOTH DIRECTIONS. CONTRACTION JOINT SPACING SHALL BE SPACED SYMMETRICALLY BASED UPON THE SIDEWALK WIDTH BEING CONSTRUCTED. (I.E. 6' WIDE WALK - 6' CONTROL JOINTS, 5' WIDE WALK - 5' CONTROL JOINTS, 8' WIDE WALK - 4' CONTROL JOINTS)
 - EXPANSION JOINTS TO BE 1/2" WIDE FILLED WITH PREMOLDED JOINT FILLER AND SEALANT CONFORMING TO ASTM D1752 TYPE II.
 - CONTROL/CONTRACTION JOINTS SHALL BE TOOLED OR SAW CUT JOINT WITH A DEPTH OF 1/4 THE CONCRETE THICKNESS.
 - CONCRETE SHALL BE 5,000 PSI (CITY OF WATERTOWN)
 - ALL WALKS WITHIN CITY MARGINS SHALL BE BUILT TO CITY SPECIFICATIONS AND SHALL MEET THE (PROWAG) PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES

3 TYPICAL CONCRETE WALK DETAIL
NOT TO SCALE



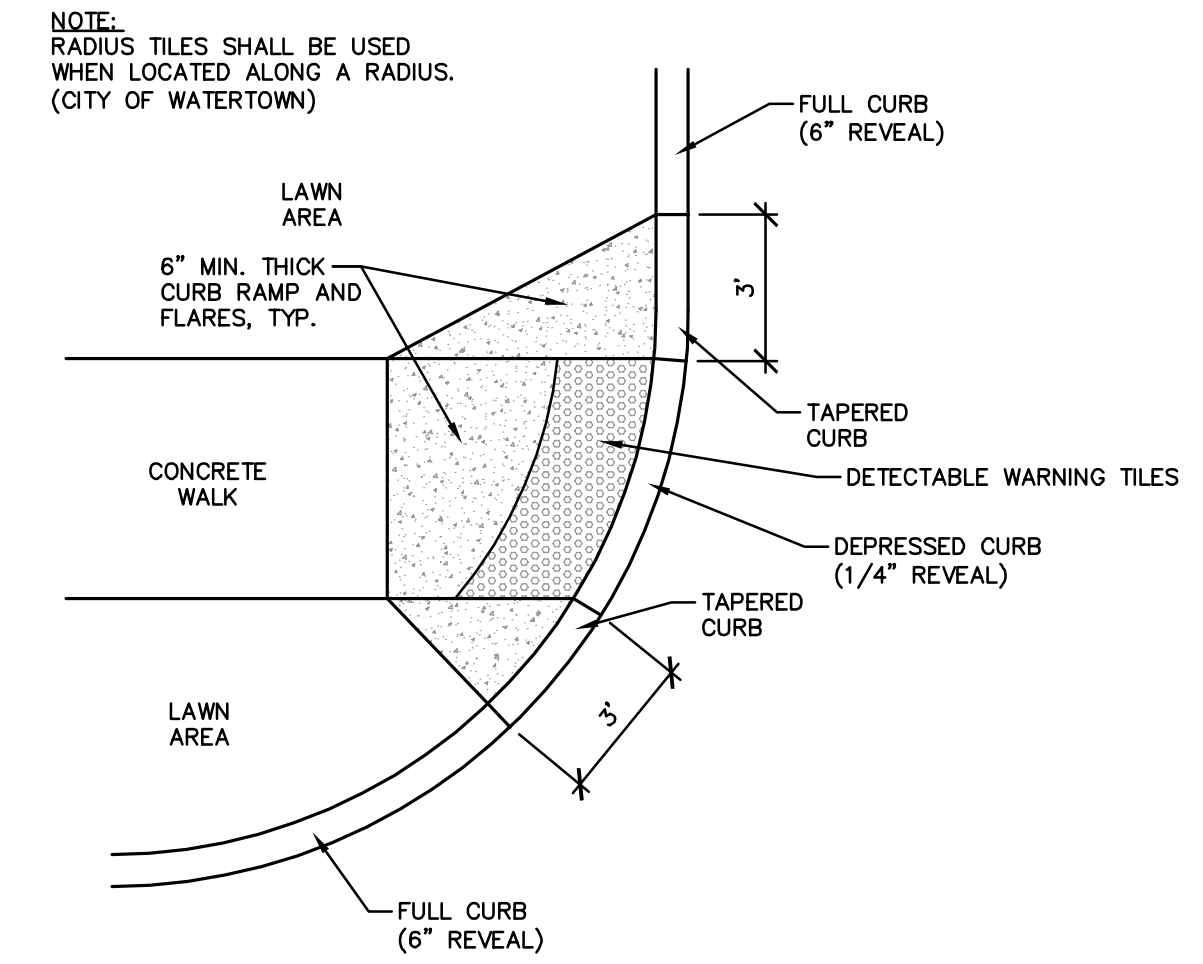
- NOTES:**
- CONCRETE CURB EXPANSION JOINTS TO COINCIDE WITH WALK OR AT 20' O.C. MAX., CONTRACTION JOINTS TO BE SPACED EVENLY AT 5' O.C. MAX.
 - PRE-MOLDED EXPANSION JOINTS TO BE 1/2" WIDE BITUMINOUS JOINT FILLER CONFORMING TO ASTM D994, TYPE 1.
 - JOINT SEALER SHALL BE NON-SAG, SINGLE COMPONENT, SYNTHETIC RUBBER BASE SEALANT, TYPE NS, CONFORMING TO ASTM C920.
 - CONCRETE SHALL BE 5,000 PSI (CITY OF WATERTOWN)
 - ALL CURBS WITHIN CITY MARGINS SHALL BE BUILT TO CITY SPECIFICATIONS AND SHALL MEET THE (PROWAG) PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES

4 TYPICAL FULL CONCRETE CURB DETAIL
NOT TO SCALE

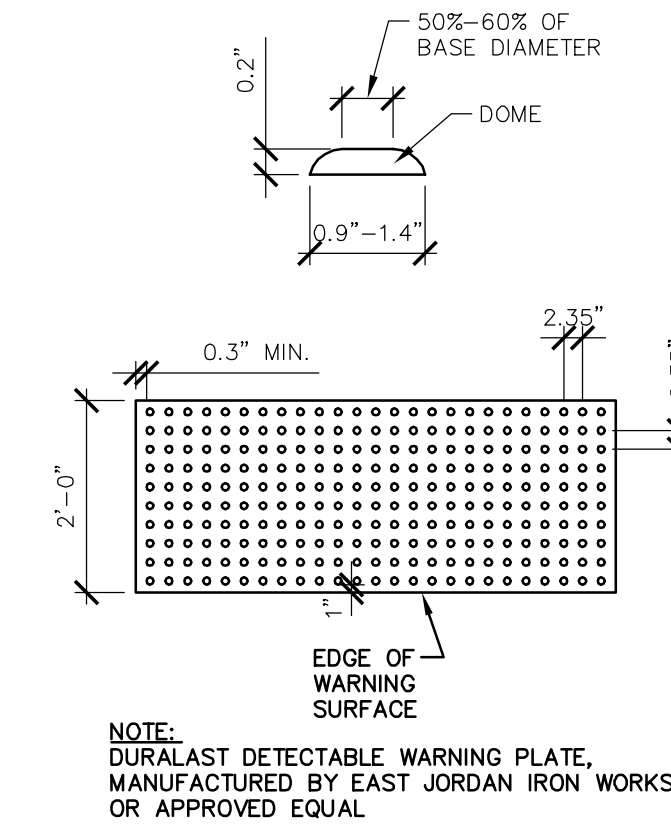


- NOTES:**
- CONCRETE CURB EXPANSION JOINTS TO COINCIDE WITH WALK OR AT 20' O.C. MAX., CONTRACTION JOINTS TO BE SPACED EVENLY AT 5' O.C. MAX.
 - PRE-MOLDED EXPANSION JOINTS TO BE 1/2" WIDE BITUMINOUS JOINT FILLER CONFORMING TO ASTM D994, TYPE 1.
 - JOINT SEALER SHALL BE NON-SAG, SINGLE COMPONENT, SYNTHETIC RUBBER BASE SEALANT, TYPE NS, CONFORMING TO ASTM C920.
 - CONCRETE SHALL BE 5,000 PSI (CITY OF WATERTOWN)
 - ALL CURBS WITHIN CITY MARGINS SHALL BE BUILT TO CITY SPECIFICATIONS AND SHALL MEET THE (PROWAG) PUBLIC RIGHT OF WAY ACCESSIBILITY GUIDELINES

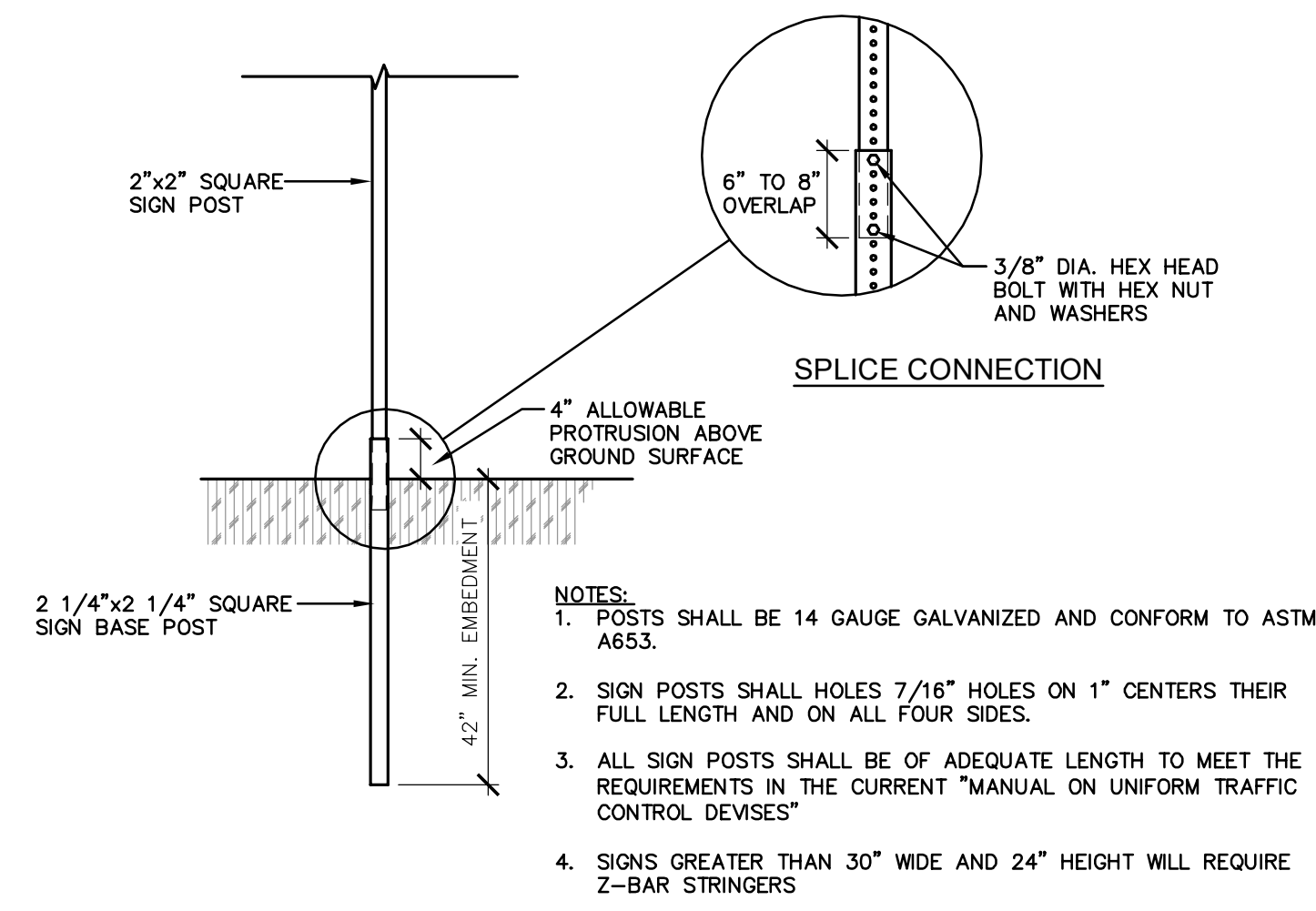
5 TYPICAL DEPRESSED CONCRETE CURB DETAIL
NOT TO SCALE



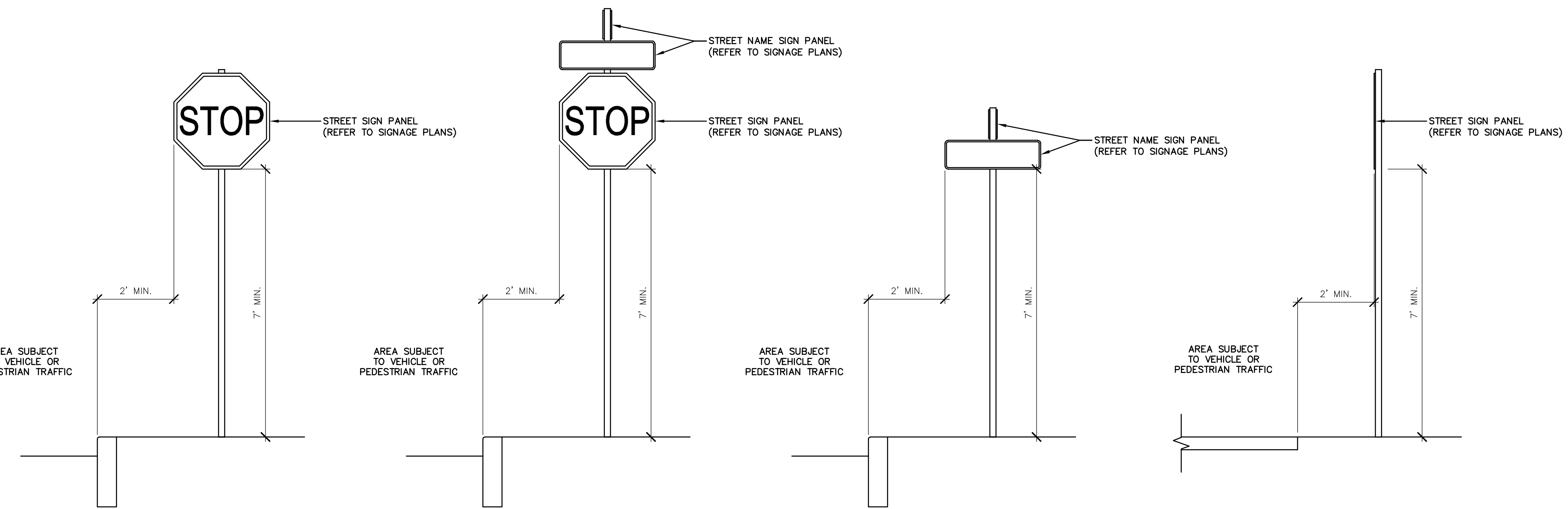
6 TYPICAL SIDEWALK WITH DETECTABLE WARNING TILE DETAIL
NOT TO SCALE



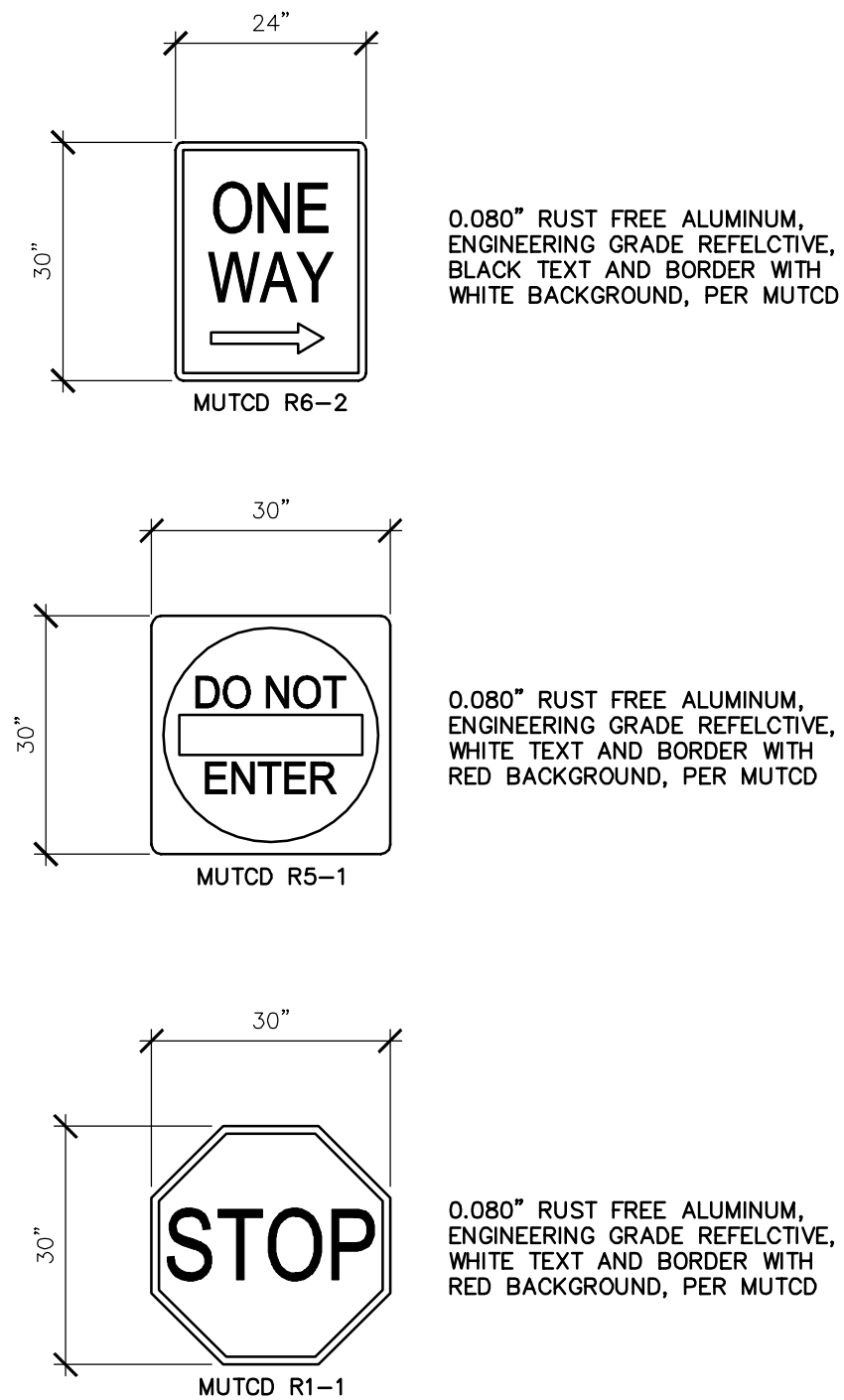
7 TYPICAL DETECTABLE WARNING SURFACE DETAIL
NOT TO SCALE



1 TYPICAL TRAFFIC SIGN POST DETAIL
NOT TO SCALE

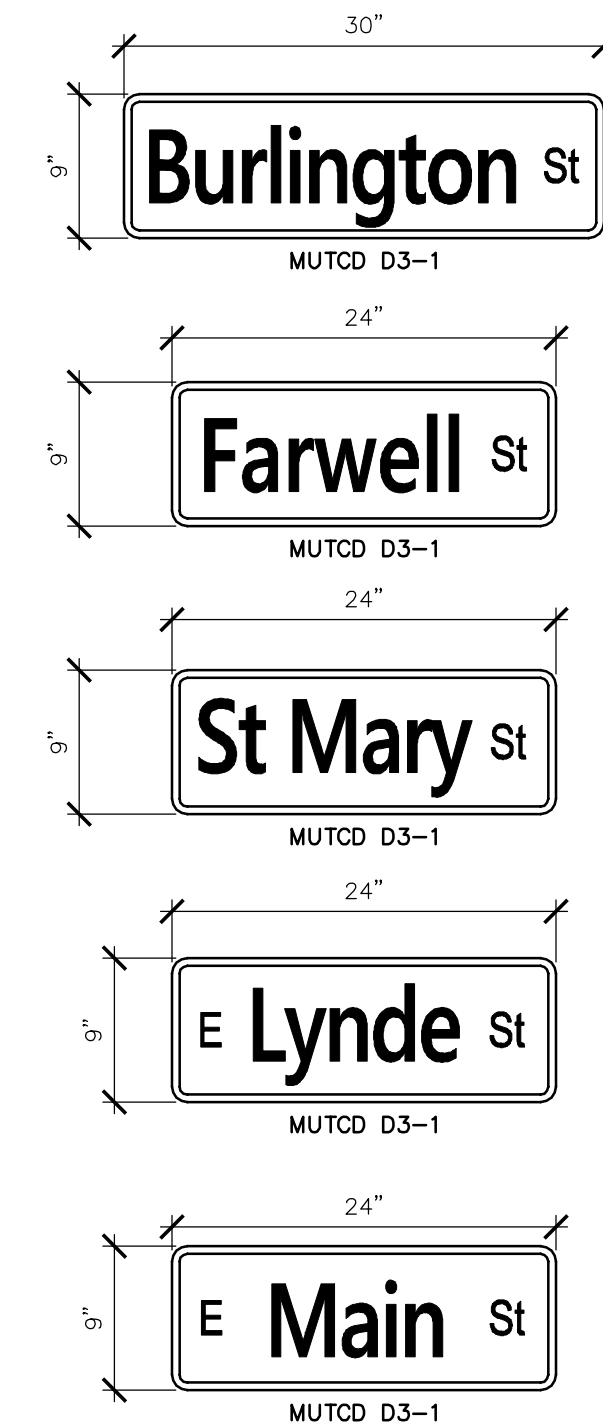


2 TYPICAL TRAFFIC SIGN LAYOUT DETAIL
NOT TO SCALE



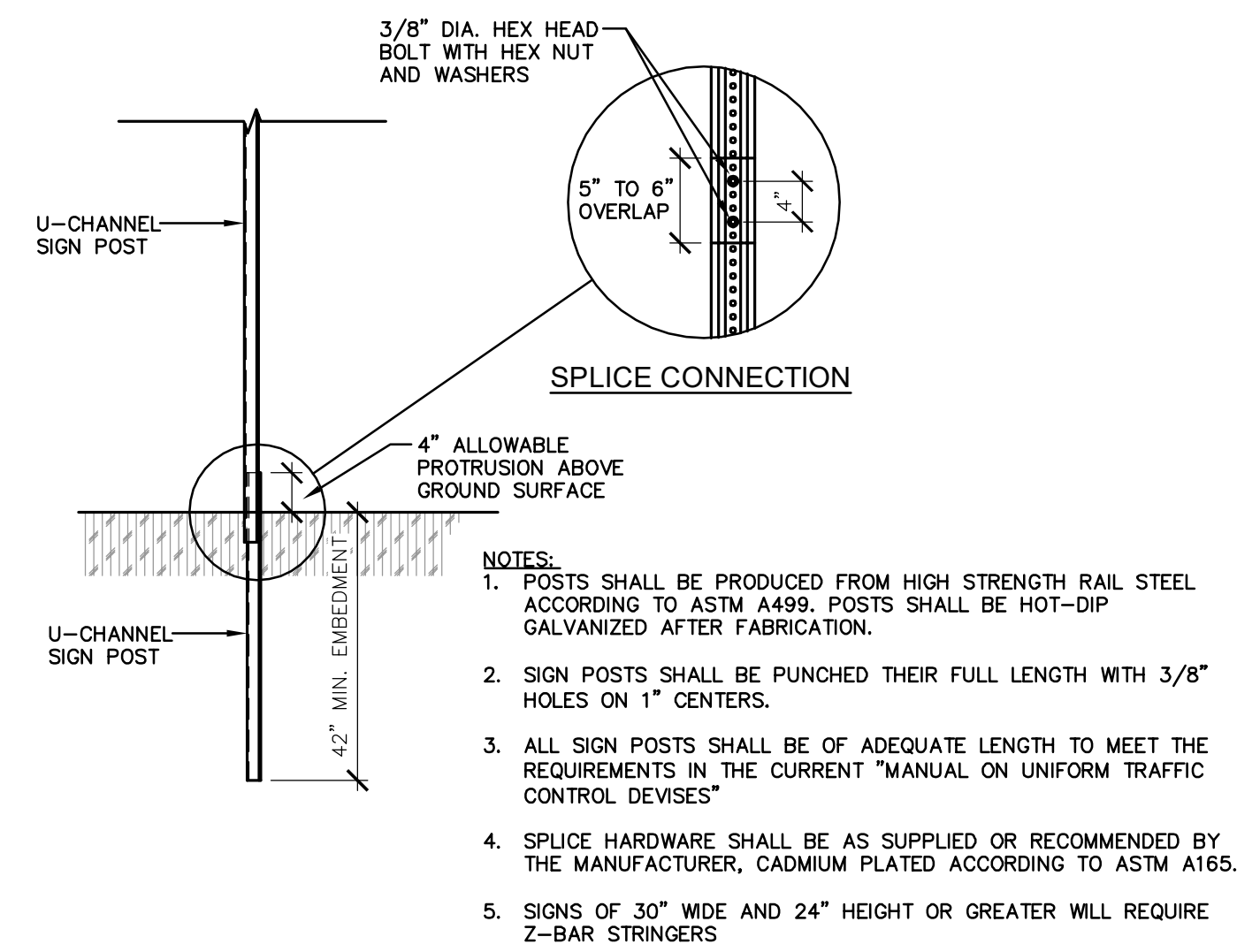
- NOTES:
- ALL SIGNS SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
 - SIGNS WIDER THAN 30" SHALL USE TWO OR MORE POSTS.
 - SIGN POSTS AND MOUNTING SHALL BE CONSTRUCTED AS TO HOLD SIGNS IN A PROPER AND PERMANENT POSITION AND TO RESIST SWAYING IN THE WIND OR DISPLACEMENT BY VANDALISM.

3 STREET SIGN PANEL DETAIL
NOT TO SCALE

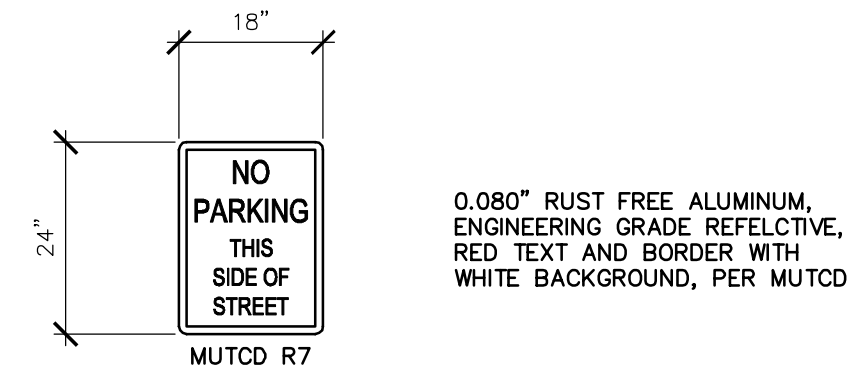


- STREET NAME SIGNS:
- 0.080" RUST FREE ALUMINUM, ENGINEERING GRADE REFLECTIVE, WHITE TEXT AND BORDER WITH GREEN BACKGROUND, PER MUTCD, INITIAL UPPER CASE LETTER SHALL BE A MIN. 4" HIGH, LOWER CASE LETTERS SHALL BE MIN. 3" HIGH, SUPPLEMENTAL UPPER CASE LETTER SHALL BE A MIN. 3" HIGH, LOWER CASE LETTERS SHALL BE MIN. 2 1/4" HIGH.

5 STREET NAME SIGN PANEL DETAIL
NOT TO SCALE

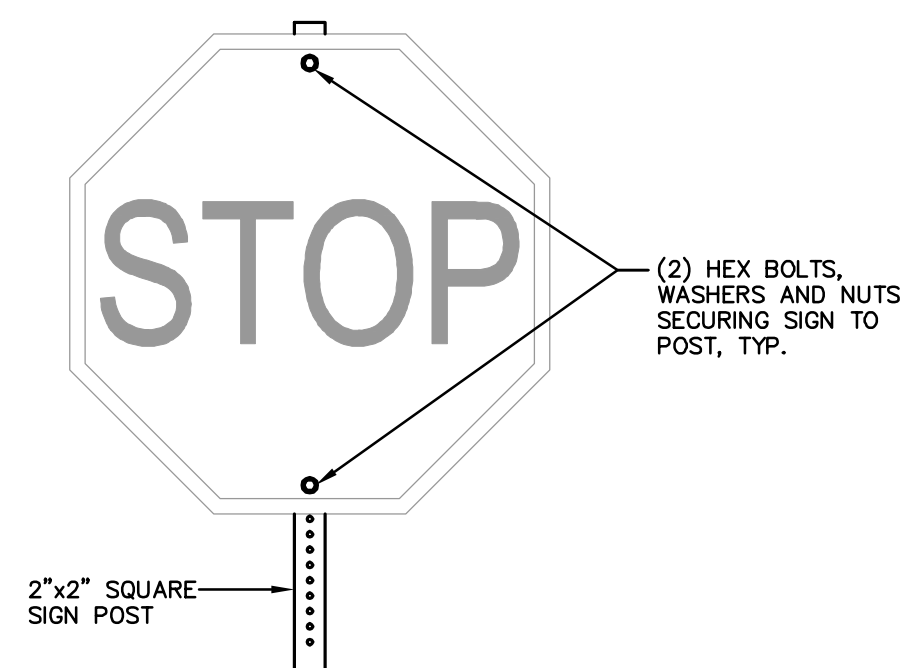


7 TYPICAL "NO PARKING" SIGN POST DETAIL
NOT TO SCALE

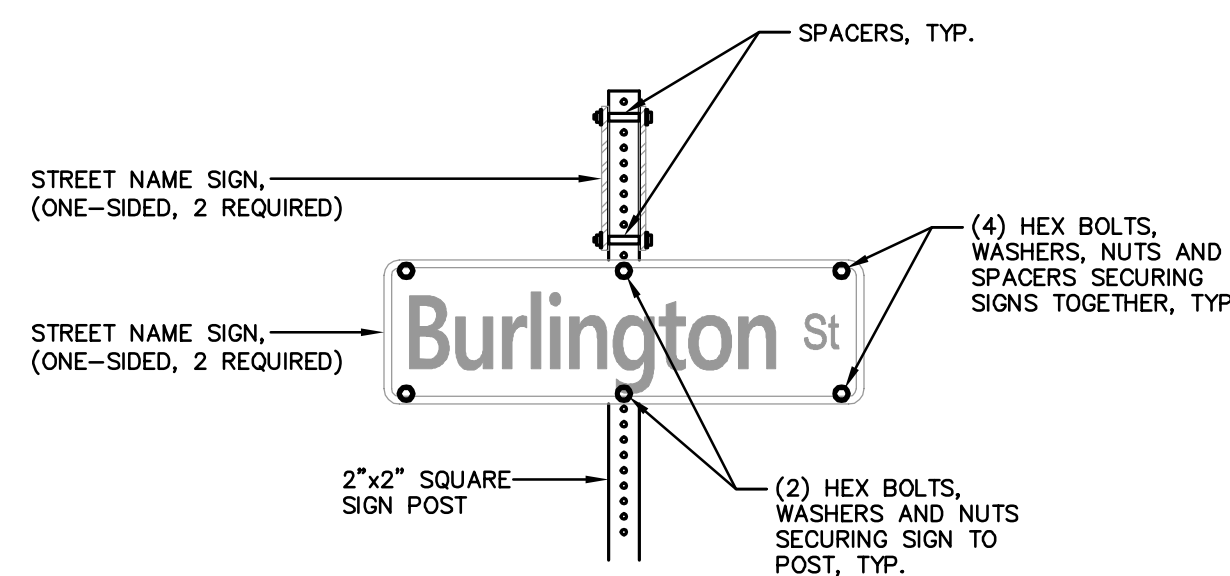


- NOTES:
- ALL SIGNS SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
 - SIGN POSTS AND MOUNTING SHALL BE CONSTRUCTED AS TO HOLD SIGNS IN A PROPER AND PERMANENT POSITION AND TO RESIST SWAYING IN THE WIND OR DISPLACEMENT BY VANDALISM.

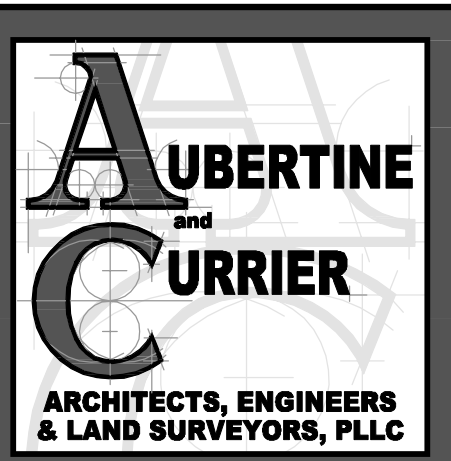
8 "NO PARKING" SIGN PANEL DETAIL
NOT TO SCALE



4 STREET SIGN MOUNTING DETAIL
NOT TO SCALE



6 STREET NAME SIGN MOUNTING DETAIL
NOT TO SCALE



NYS WBE/DBE Certified
SBA Woman Owned
Small Business (WOSB)

aubertinecurrier.com

522 Bradley Street
Watertown, New York 13601
Phone: (315)782-2005

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BURLINGTON STREET RECONSTRUCTION PROJECT
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: AS NOTED
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/23/2024
12/13/2024

SIGNAGE DETAILS

CS-502



**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

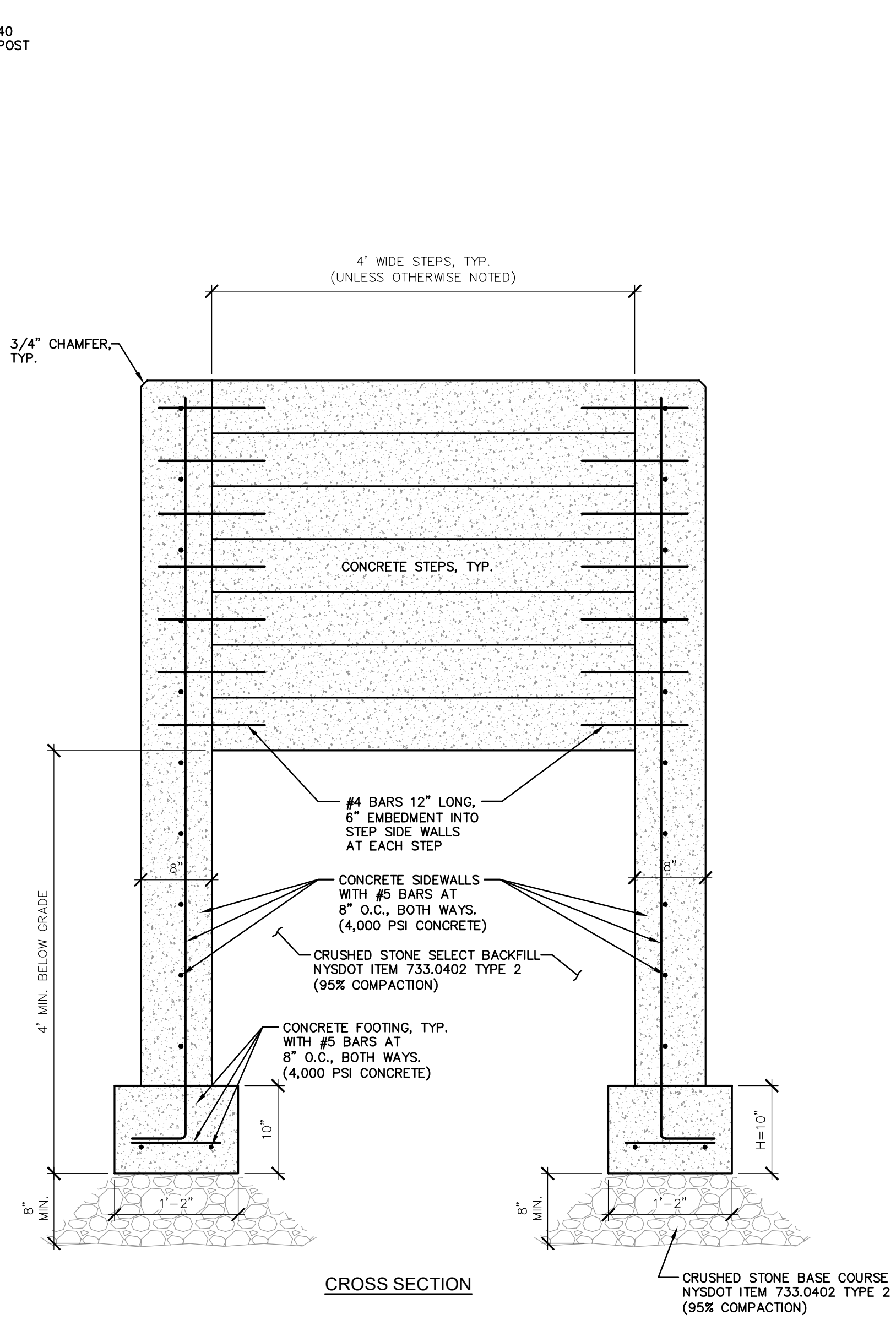
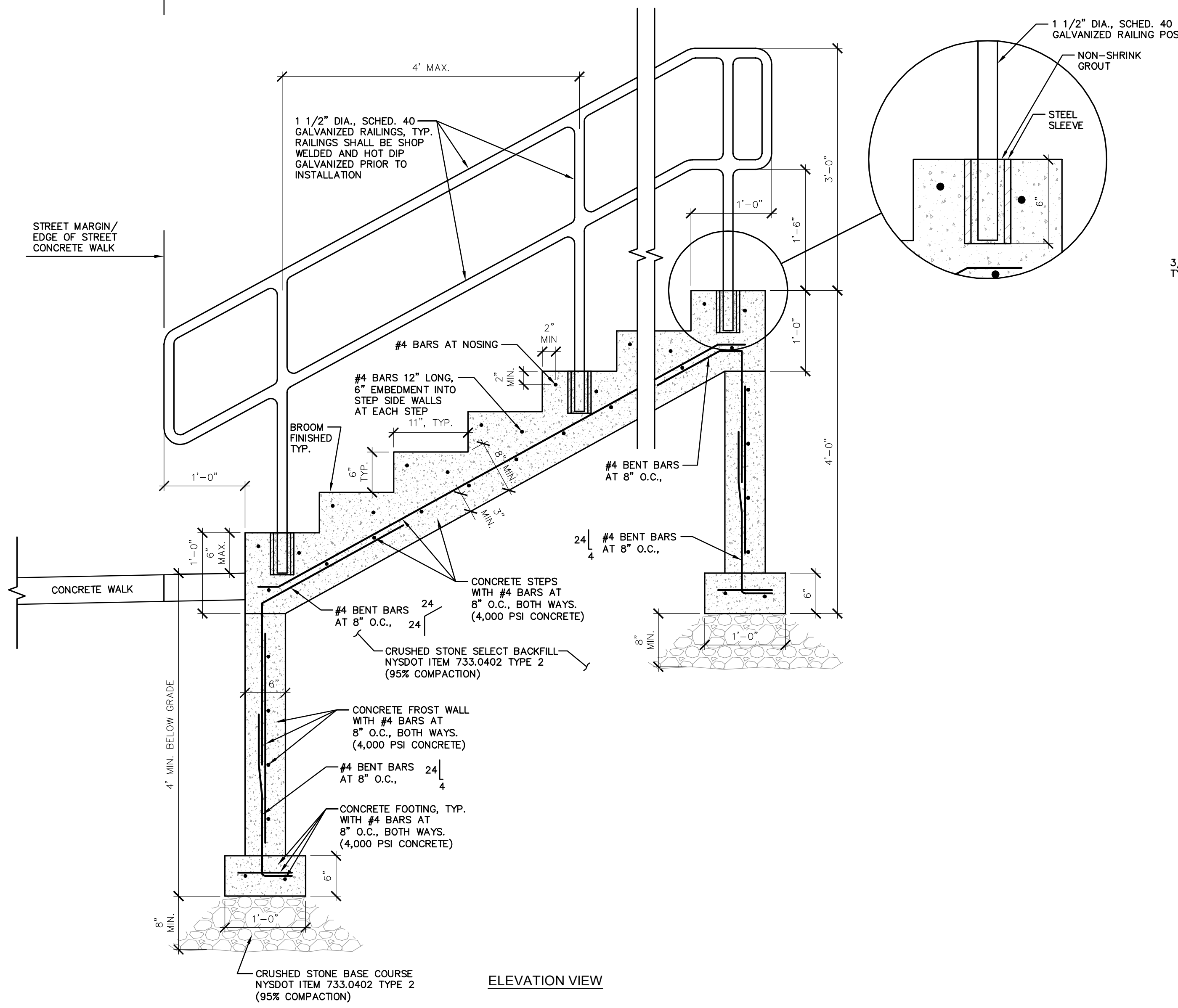
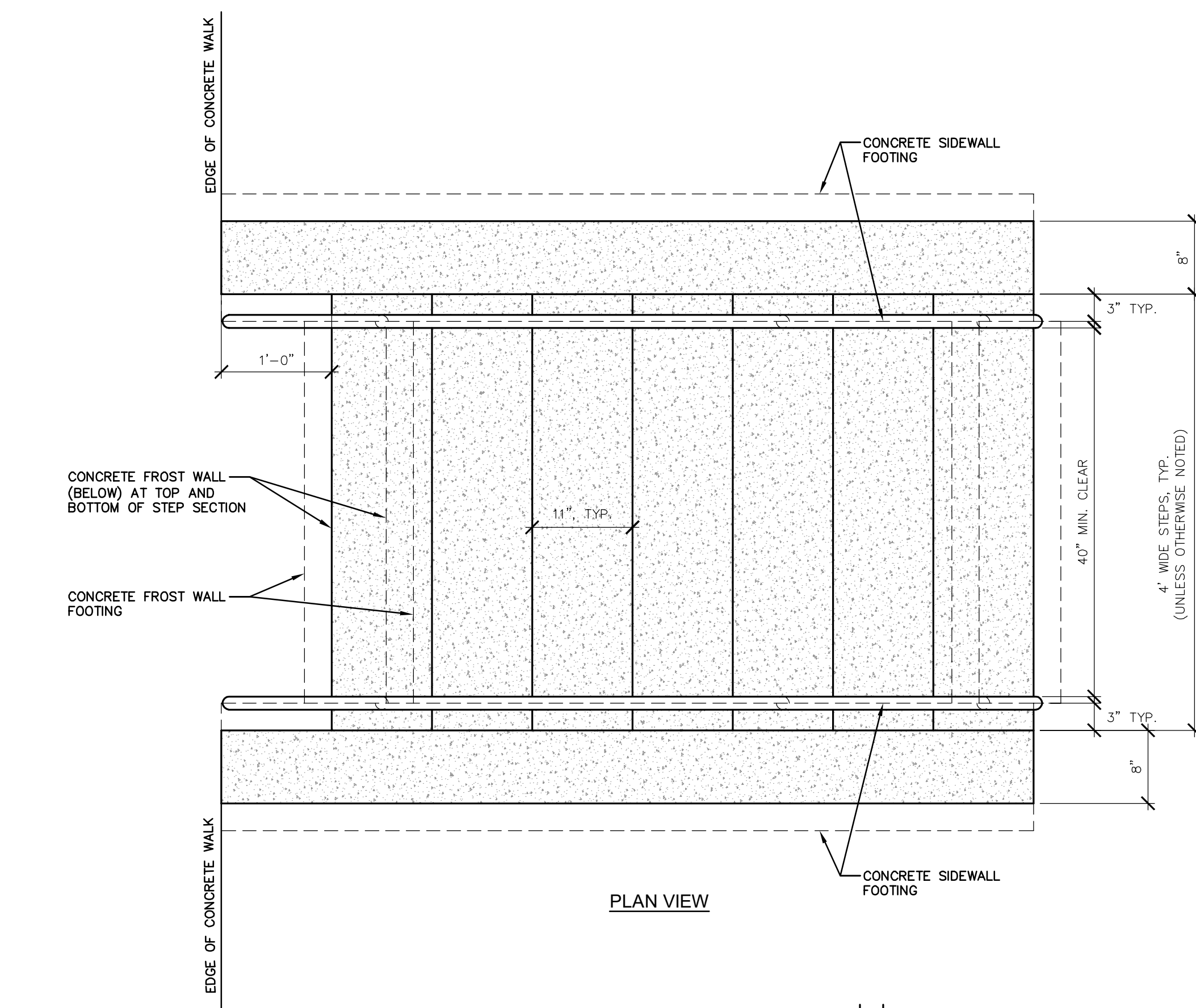
PROJECT NO: 2023-052
SCALE: AS NOTED
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/30/2024
12/13/2024

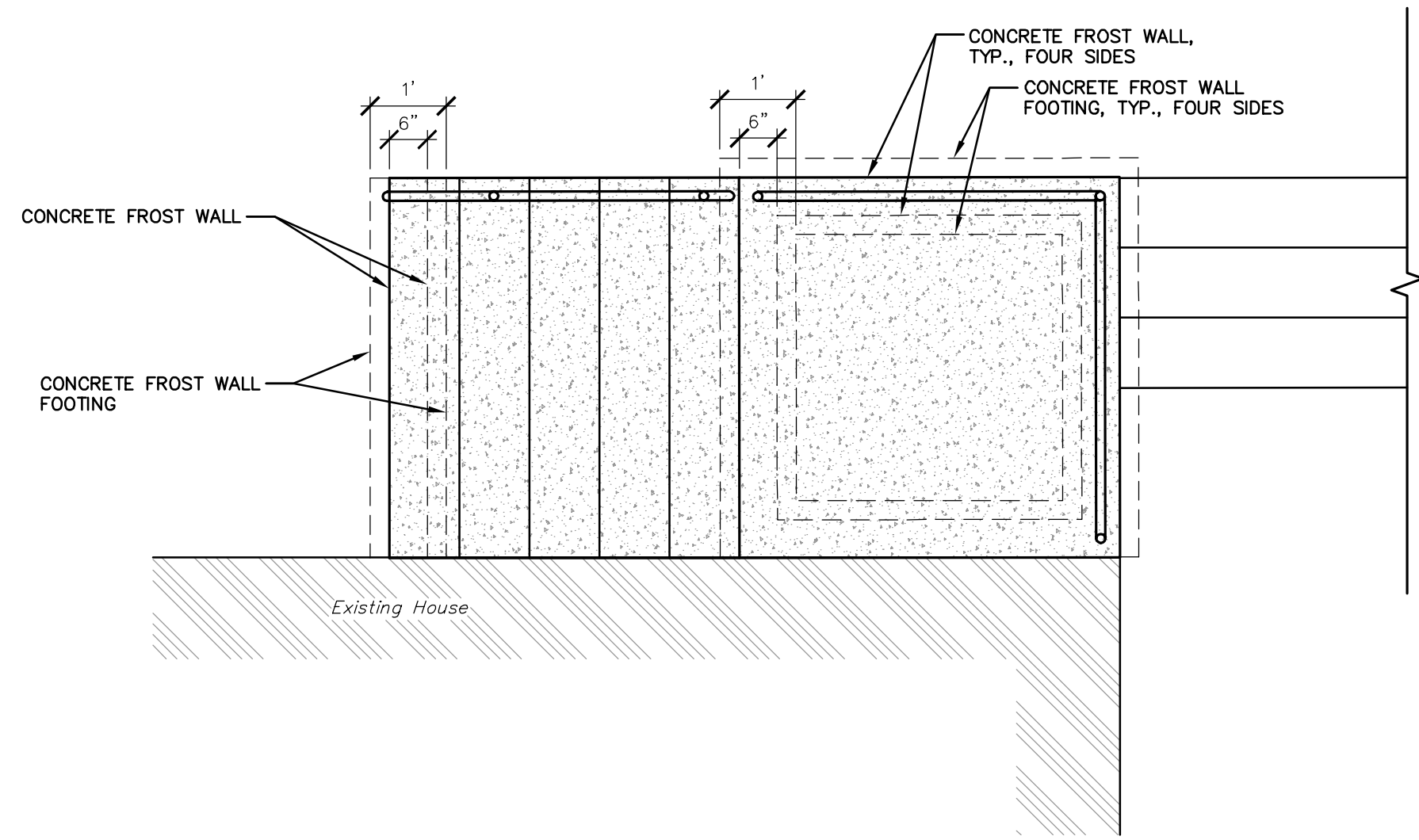
CONCRETE STEP AND
SIDEWALL DETAILS

CS-504

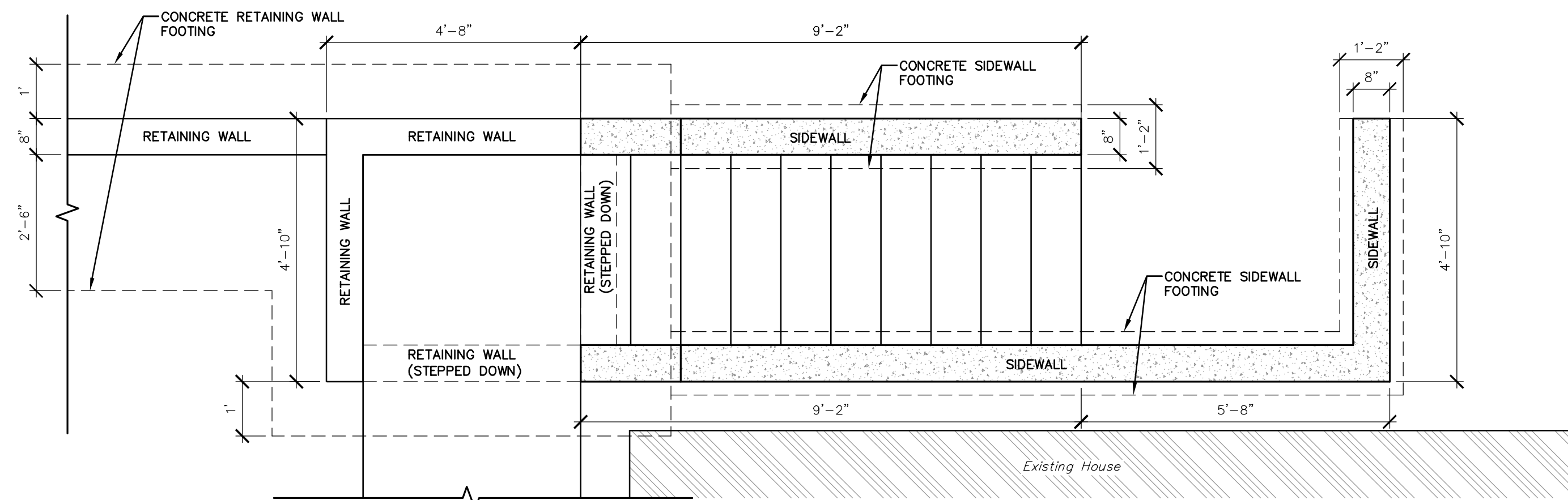
REINFORCEMENT SPLICE LENGTHS (DEVELOPMENT AND LAP SPLICES FOR TYPICAL STRUCTURES)		
SIZE	HORIZONTAL BARS*	OTHER BARS*
#4	24" (2'-0")	18" (1'-6")
#5	30" (2'-6")	24" (2'-0")
#6	36" (3'-0")	30" (2'-6")
#7	54" (4'-6")	42" (3'-6")
#8	60" (5'-0")	48" (4'-0")
#9	72" (6'-0")	54" (4'-6")
#10	84" (7'-0")	60" (5'-0")

HORIZONTAL BARS INCLUDE:
1. HORIZONTAL WALL REINFORCING BARS.
2. TOP REINFORCING BARS IN BEAMS.
3. TOP REINFORCING BARS IN SLABS AND WALL FOOTINGS GREATER THAN 12" THICK.

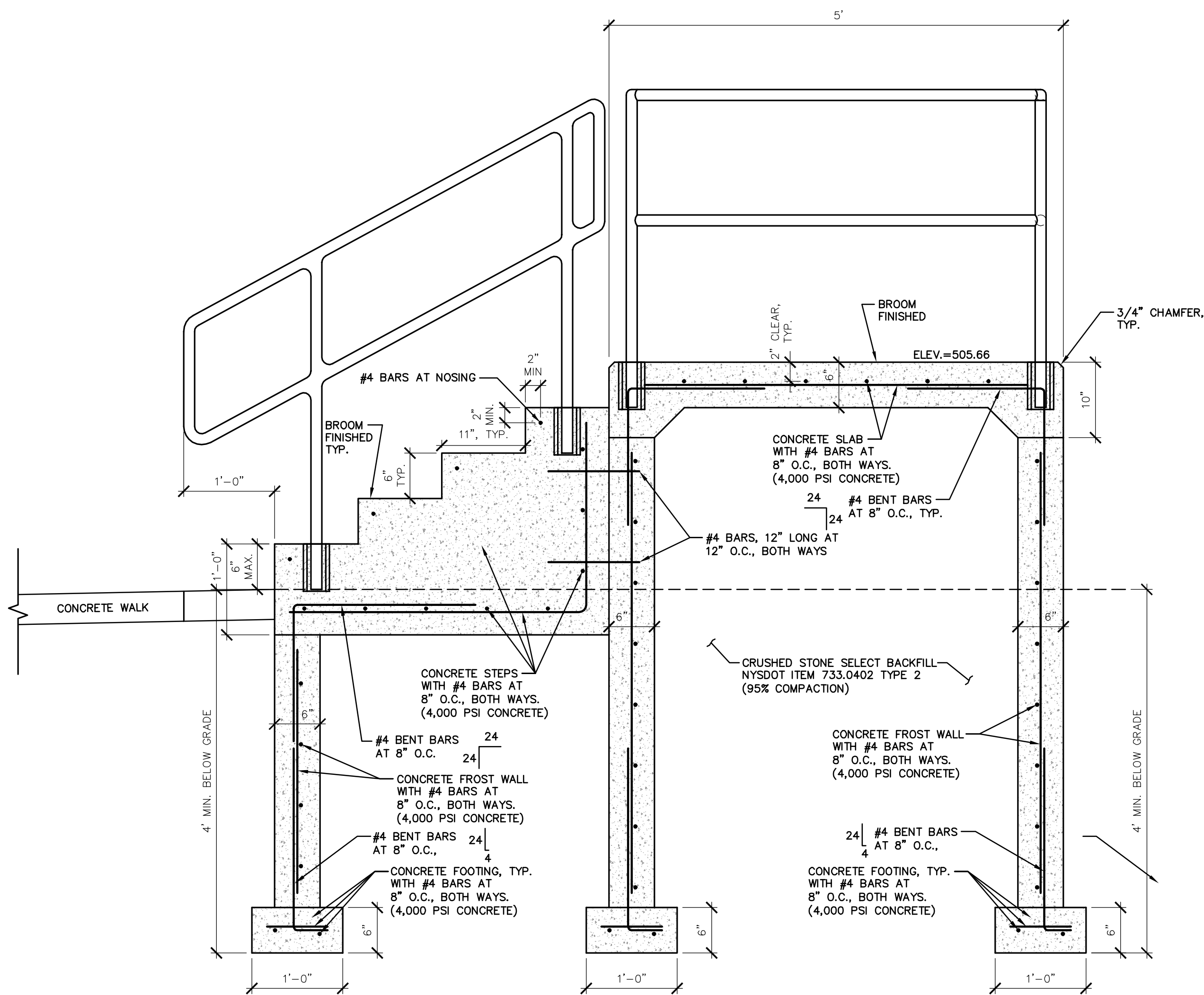




PLAN VIEW

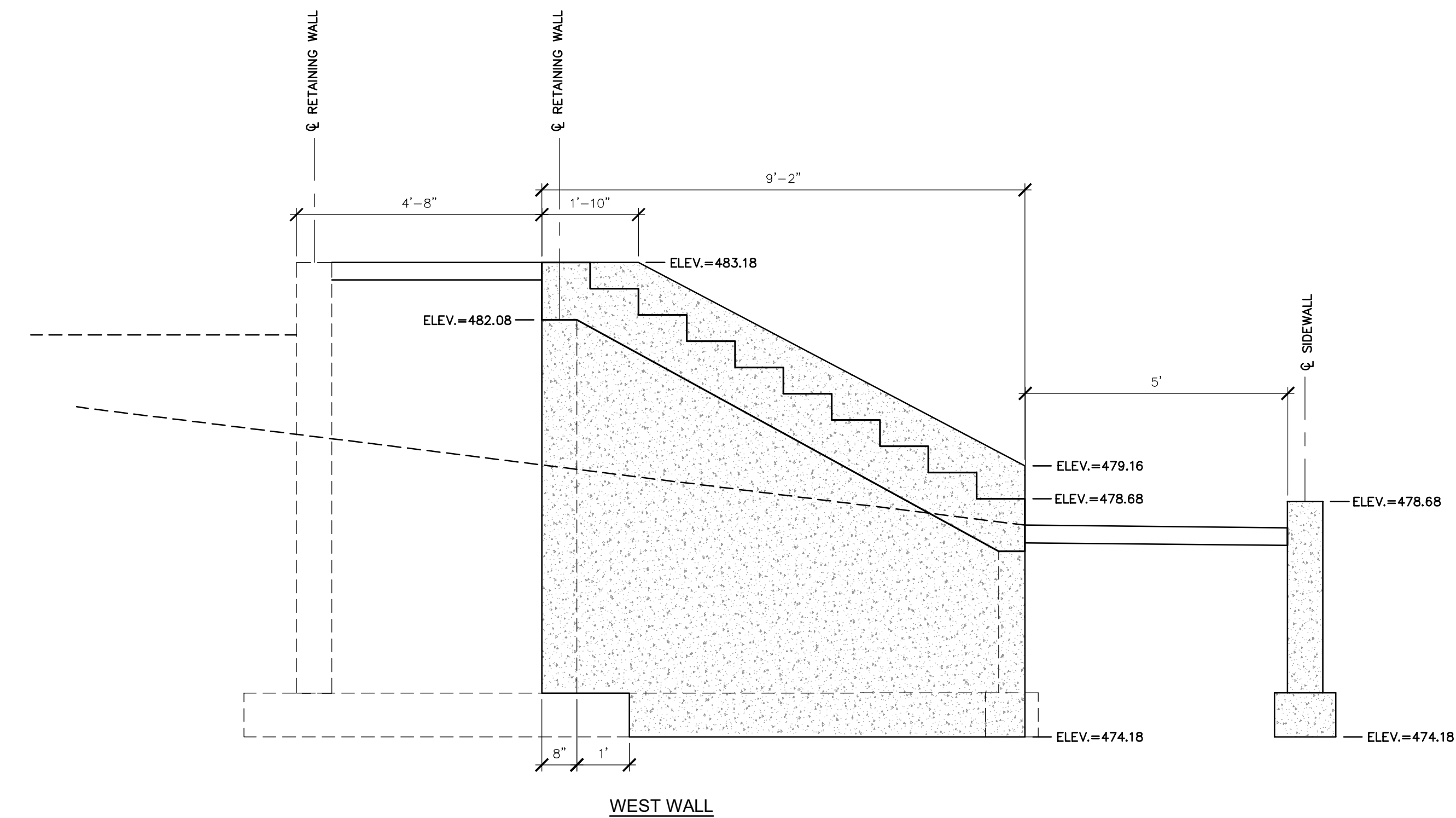


PLAN VIEW

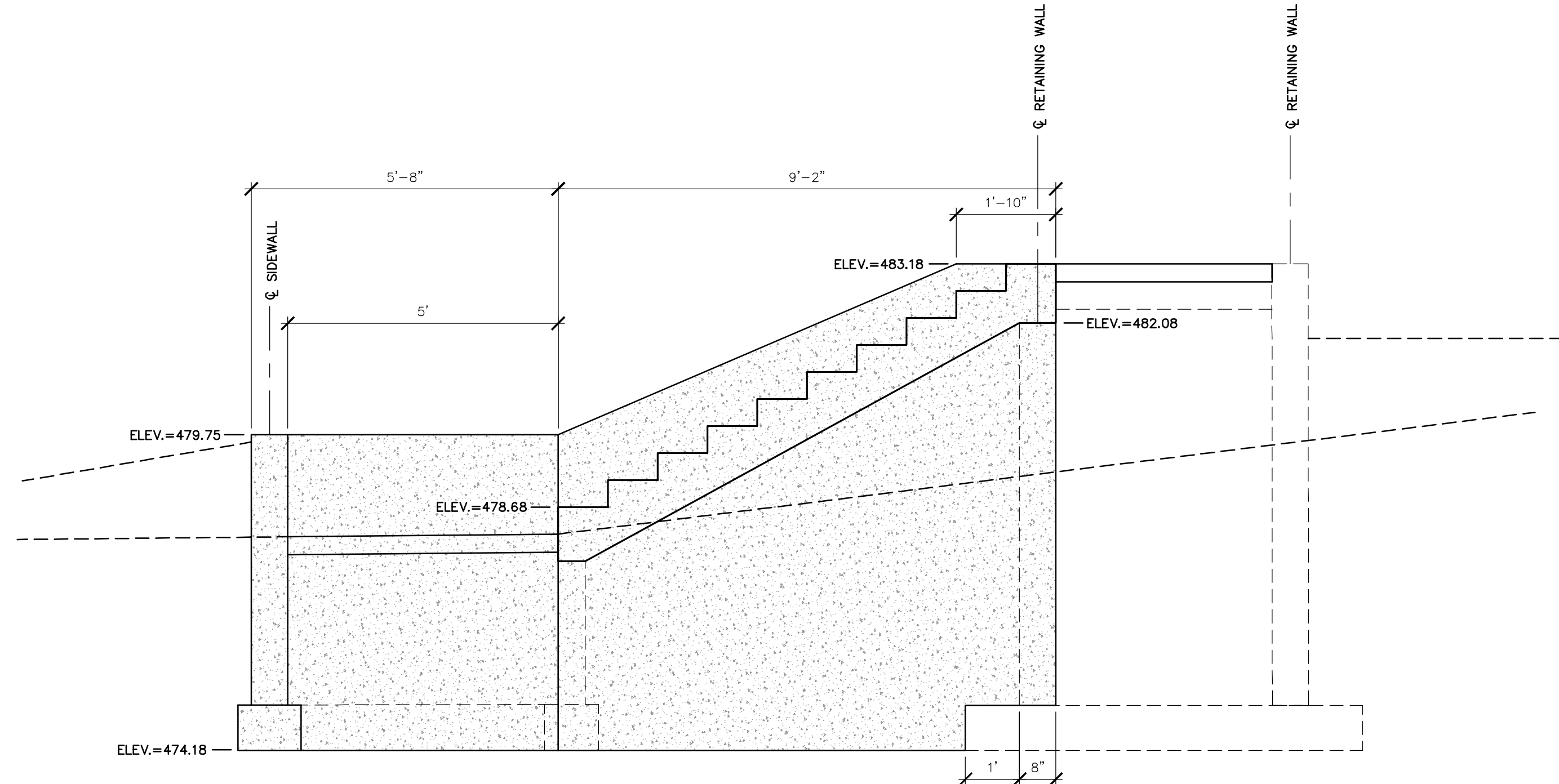


ELEVATION VIEW

1 CONCRETE STEPS DETAIL AT #237 EAST MAIN STREET
NOT TO SCALE



WEST WALL



EAST WALL

2 CONCRETE STEPS DETAIL AT #644/646 BURLINGTON STREET
NOT TO SCALE

12/13/2024 BID DOCUMENTS
FOR CONSTRUCTION



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Small Business (WOSB)

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**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052

SCALE: AS NOTED

DRAWN BY: TFT

CHECKED BY: MRM

ISSUE DATES:
02/23/2024
04/23/2024
12/13/2024

CONCRETE STEP DETAILS

CS-505

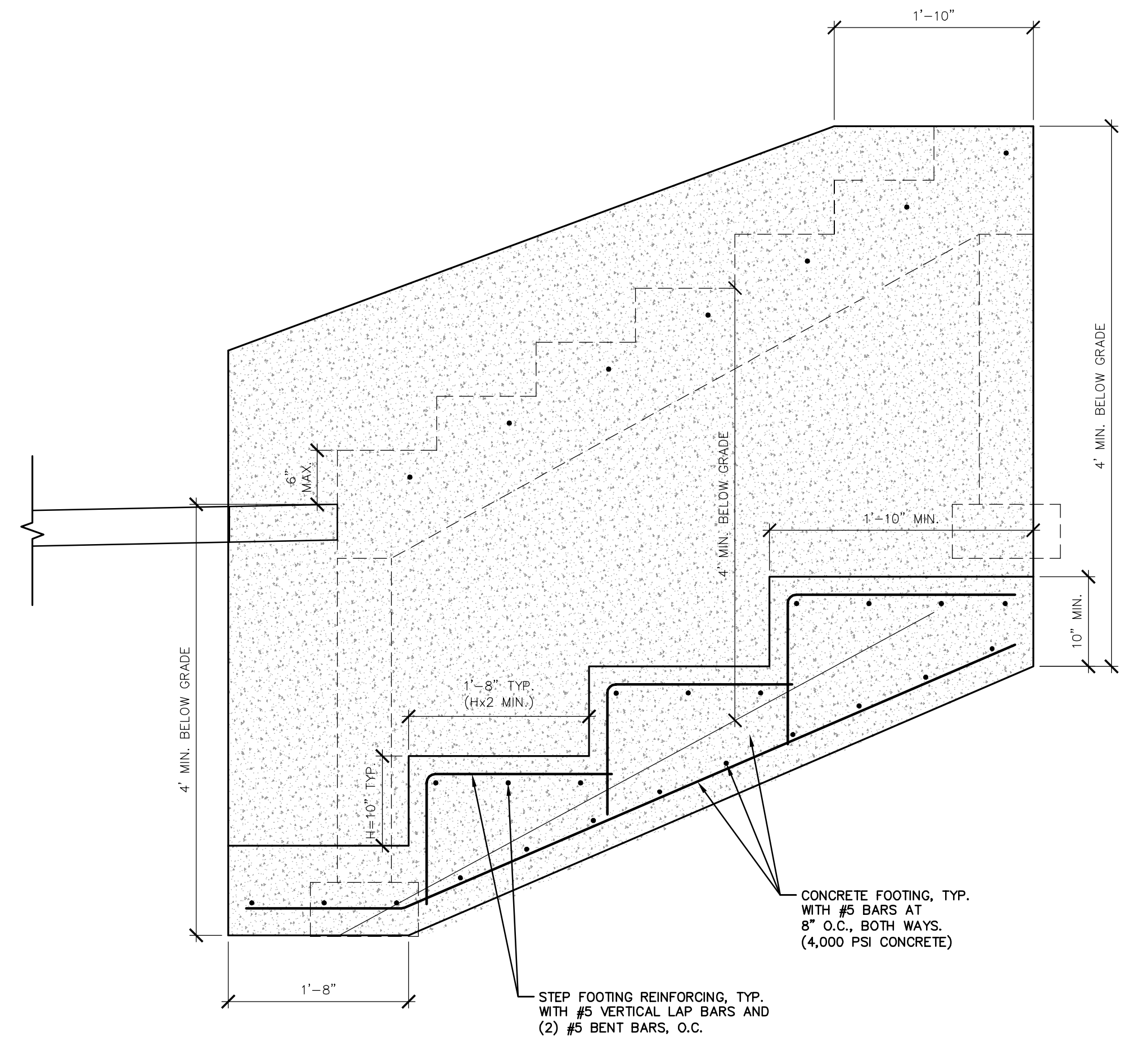


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

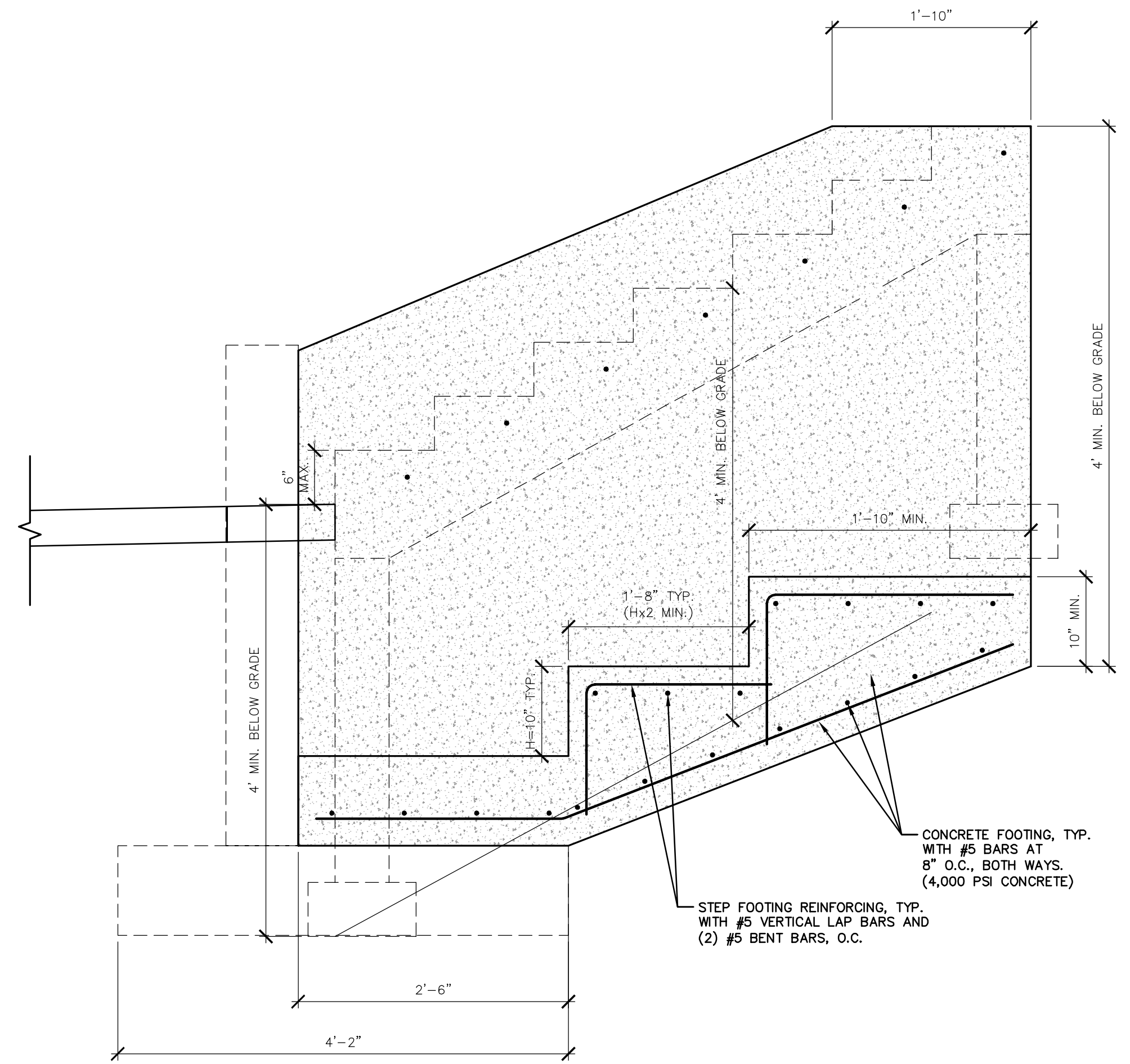
PROJECT NO.	2023-052
SCALE	AS NOTED
DRAWN BY	TFT
CHECKED BY	MRM
ISSUE DATES	02/23/2024 04/23/2024 12/13/2024

CONCRETE STEP SIDEWALL
DETAILS

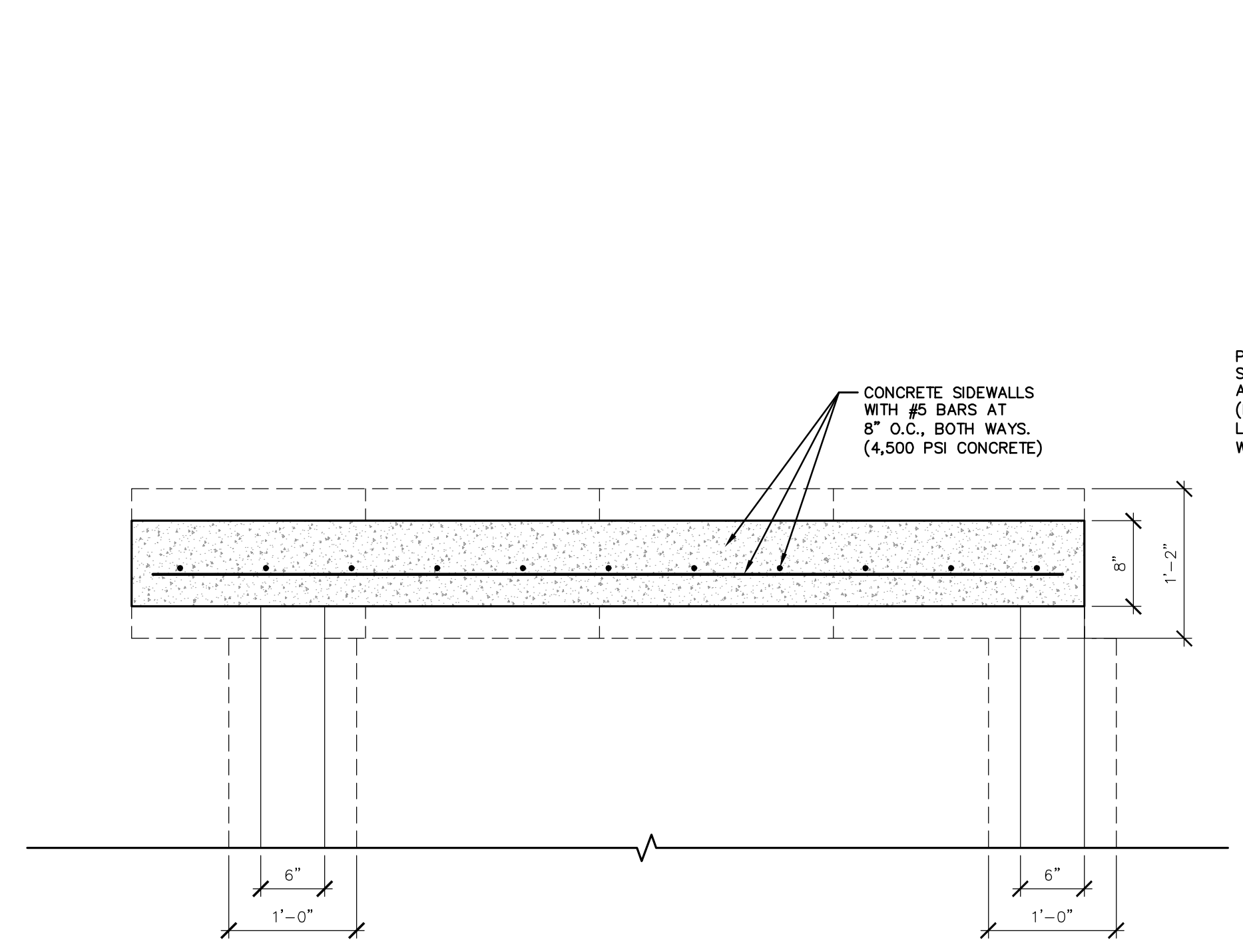
CS-506



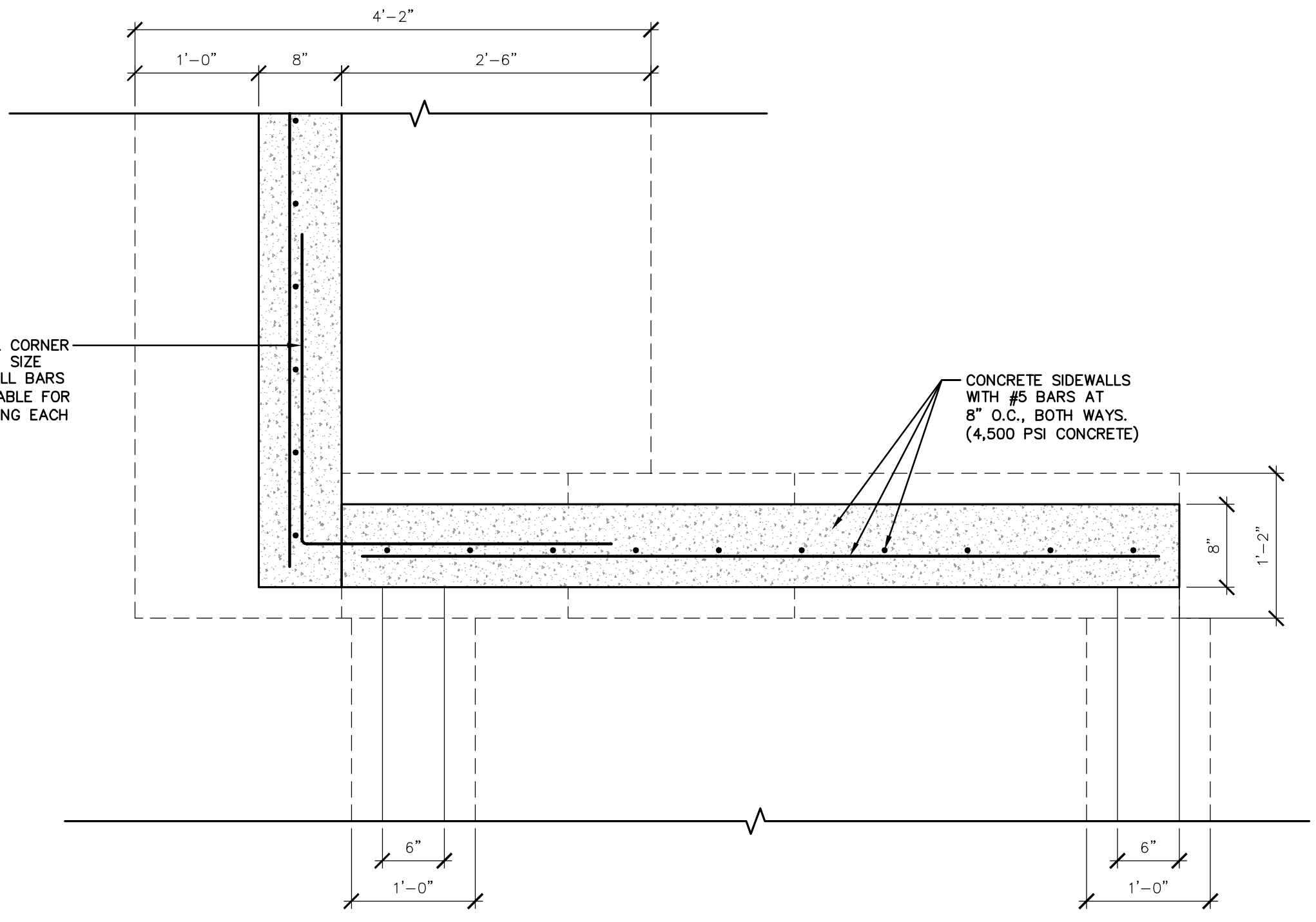
ELEVATION VIEW



ELEVATION VIEW ADJACENT TO RETAINING WALL



PLAN VIEW



PLAN VIEW ADJACENT TO RETAINING WALL

1 TYPICAL CONCRETE STEP SIDEWALL DETAIL
NOT TO SCALE

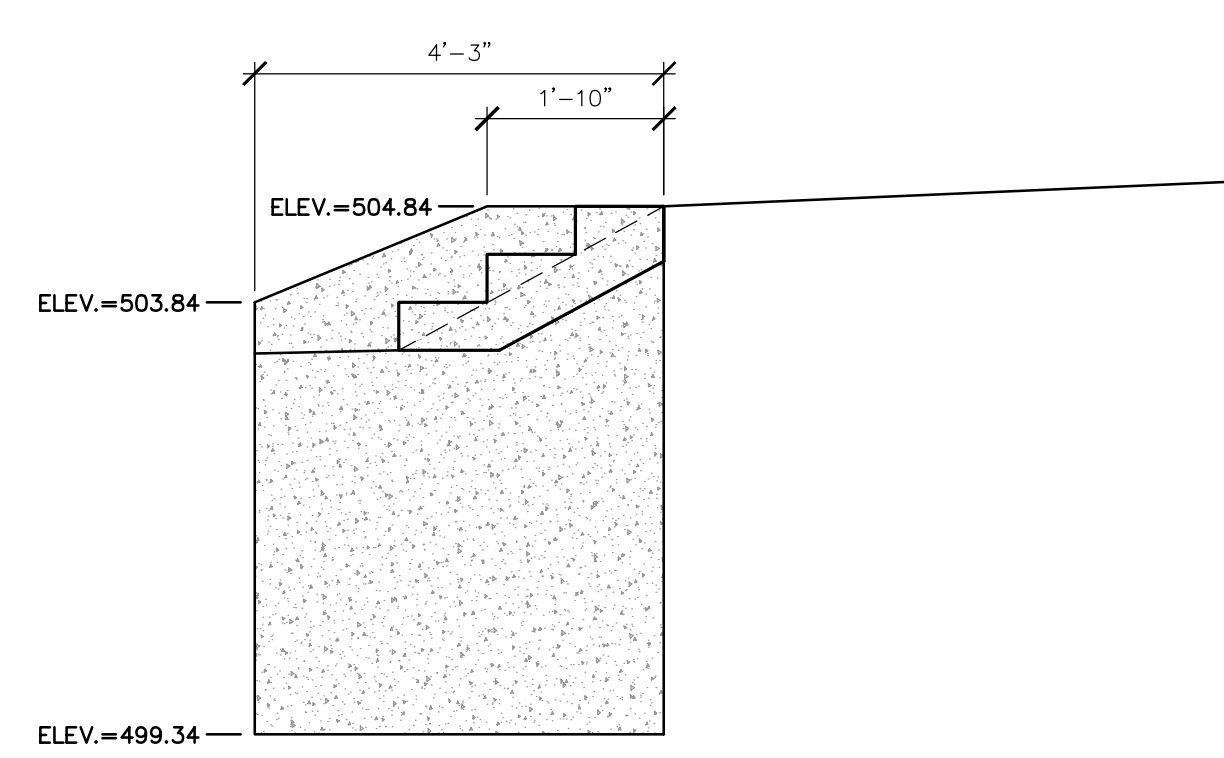


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

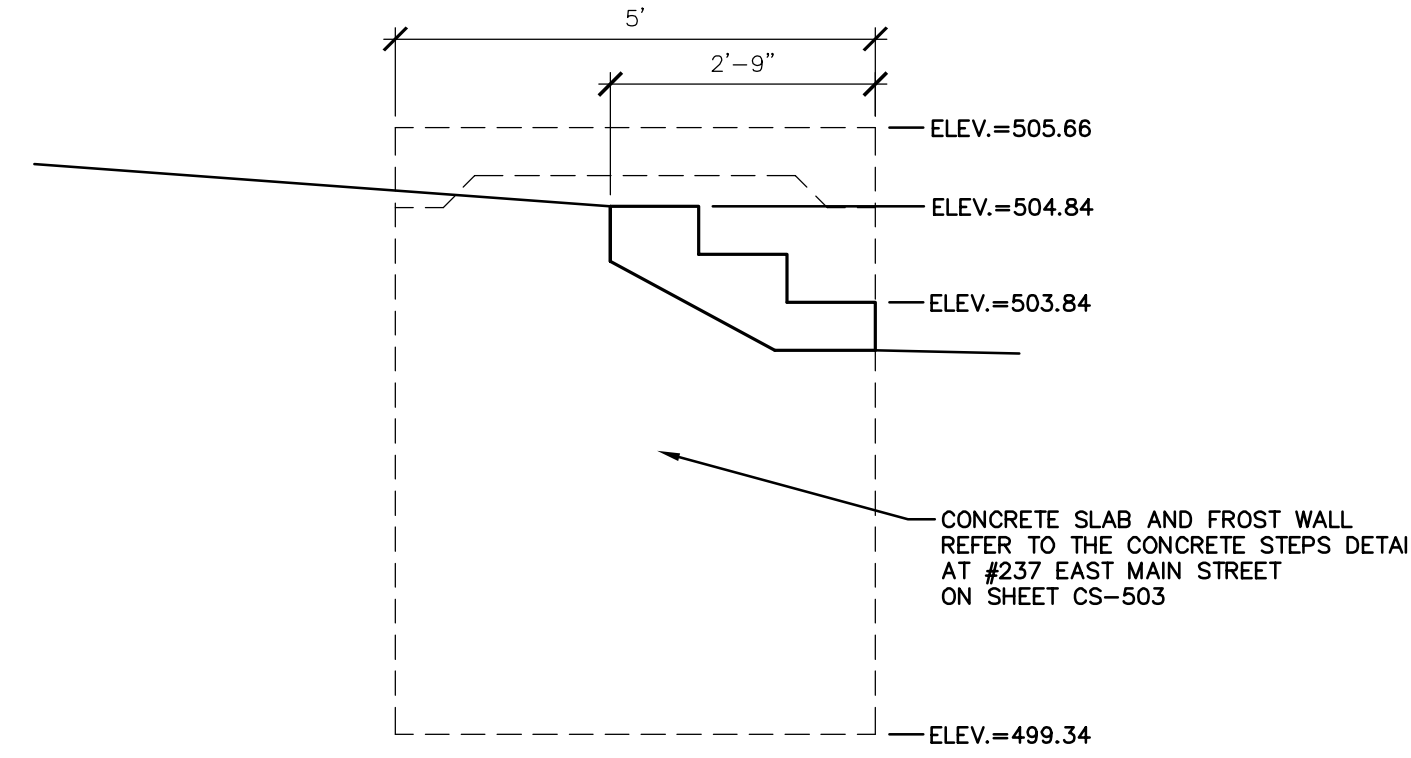
PROJECT NO:	2023-052
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DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/23/2024 12/13/2024

CONCRETE STEP SIDEWALL
CROSS SECTIONS

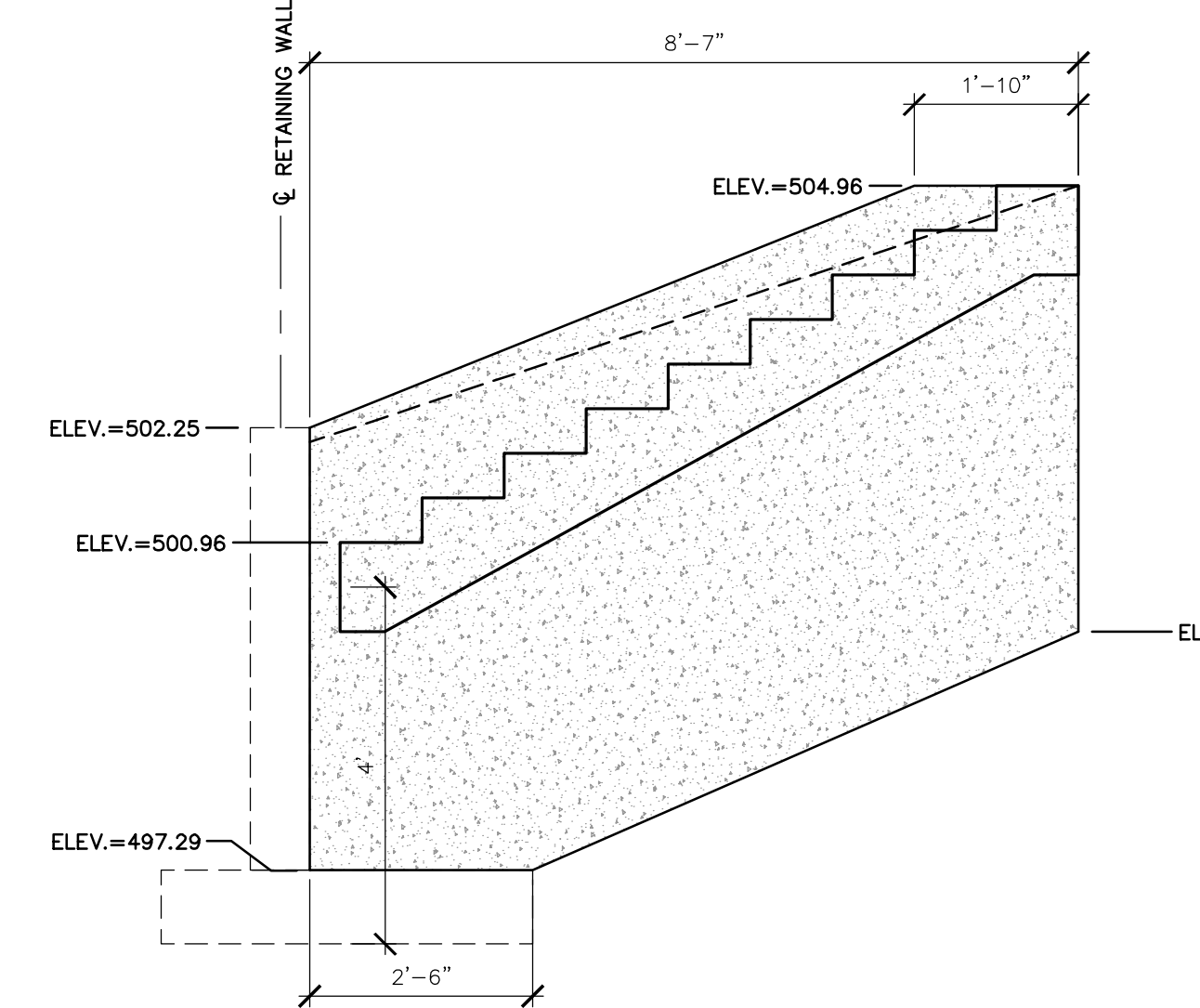
CS-507



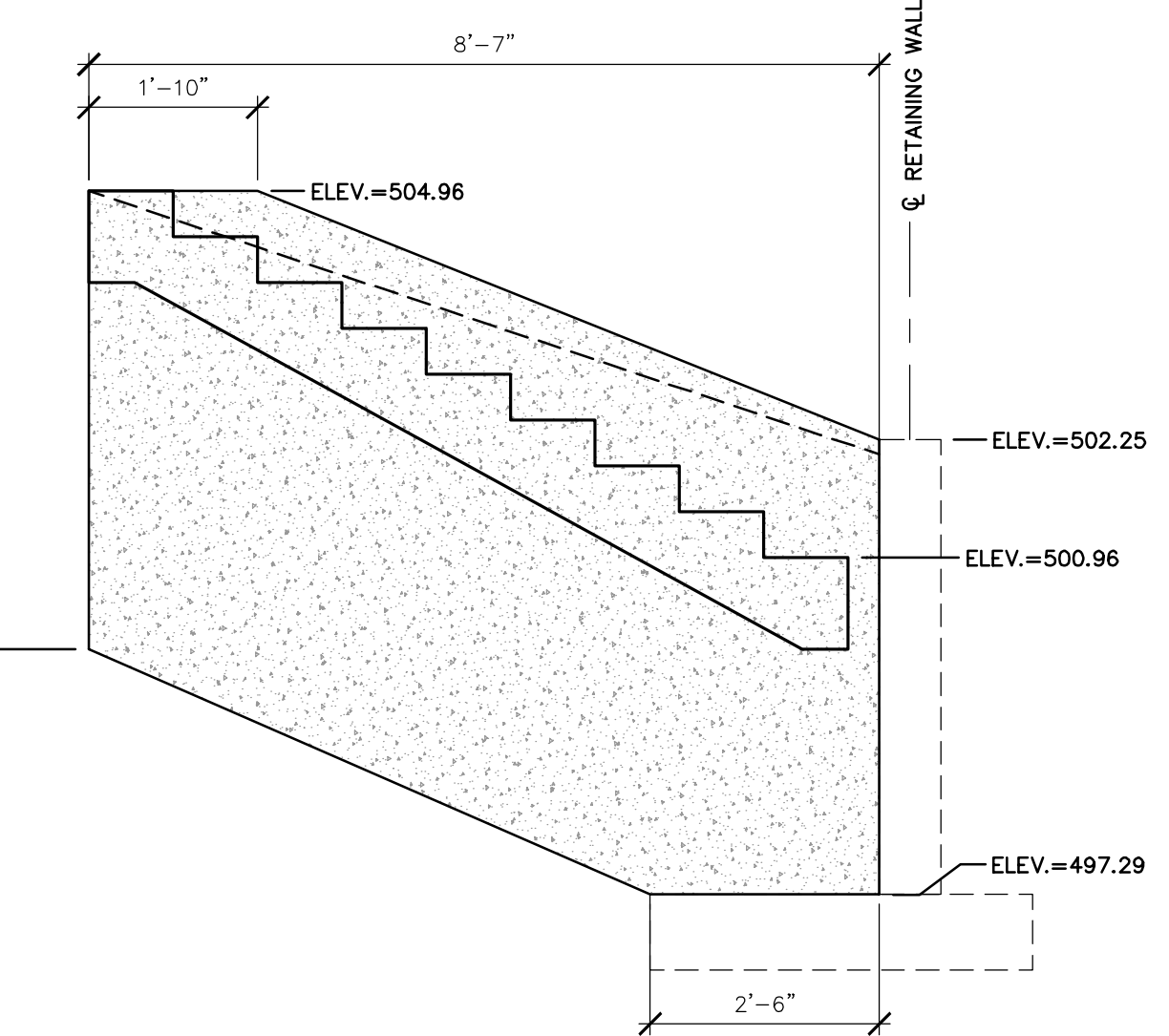
STEPS AT STA.0+96
NORTH WALL



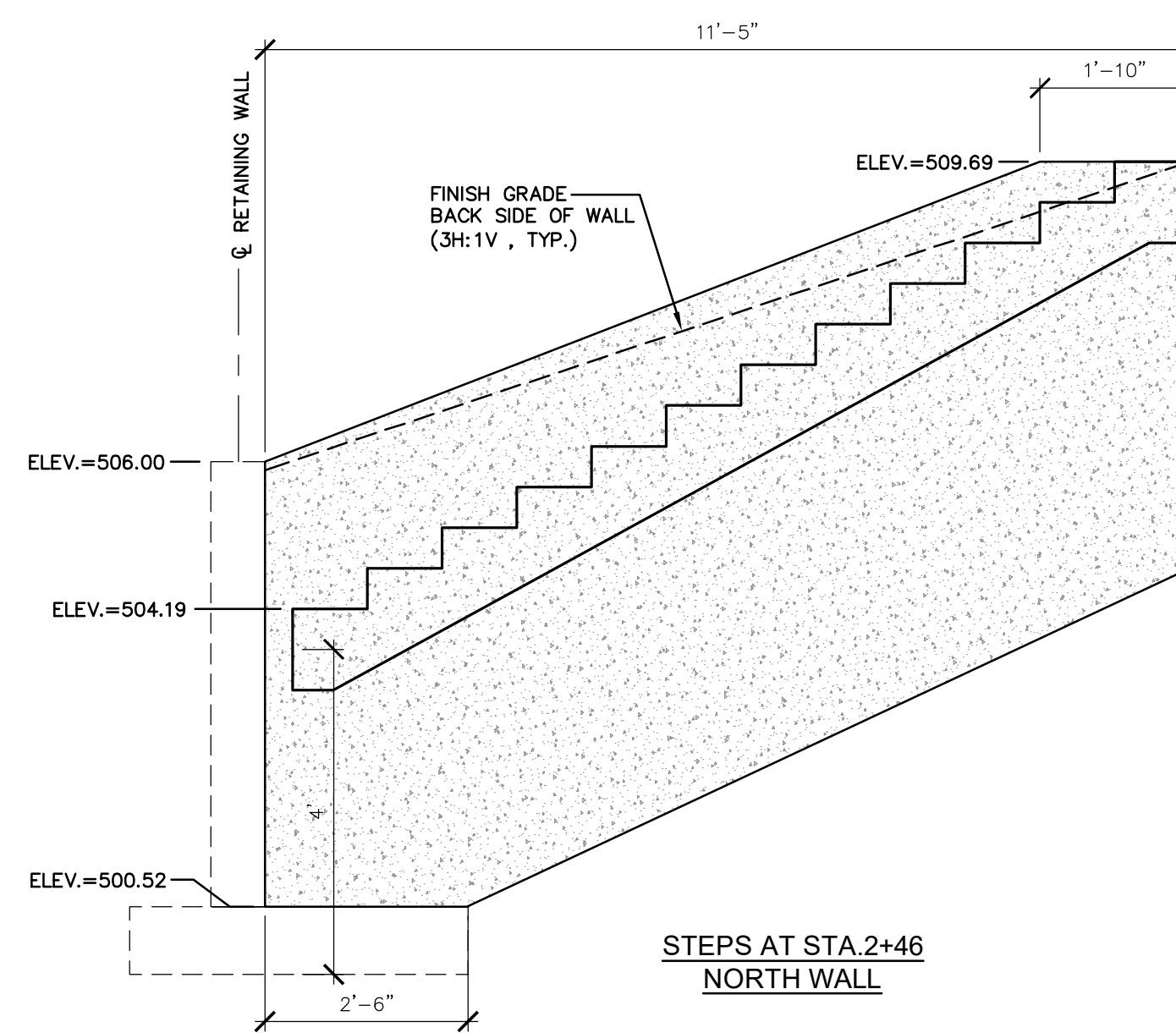
STEPS AT STA.0+96
SOUTH WALL



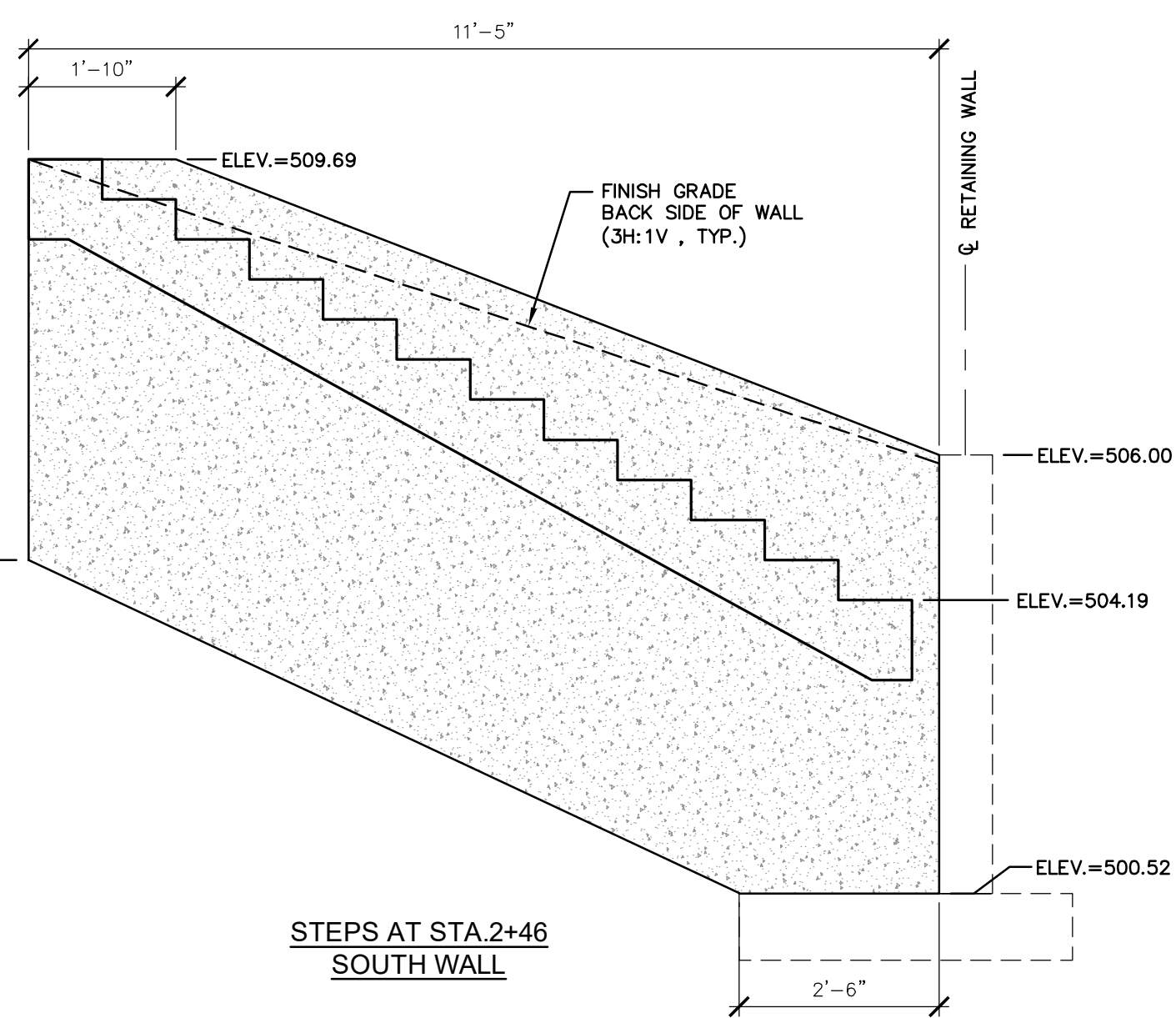
STEPS AT STA.3+81
NORTH WALL



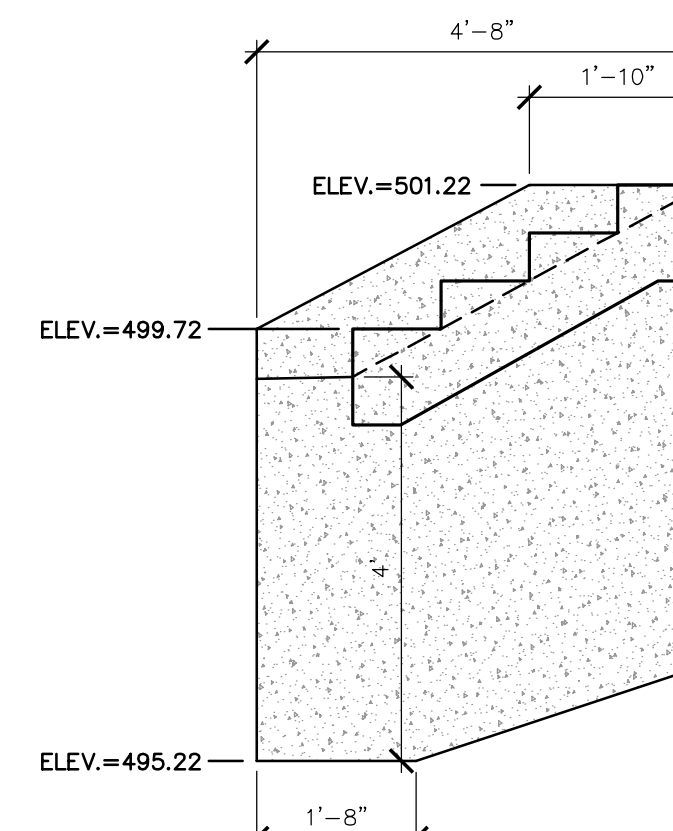
STEPS AT STA.3+81
SOUTH WALL



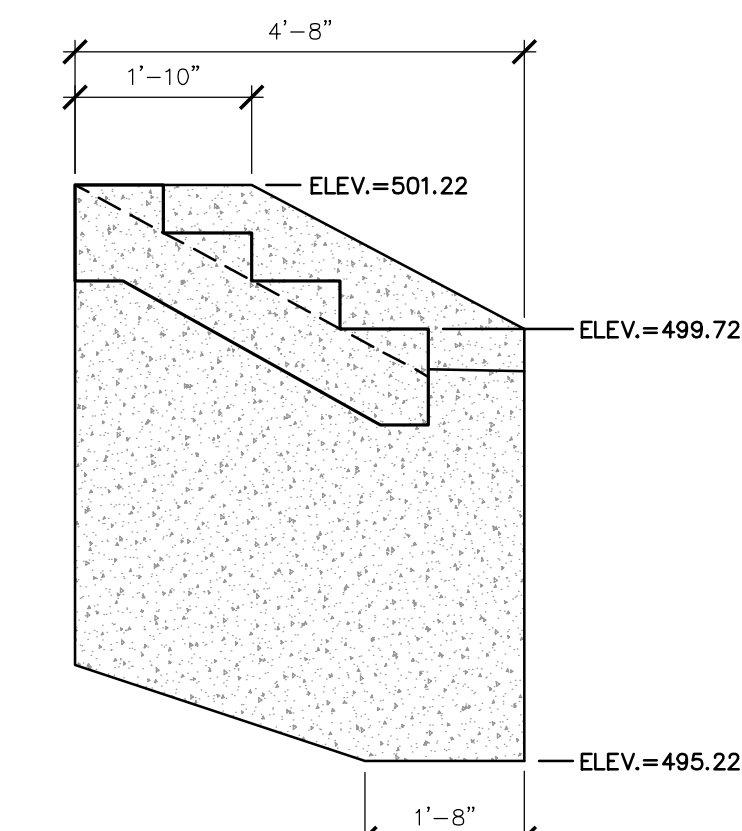
STEPS AT STA.2+46
NORTH WALL



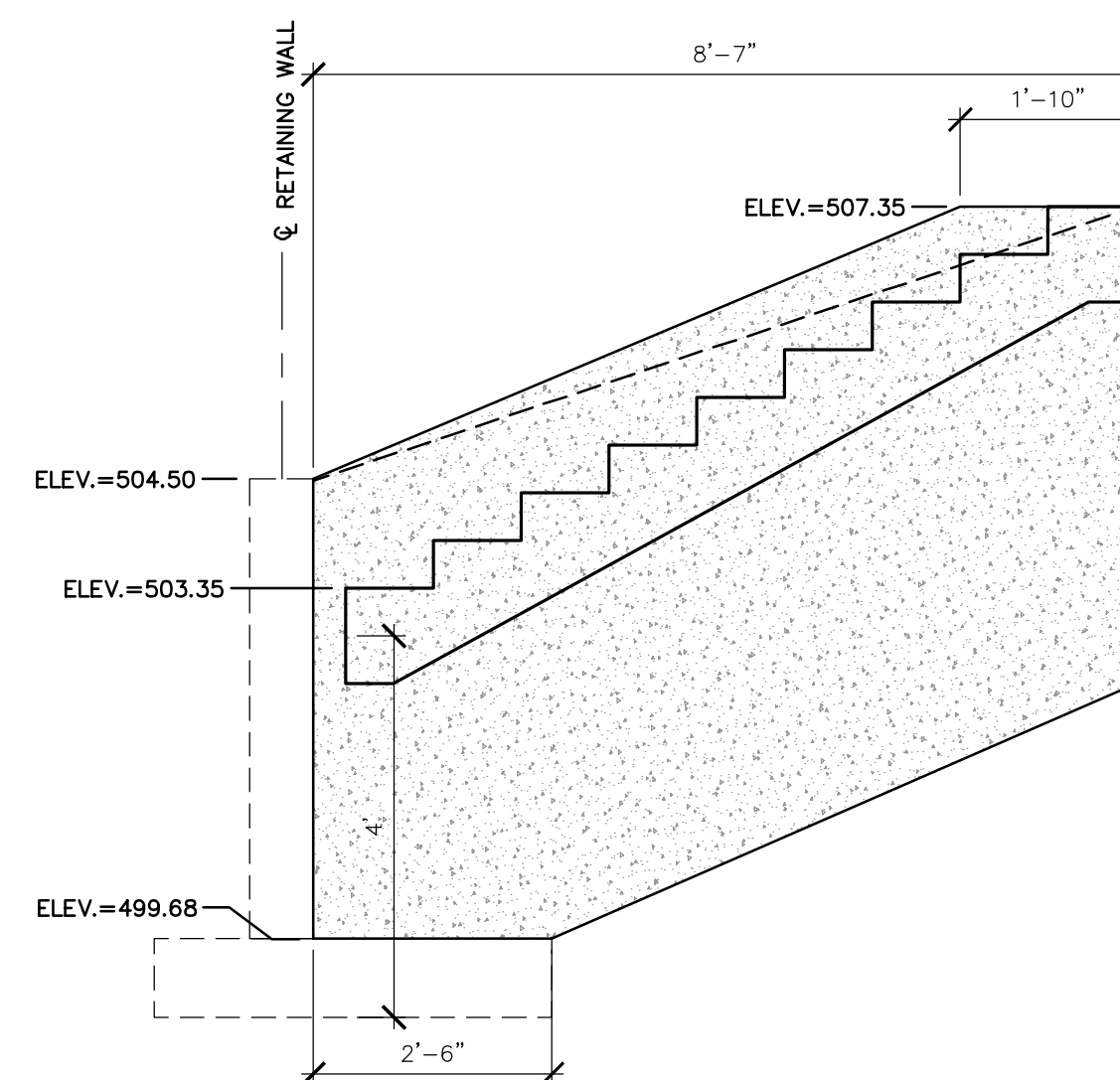
STEPS AT STA.2+46
SOUTH WALL



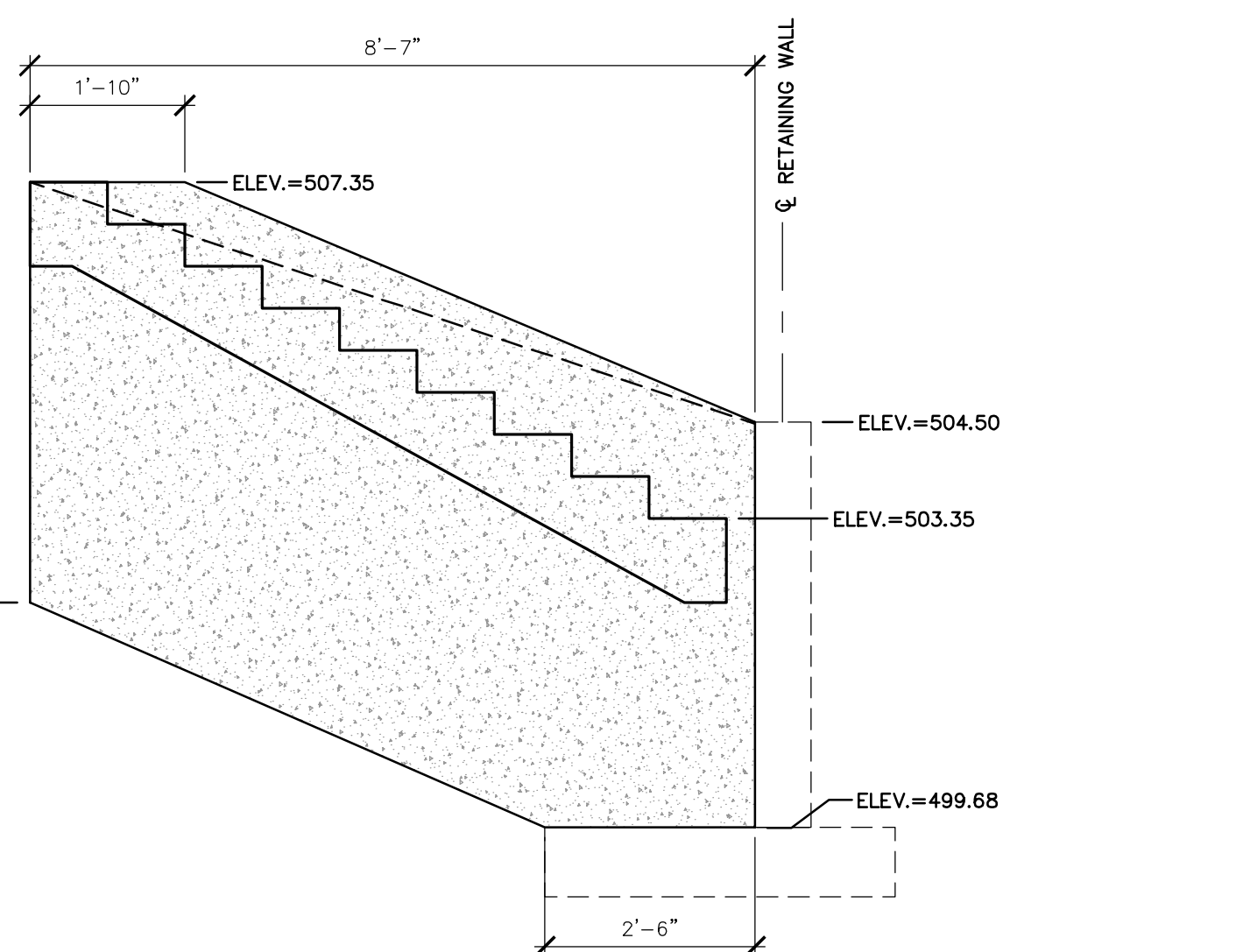
STEPS AT STA.4+22
NORTH WALL



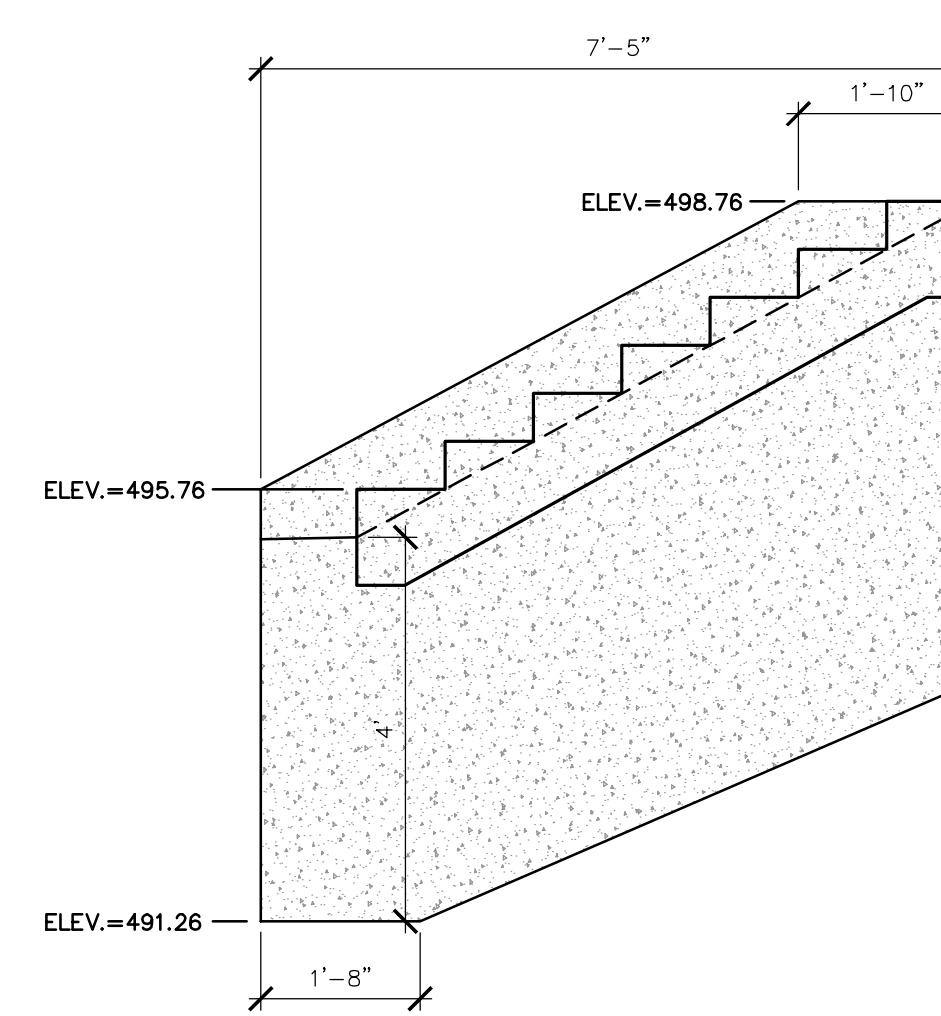
STEPS AT STA.4+22
SOUTH WALL



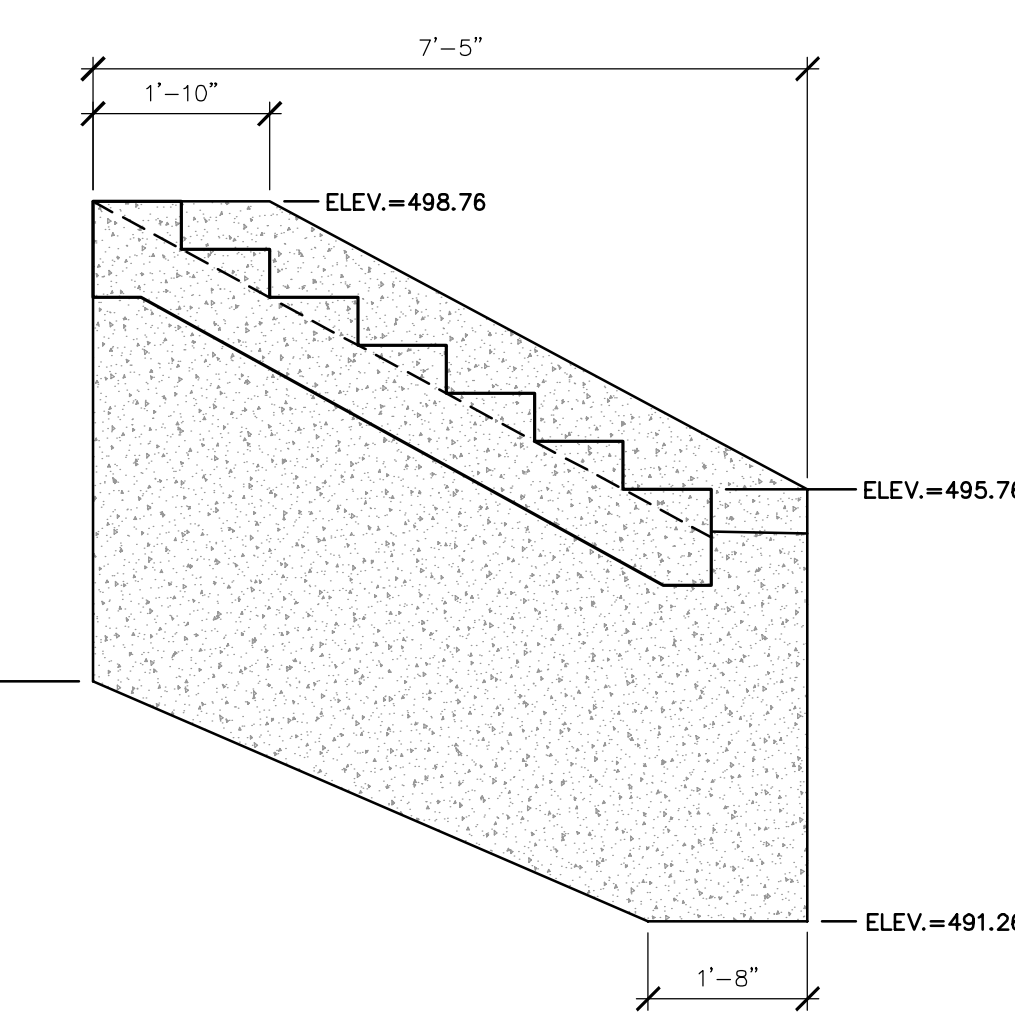
STEPS AT STA.2+88
NORTH WALL



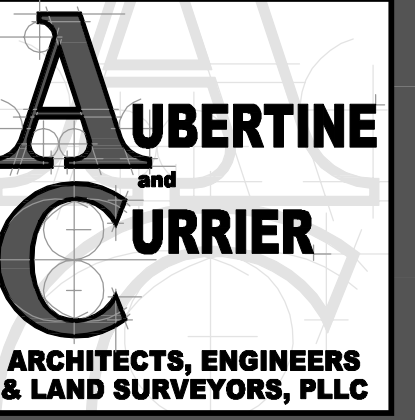
STEPS AT STA.2+88
SOUTH WALL



STEPS AT STA.4+69
NORTH WALL



STEPS AT STA.4+69
SOUTH WALL



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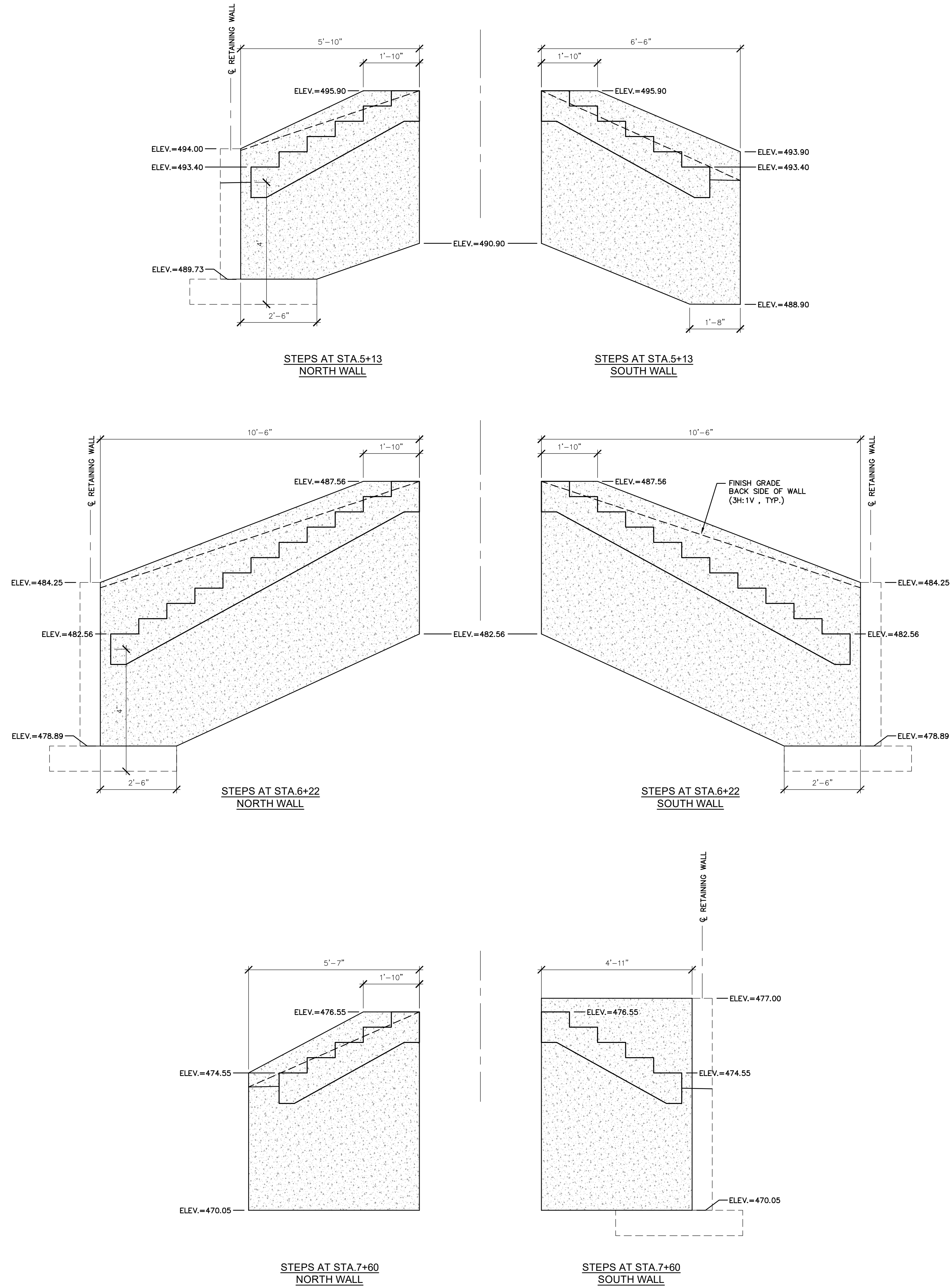


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

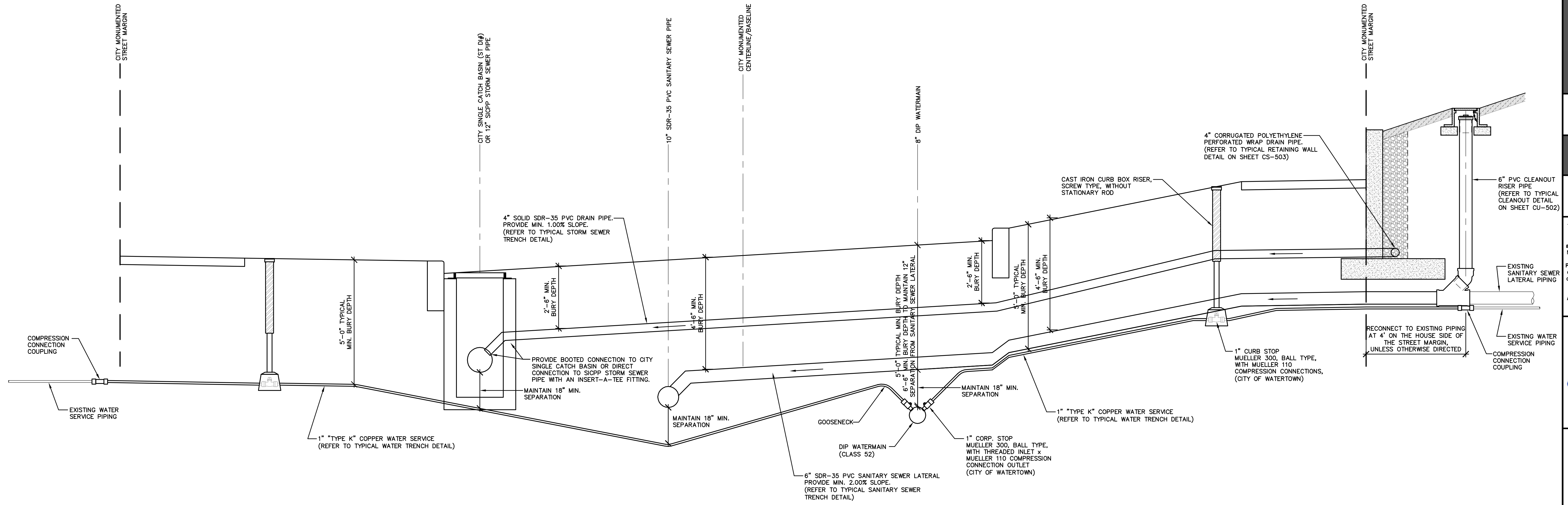
PROJECT NO: 2023-052
SCALE: AS NOTED
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02/23/2024
04/23/2024
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CONCRETE STEP SIDEWALL
CROSS SECTIONS

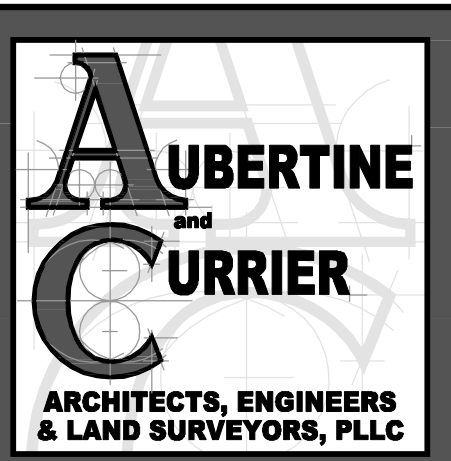
CS-508



1 CONCRETE STEP SIDEWALL CROSS SECTIONS
NOT TO SCALE



1 TYPICAL UTILITIES STREET CROSS SECTION
NOT TO SCALE

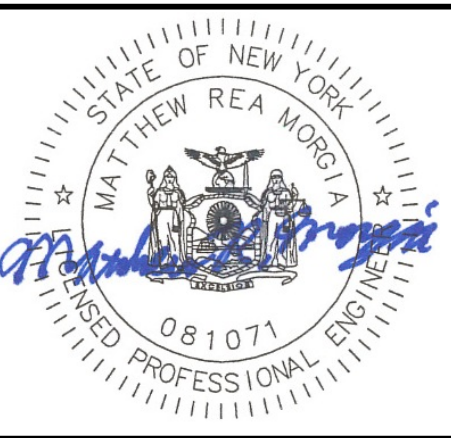


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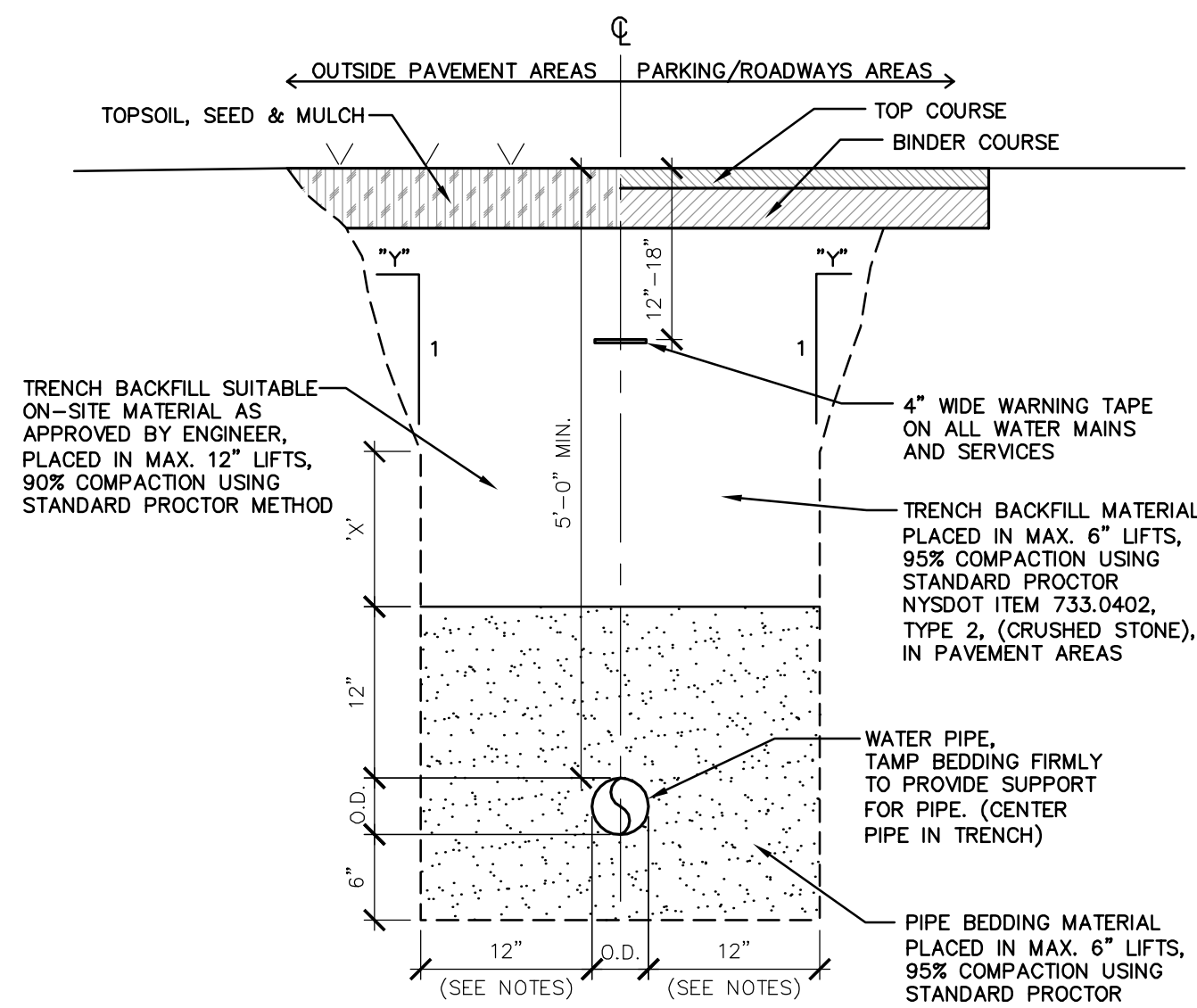


**BURLINGTON STREET
RECONSTRUCTION PROJECT**
CITY OF WATERTOWN
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JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
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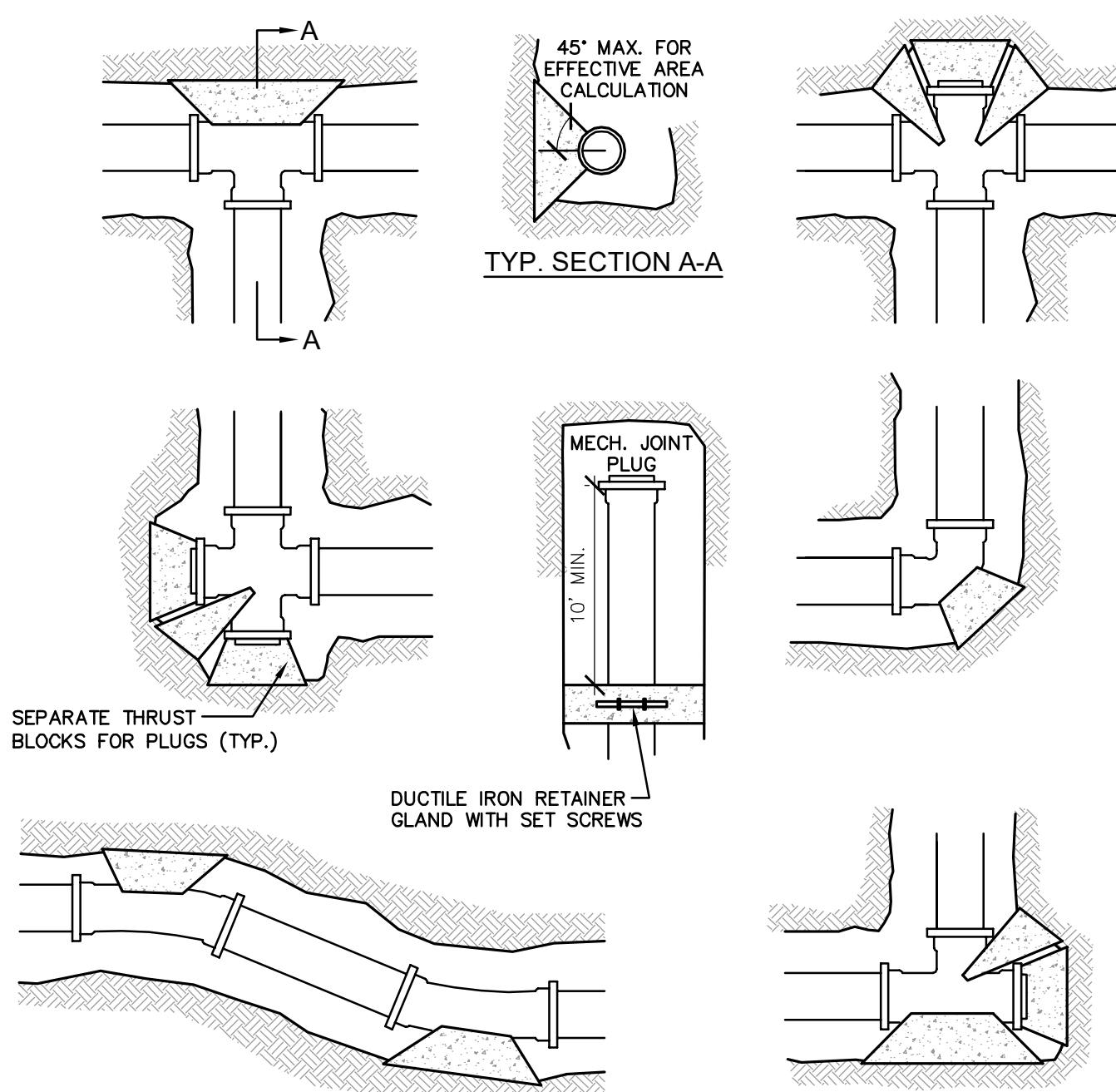
TYPICAL UTILITIES
STREET CROSS SECTION

CU-500



- NOTES**
- DIMENSIONS "X" AND "Y" SHOWN ABOVE SHALL BE DETERMINED BY CONTRACTOR TO COMPLY WITH O.S.H.A., NEW YORK STATE DEPARTMENT OF LABOR, NEW YORK STATE INDUSTRIAL CODE AND ALL OTHER APPLICABLE SAFETY STANDARDS.
 - SAFETY SHEETING OR TRENCH BOX MAY BE USED IN PLACE OF SLOPED TRENCH WALLS.
 - SHEETING, WHEN REQUIRED, TO BE CUT OFF AT LEAST 5 FEET BELOW STREET AND A MINIMUM OF 1 FOOT ABOVE TOP OF PIPE. WOOD SHEETING DRIVEN BELOW MID-DIAMETER OF THE PIPE SHALL BE LEFT IN PLACE. STEEL SHEETING DRIVEN BELOW MID-DIAMETER MAY BE WITHDRAWN IF APPROVED IN WRITING BY THE ENGINEER. FOR PVC PIPE ALL SHEETING DRIVEN BELOW MID-DIAMETER SHALL BE LEFT IN PLACE.
 - TRENCHES LOCATED WITHIN 5' OF ROAD SHOULDERS SHALL BE TREATED THE SAME AS UNDER PAVEMENT.
 - PIPE TO TRENCH WALL DISTANCE MAY BE REDUCED WHEN INSTALLED IN SAWCUT ROCK TRENCH.
 - PROVIDE WARNING TAPE AT ALL UNDERGROUND UTILITIES.
 - DUCTILE IRON PIPE SHALL BE CLASS 52 WITH FOUR (4) BRONZE WEDGES PER JOINT.
 - COPPER WATER SERVICE PIPING SHALL BE TYPE K, SOFT.

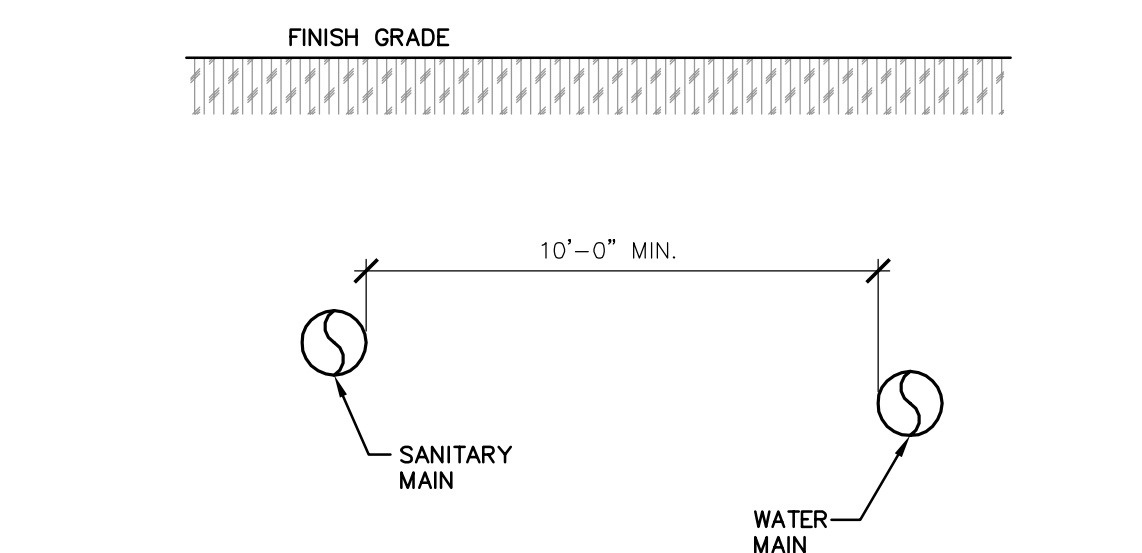
1 TYPICAL WATER TRENCH DETAIL
NOT TO SCALE



ANCHOR SCHEDULE

NOMINAL PIPE SIZE	150 P.S.I. HYDROSTATIC PRESSURE AGAINST UNDISTURBED SOIL, AT 2,000 P.S.F. BRNG. CAPACITY					150 P.S.I. HYDROSTATIC PRESSURE AGAINST ROCK TRENCH, AT 10,000 P.S.F. BRNG. CAPACITY				
	11-1/4" BEND	22-1/2" BEND	45° BEND	90° BEND	PLUG OR TEE	11-1/4" BEND	22-1/2" BEND	45° BEND	90° BEND	PLUG OR TEE
4"	1.0	1.0	1.0	1.9	1.4	1.0	1.0	1.0	1.0	1.0
6"	1.0	1.1	2.1	4.0	2.8	1.0	1.0	1.0	1.0	1.0
8"	1.0	1.9	3.7	6.8	4.8	1.0	1.0	1.0	1.4	1.0
10"	1.4	2.8	5.6	10.3	7.3	1.0	1.0	1.1	2.1	1.5
12"	2.0	4.0	7.9	14.5	10.3	1.0	1.0	1.6	2.9	2.1
14"	2.7	5.4	10.6	19.5	13.8	1.0	1.1	2.1	3.9	2.8
16"	3.5	7.0	13.6	25.2	17.8	1.0	1.4	2.7	5.0	3.6
18"	4.4	8.7	17.1	31.7	22.4	1.0	1.7	3.4	6.3	4.5
20"	5.4	10.7	21.0	38.9	27.5	1.2	2.1	4.2	7.8	5.5
24"	7.7	15.3	30.0	55.5	39.2	1.5	3.1	6.0	11.1	7.8

4 TYPICAL HORIZONTAL THRUST BLOCK DETAIL
NOT TO SCALE



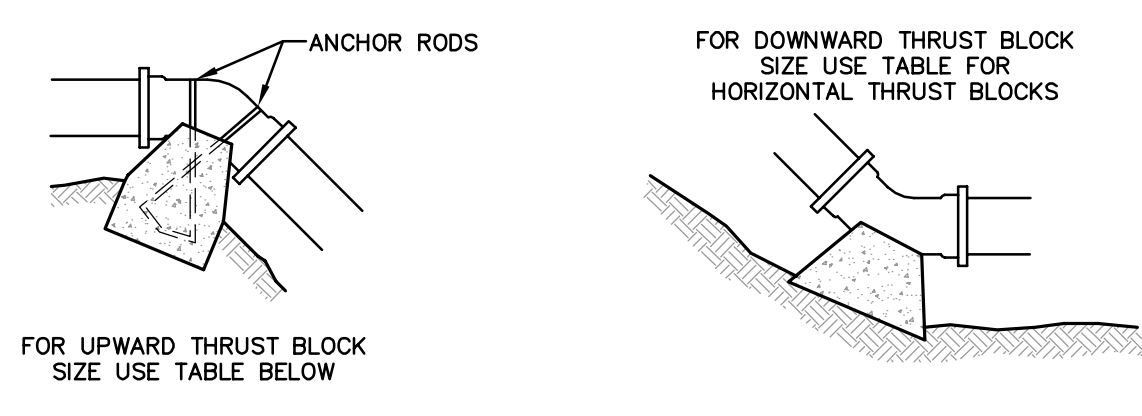
2 TYPICAL WATER / SEWER CROSSING HORIZONTAL SEPARATION DETAIL
NOT TO SCALE

WATER MAIN / SANITARY SEWER DETAIL

CONDITION	SCHEMATIC	REQUIREMENTS
I WATER LINE ABOVE SEWER LINE		A.) WATER LINE AND SEWER LINE PIPE LENGTHS TO BE CENTERED AT CROSSING. PIPE SECTIONS SHALL BE MIN. 20' LONG WITH EACH PIPE JOINTS AT MIN. 10' FROM CROSSING.
II WATER LINE ABOVE SEWER LINE		A.) WATER LINE AND SEWER LINE PIPE LENGTHS TO BE CENTERED AT CROSSING. PIPE SECTIONS SHALL BE MIN. 20' LONG WITH EACH PIPE JOINTS AT MIN. 10' FROM CROSSING. B.) SEWER LINE TO BE AWWA C900 WATER WORKS GRADE 150 PSI PRESSURE RATED PVC PIPE. PRESSURE TEST TO ENSURE WATER TIGHTNESS OR ENCASE SEWER LINE WITH CONCRETE FOR 10' MIN. EACH SIDE OF CROSSING. 6" MIN. AROUND BELL OF PIPE
III SEWER LINE ABOVE WATER LINE		A.) WATER LINE AND SEWER LINE PIPE LENGTHS TO BE CENTERED AT CROSSING. PIPE SECTIONS SHALL BE MIN. 20' LONG WITH EACH PIPE JOINTS AT MIN. 10' FROM CROSSING. B.) SEWER LINE TO BE AWWA C900 WATER WORKS GRADE 150 PSI PRESSURE RATED PVC PIPE. PRESSURE TEST TO ENSURE WATER TIGHTNESS OR ENCASE SEWER LINE WITH CONCRETE FOR 10' MIN. EACH SIDE OF CROSSING. 6" MIN. AROUND BELL OF PIPE

NOTE: IN NO CASE SHALL PIPES BE CLOSER THAN 12" APART. DISTANCES SHOULD BE MEASURED BETWEEN OUTSIDE OF PIPES

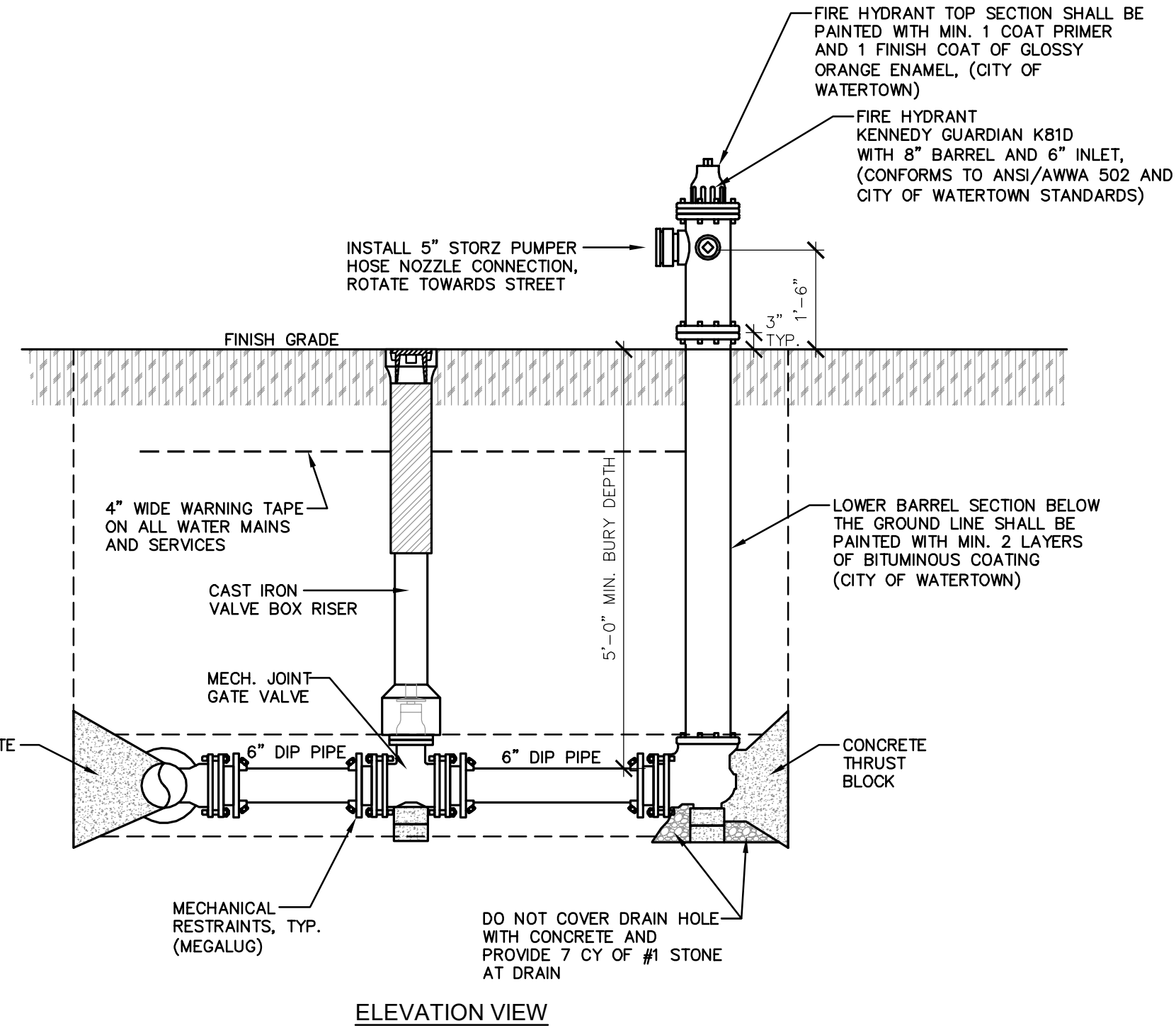
3 TYPICAL WATER / SEWER CROSSING VERTICAL SEPARATION DETAIL
NOT TO SCALE



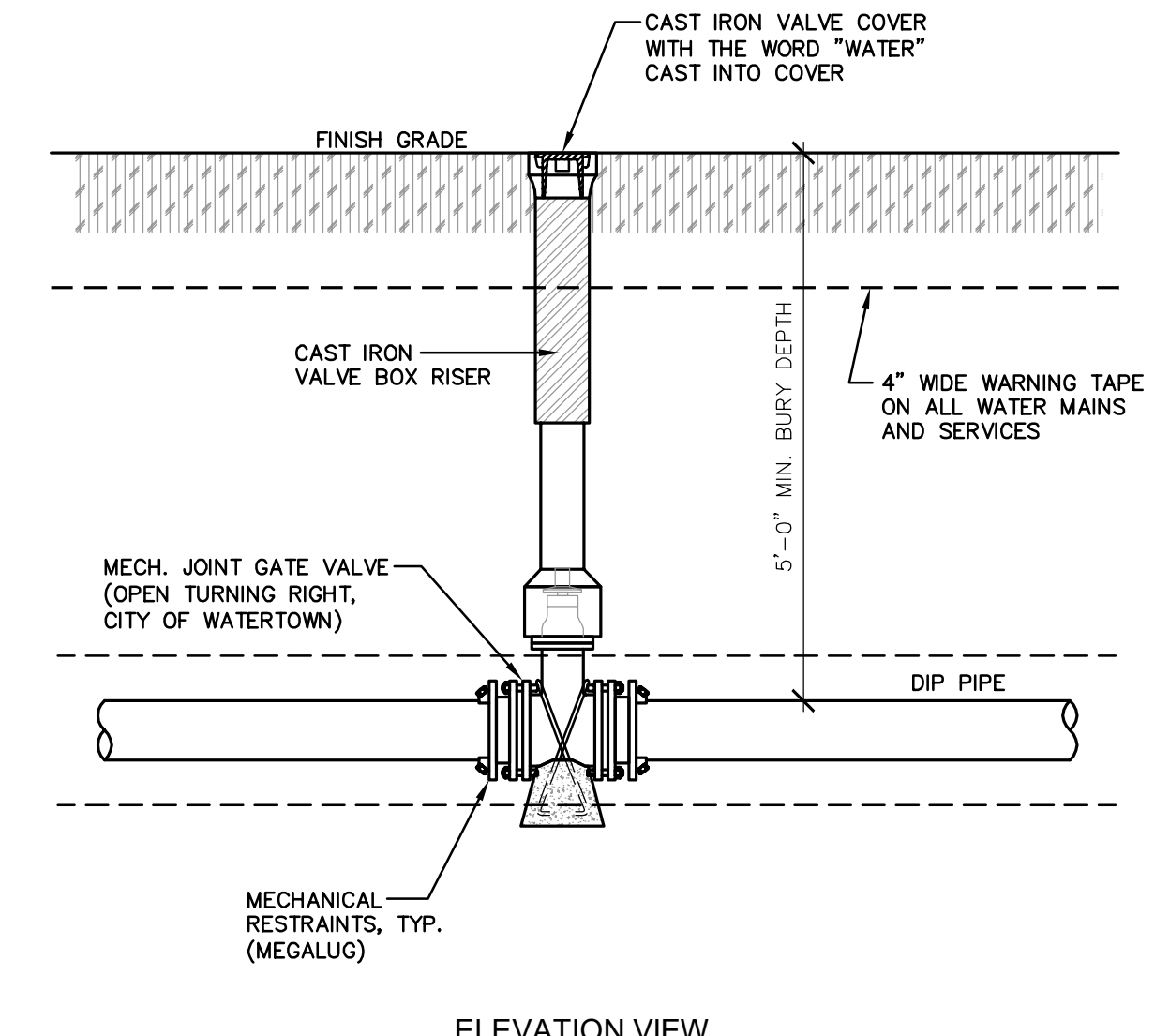
ANCHOR SCHEDULE

NOMINAL PIPE SIZE	150 P.S.I. HYDROSTATIC PRESSURE					
	11-1/4" BEND		22-1/2" BEND		45° BEND	
	CONCRETE VOLUME (CU. YDS.)	ANCHOR ROD (DIA.)	EMBED LENGTH	CONCRETE VOLUME (CU. YDS.)	ANCHOR ROD (DIA.)	EMBED LENGTH
4"	0.2	3/8"	0.4	3/8"	3/8"	1'-2"
6"	0.4	3/8"	0.7	3/8"	3/8"	1'-2"
8"	0.8	3/8"	1.2	3/8"	3/8"	1'-2"
10"	0.9	3/8"	1.7	3/8"	3/8"	1'-2"
12"	1.3	3/8"	2.5	1/2"	3/8"	1'-6"
14"	1.7	3/8"	3.3	1/2"	3/8"	1'-6"
16"	2.2	3/8"	4.3	1/2"	3/8"	1'-6"
18"	2.7	1/2"	5.4	5/8"	1/2"	2'-0"
20"	3.3	1/2"	6.6	5/8"	1/2"	2'-0"
24"	4.7	5/8"	9.4	3/4"	5/8"	2'-3"

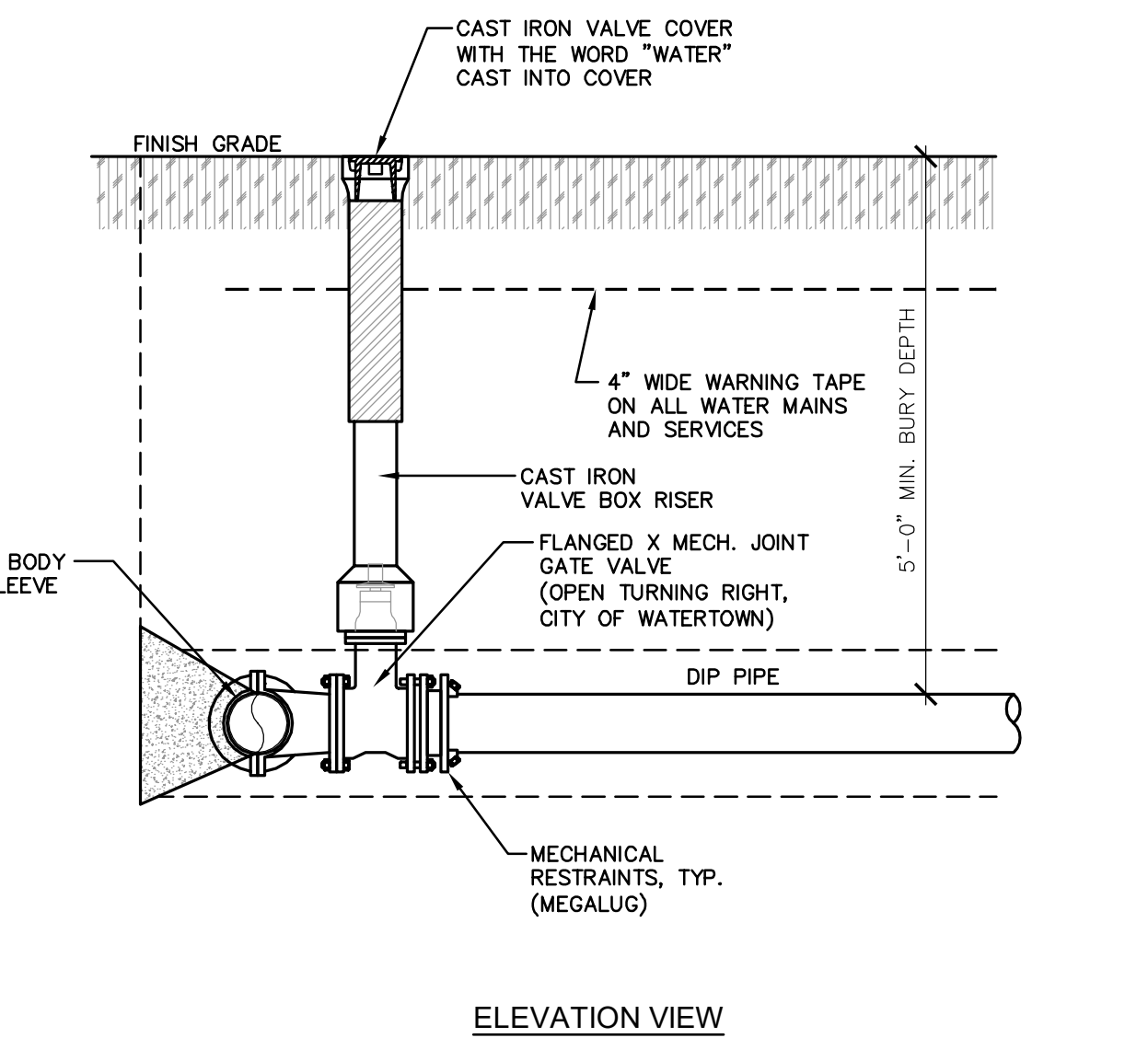
5 TYPICAL VERTICAL THRUST BLOCK DETAIL
NOT TO SCALE



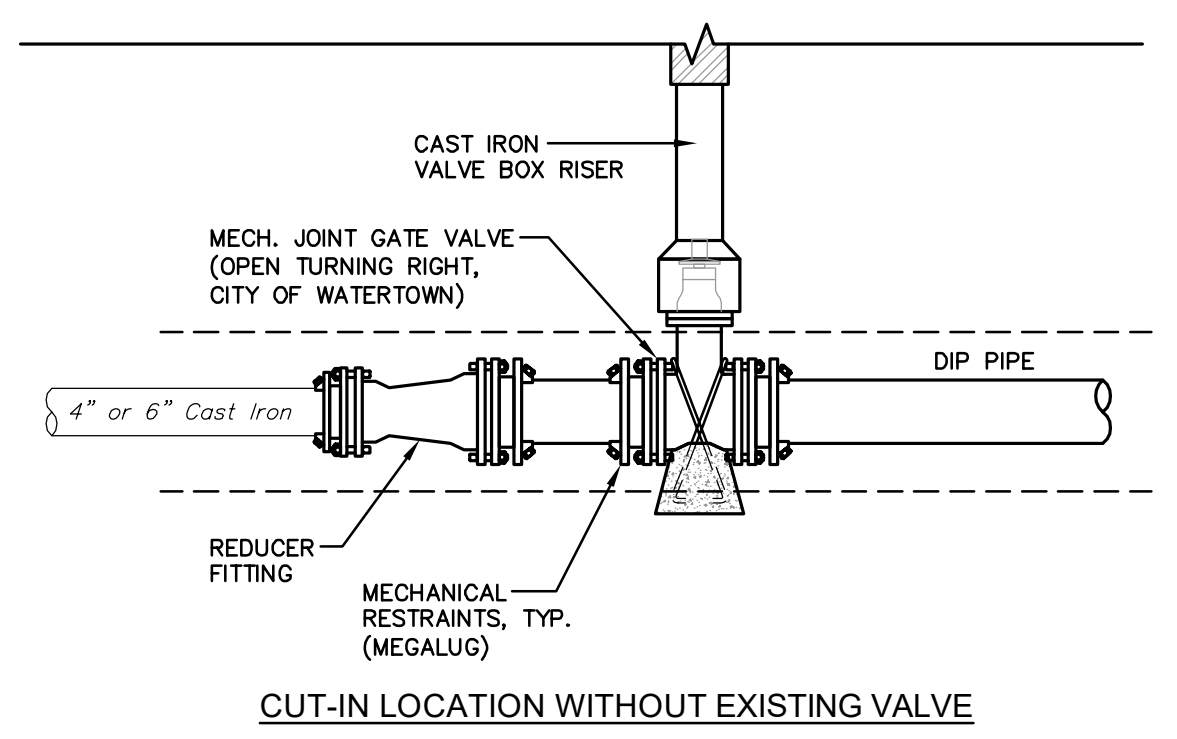
6 TYPICAL FIRE HYDRANT ASSEMBLY DETAIL
NOT TO SCALE



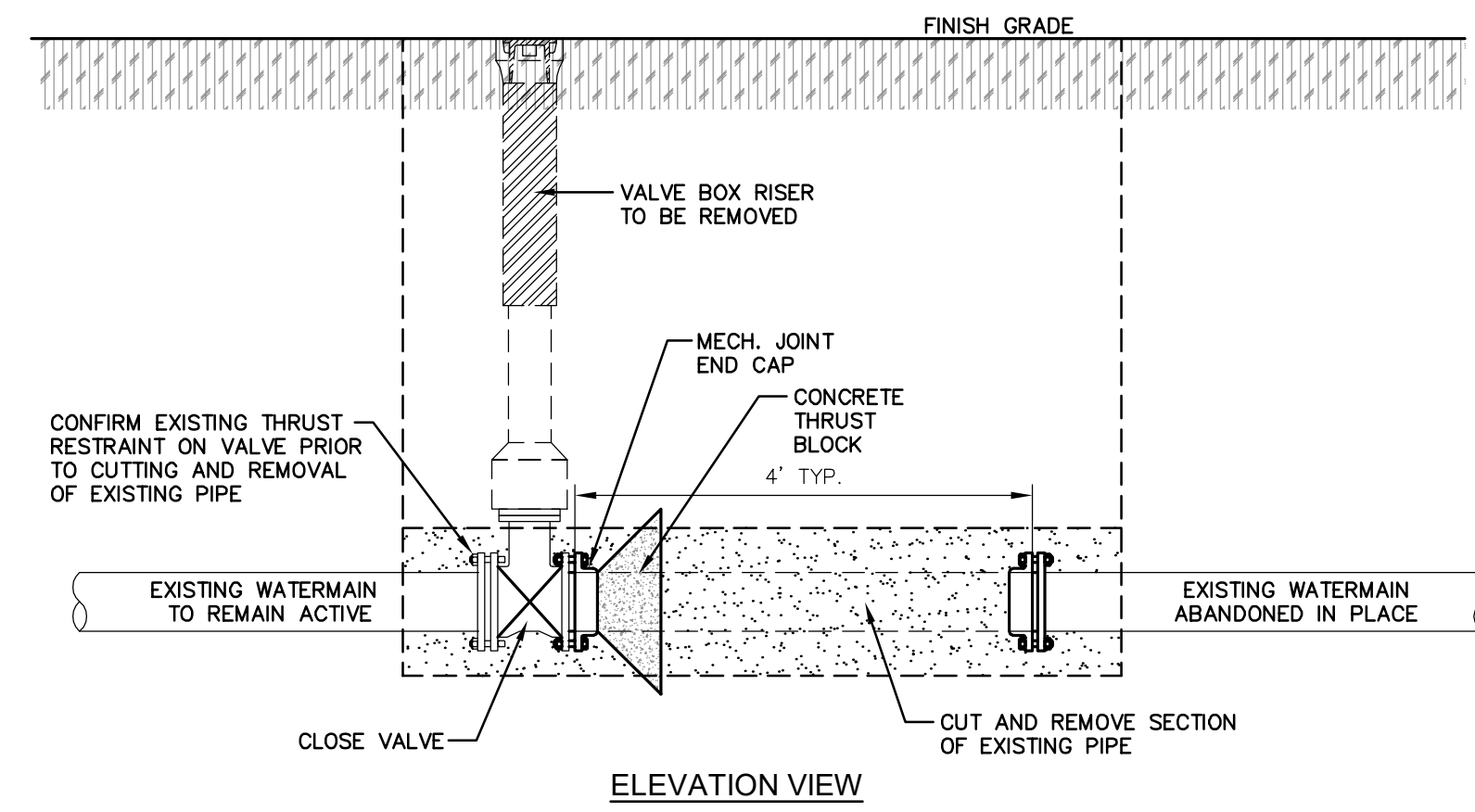
7 TYPICAL GATE VALVE DETAIL
NOT TO SCALE



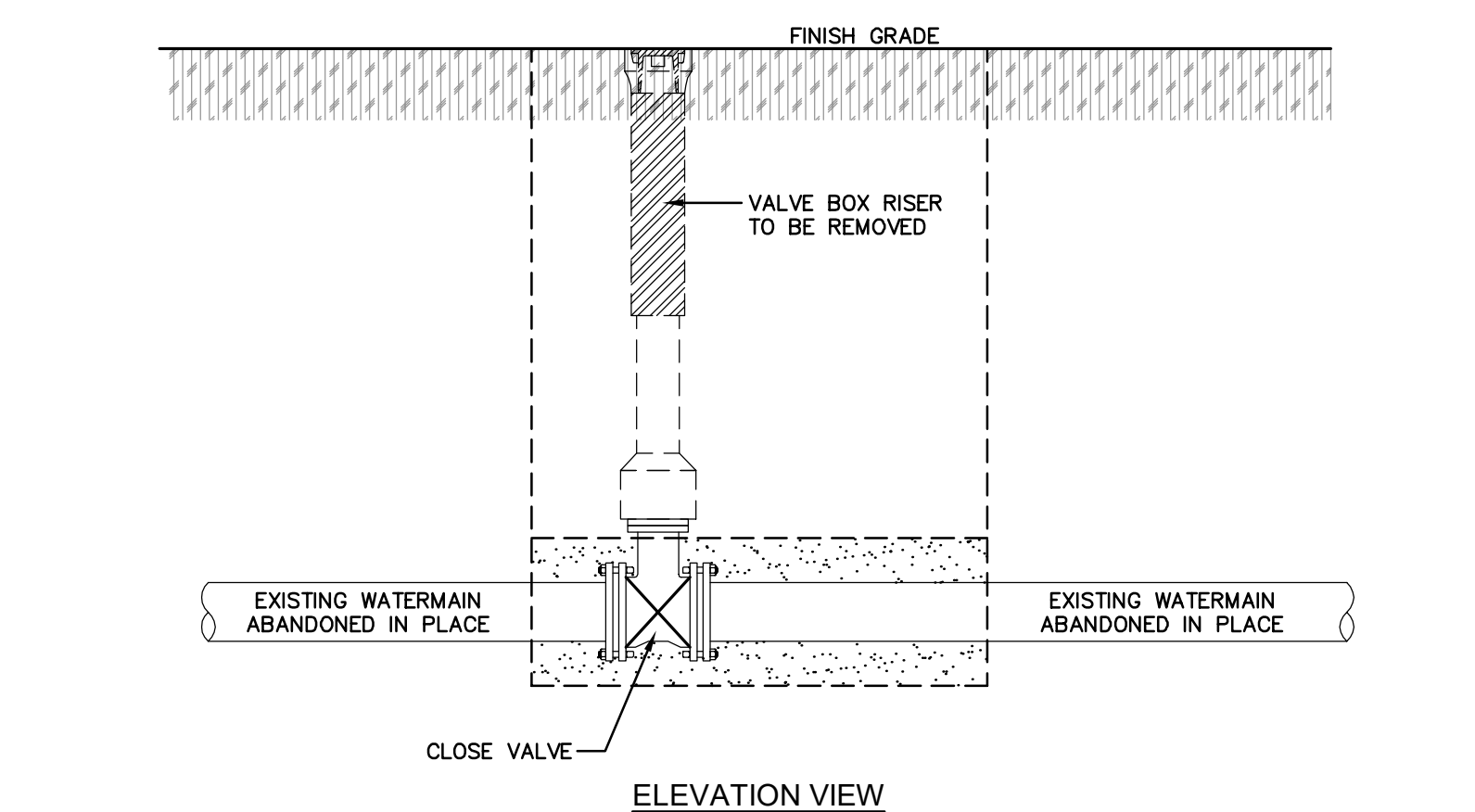
8 TYPICAL TAPPING SLEEVE AND VALVE DETAIL
NOT TO SCALE



9 TYPICAL CUT-IN VALVE DETAILS
NOT TO SCALE



10 CUT AND CAP OF EXISTING WATERMAIN FOR ABANDONMENT OF WATERMAIN DETAIL
NOT TO SCALE



11 CLOSE VALVE AND REMOVAL OF RISER FOR ABANDONMENT OF WATERMAIN DETAIL
NOT TO SCALE

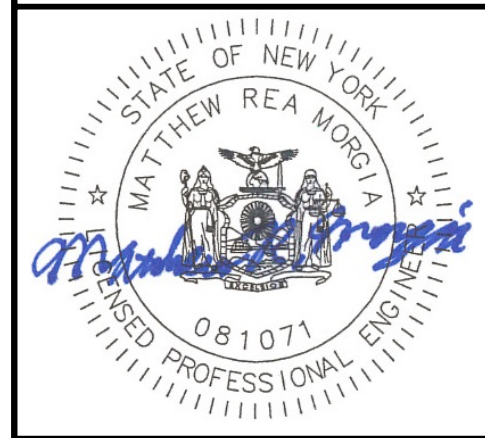


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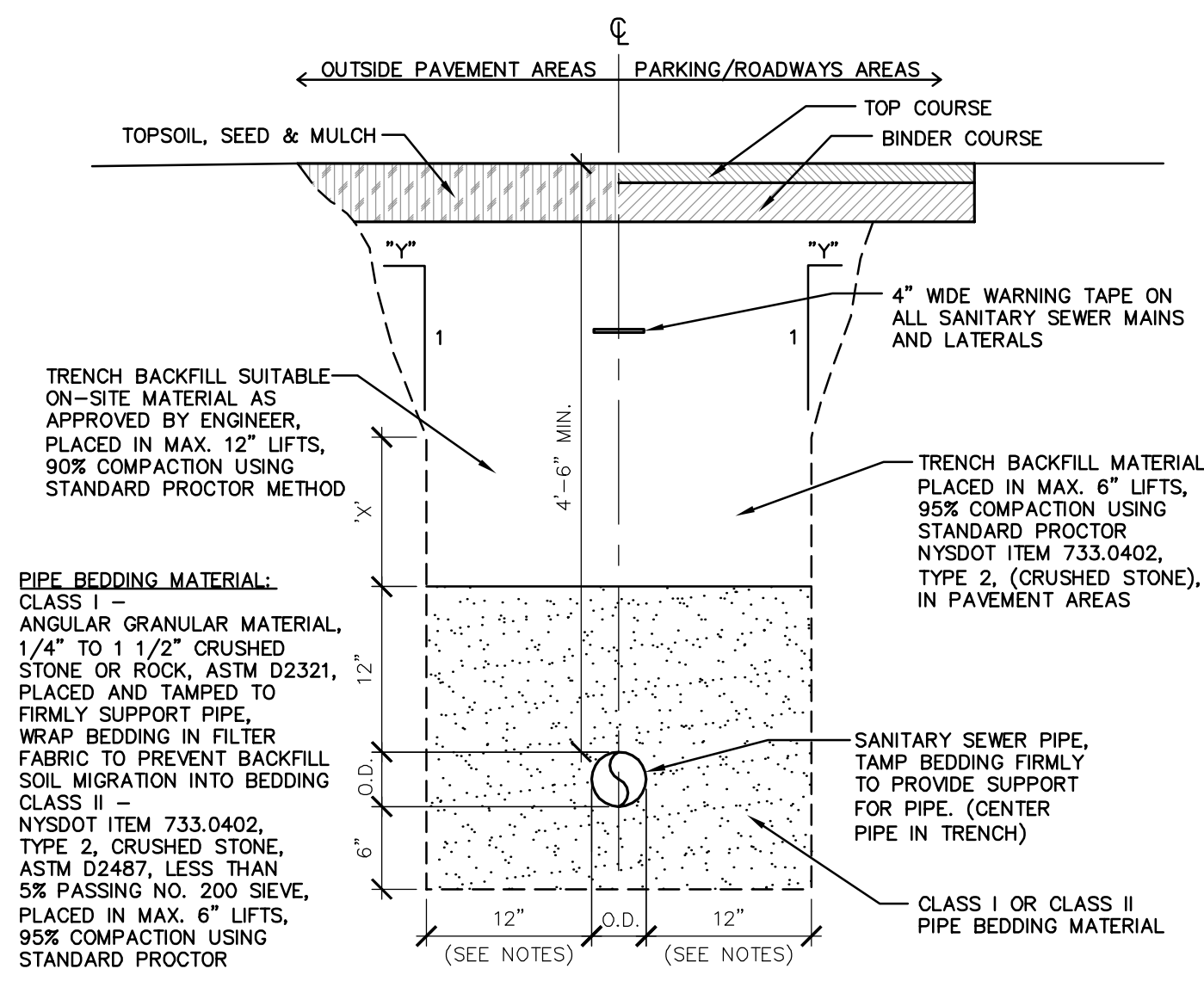


BURLINGTON STREET RECONSTRUCTION PROJECT
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
SCALE: AS NOTED
DRAWN BY: TFT
CHECKED BY: MRM
ISSUE DATES:
02/23/2024
04/03/2024
12/13/2024

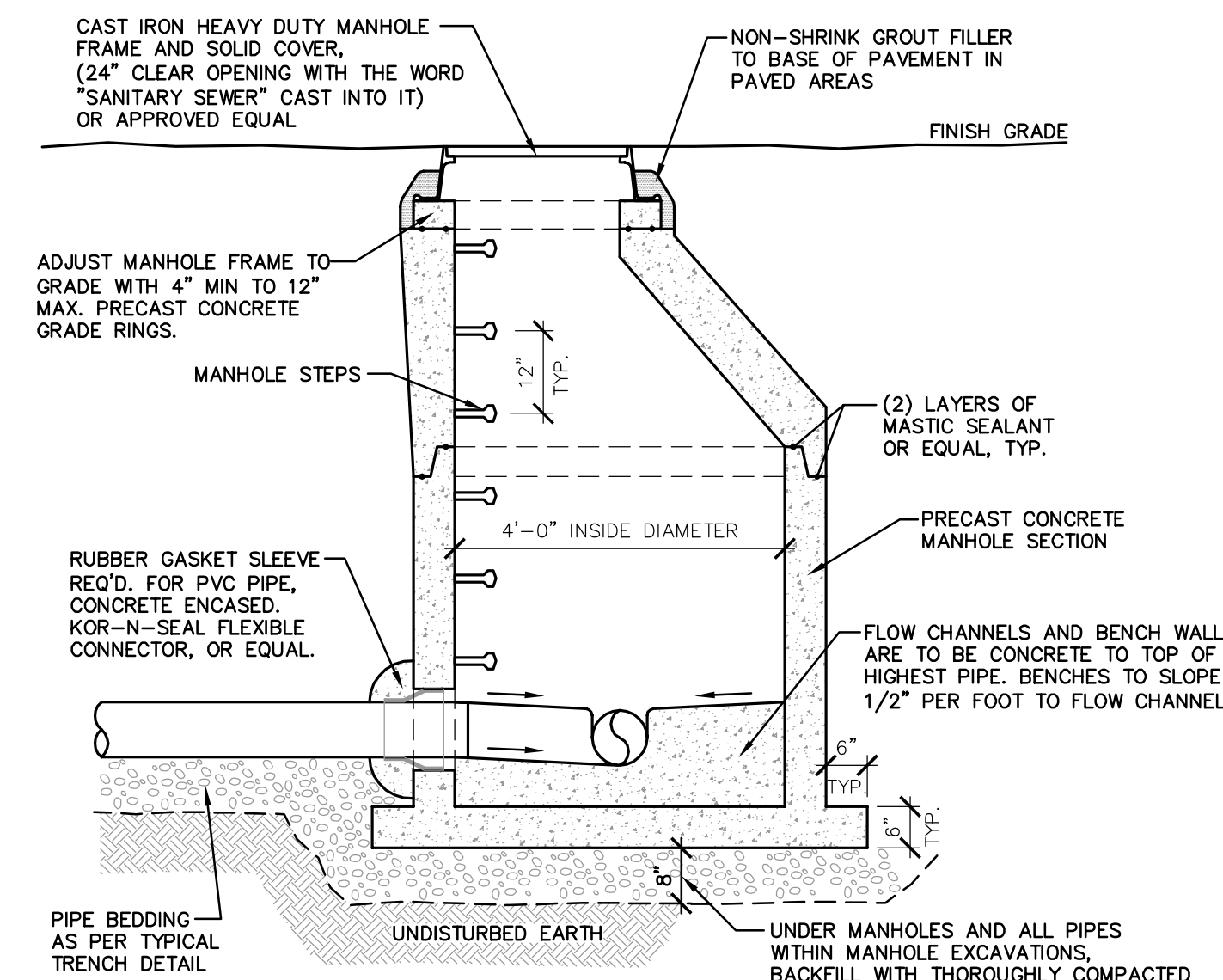
WATER DETAILS

CU-501



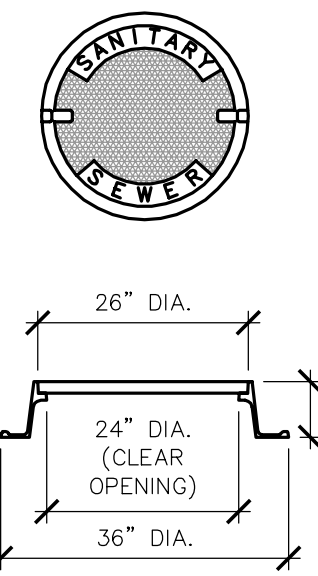
- PIPE BEDDING MATERIAL:**
CLASS I - ANGULAR GRANULAR MATERIAL, 1/4" TO 1 1/2" CRUSHED STONE OR ROCK, ASTM D2321, PLACED AND TAMPED TO FIRMLY SUPPORT PIPE, WRAP BEDDING IN FILTER FABRIC TO PREVENT BACKFILL SOIL MIGRATION INTO BEDDING CLASS II - NYSDOT ITEM 733.0402, TYPE 2, CRUSHED STONE, ASTM D2487, LESS THAN 5% PASSING NO. 200 SIEVE, PLACED IN MAX. 6" LIFTS, 95% COMPACTION USING STANDARD PROCTOR
- NOTES**
1. DIMENSIONS "X" AND "Y" SHOWN ABOVE SHALL BE DETERMINED BY CONTRACTOR TO COMPLY WITH O.S.H.A., NEW YORK STATE DEPARTMENT OF LABOR, NEW YORK STATE INDUSTRIAL CODE AND ALL OTHER APPLICABLE SAFETY STANDARDS.
2. SAFETY SHEETING OR TRENCH BOX MAY BE USED IN PLACE OF SLOPED TRENCH WALLS.
3. SHEETING, WHEN REQUIRED, TO BE CUT OFF AT LEAST 5 FEET BELOW STREET AND A MINIMUM OF 1 FOOT ABOVE TOP OF PIPE. WOOD SHEETING DRIVEN BELOW MID-DIAMETER OF THE PIPE SHALL BE LEFT IN PLACE. STEEL SHEETING DRIVEN BELOW MID-DIAMETER MAY BE WITHDRAWN IF APPROVED IN WRITING BY THE ENGINEER. FOR PVC PIPE ALL SHEETING DRIVEN BELOW MID-DIAMETER SHALL BE LEFT IN PLACE.
4. TRENCHES LOCATED WITHIN 5' OF ROAD SHOULDERS SHALL BE TREATED THE SAME AS UNDER PAVEMENT.
5. PIPE TO TRENCH WALL DISTANCE MAY BE REDUCED WHEN INSTALLED IN SAWCUT ROCK TRENCH.
6. PROVIDE WARNING TAPE AT ALL UNDERGROUND UTILITIES.

1 TYPICAL SANITARY SEWER TRENCH DETAIL
NOT TO SCALE

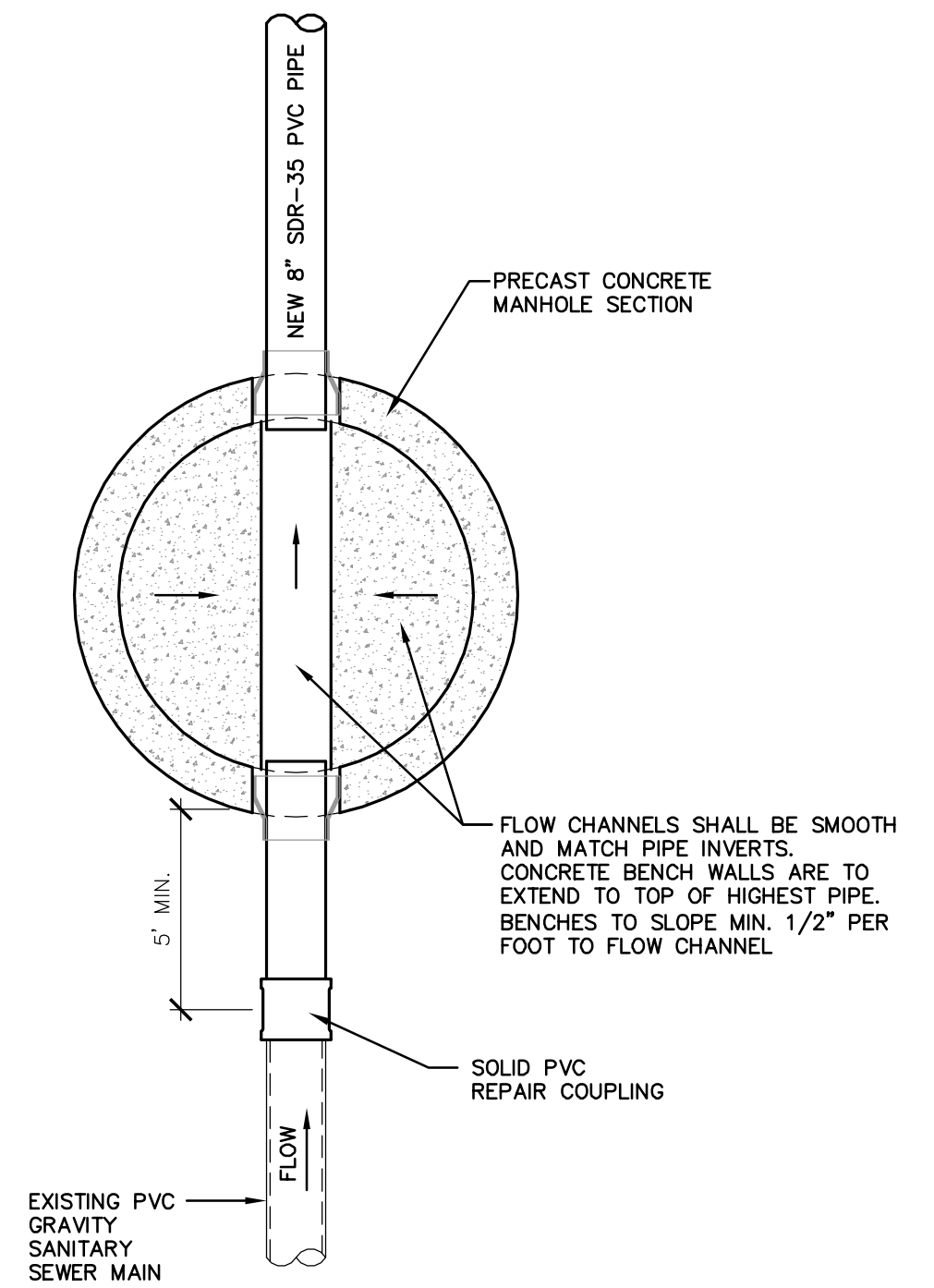


- TYPICAL PRECAST SANITARY SEWER MANHOLE NOTES:**
1. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 5,000 PSI AT 28 DAYS.
2. CONCRETE SHALL BE AIR ENTRAINED 5% - 6%.
3. WALLS, FLOOR, TOP SLAB AND ACCESS COVERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM C890 (HS-20 LOAD).
4. REINFORCEMENT - ASTM A497/A615 GRADE 60.
5. ALL JOINTS SHALL BE SEALED SUCH THAT THE TANK IS WATERTIGHT WITH BUTYL SEALANT CS-102 ASTM C-990.
6. 2 COATS OF BITUMASTIC WATERPROOF COATING TO BE APPLIED TO OUTER SURFACE OF PRECAST CONCRETE STRUCTURES.
7. ALL MANHOLE COVERS SHALL HAVE THE WORDS "SANITARY SEWER" IN 2" LETTERS, CAST IN COVERS.
8. ALL MANHOLES SHALL BE 4'-0" I.D. UNLESS OTHERWISE NOTED.

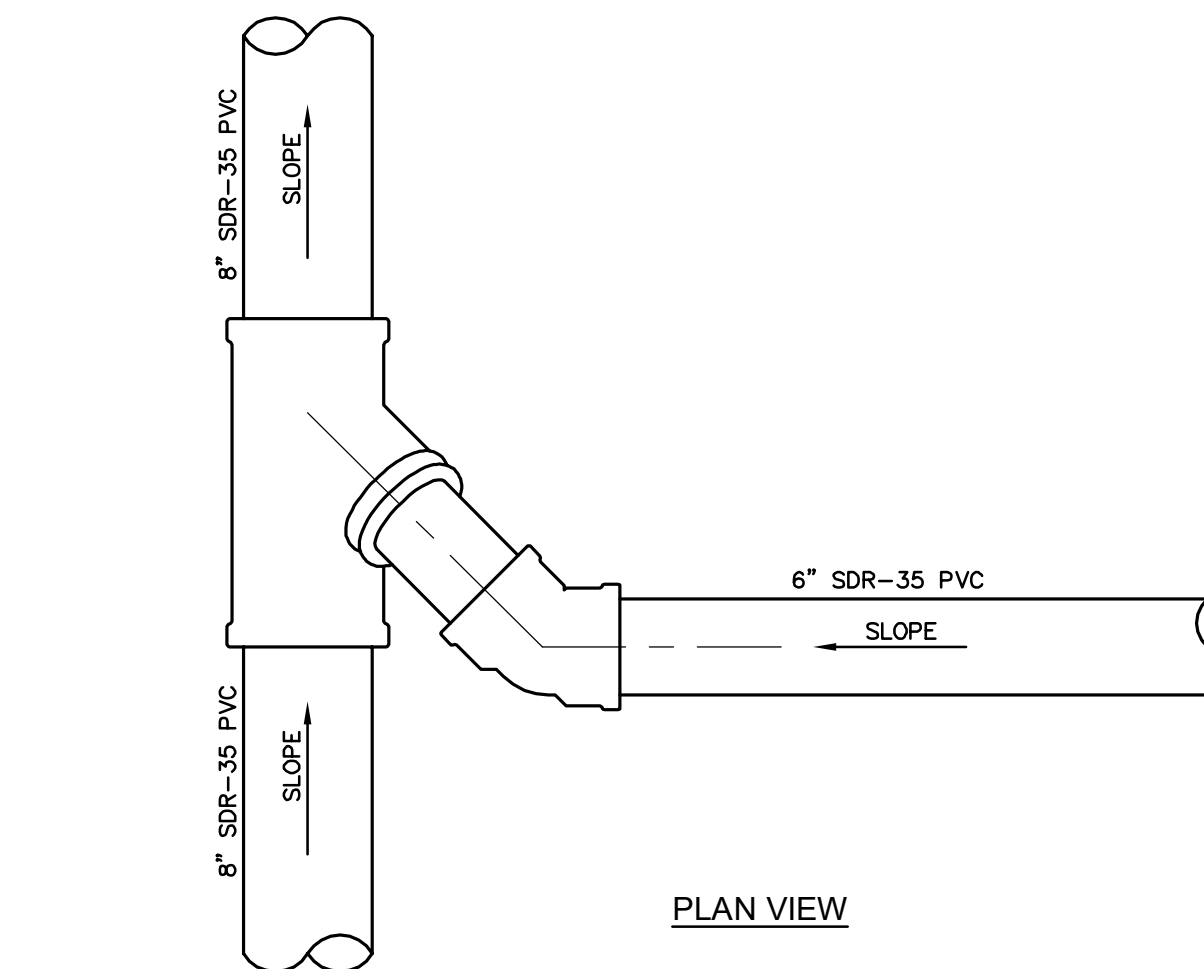
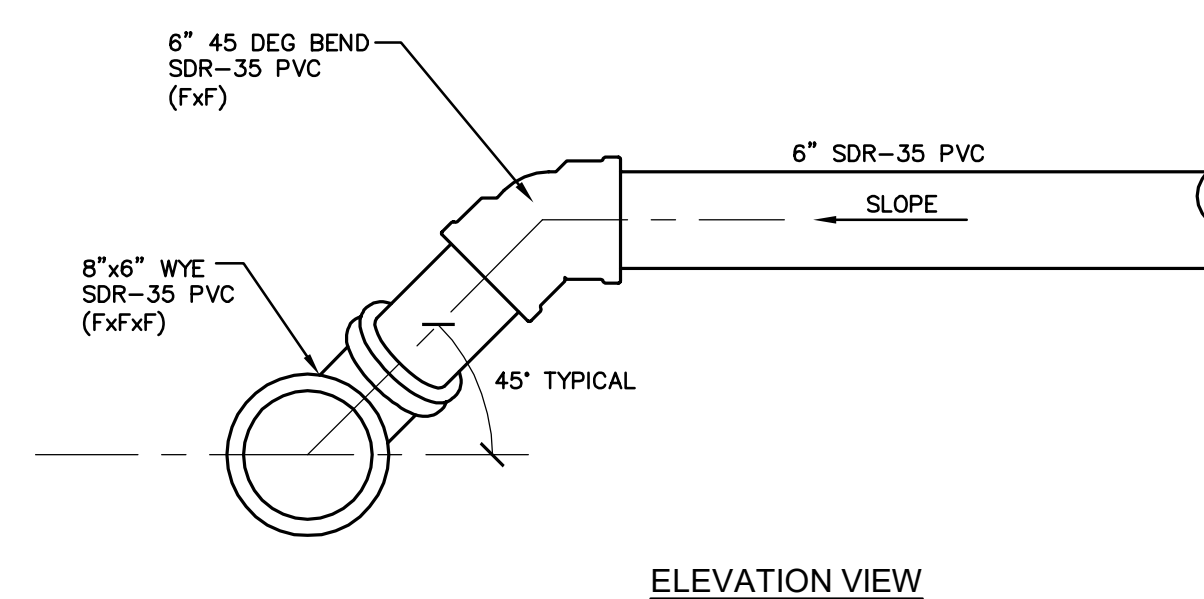
2 TYPICAL SANITARY SEWER MANHOLE DETAIL
NOT TO SCALE



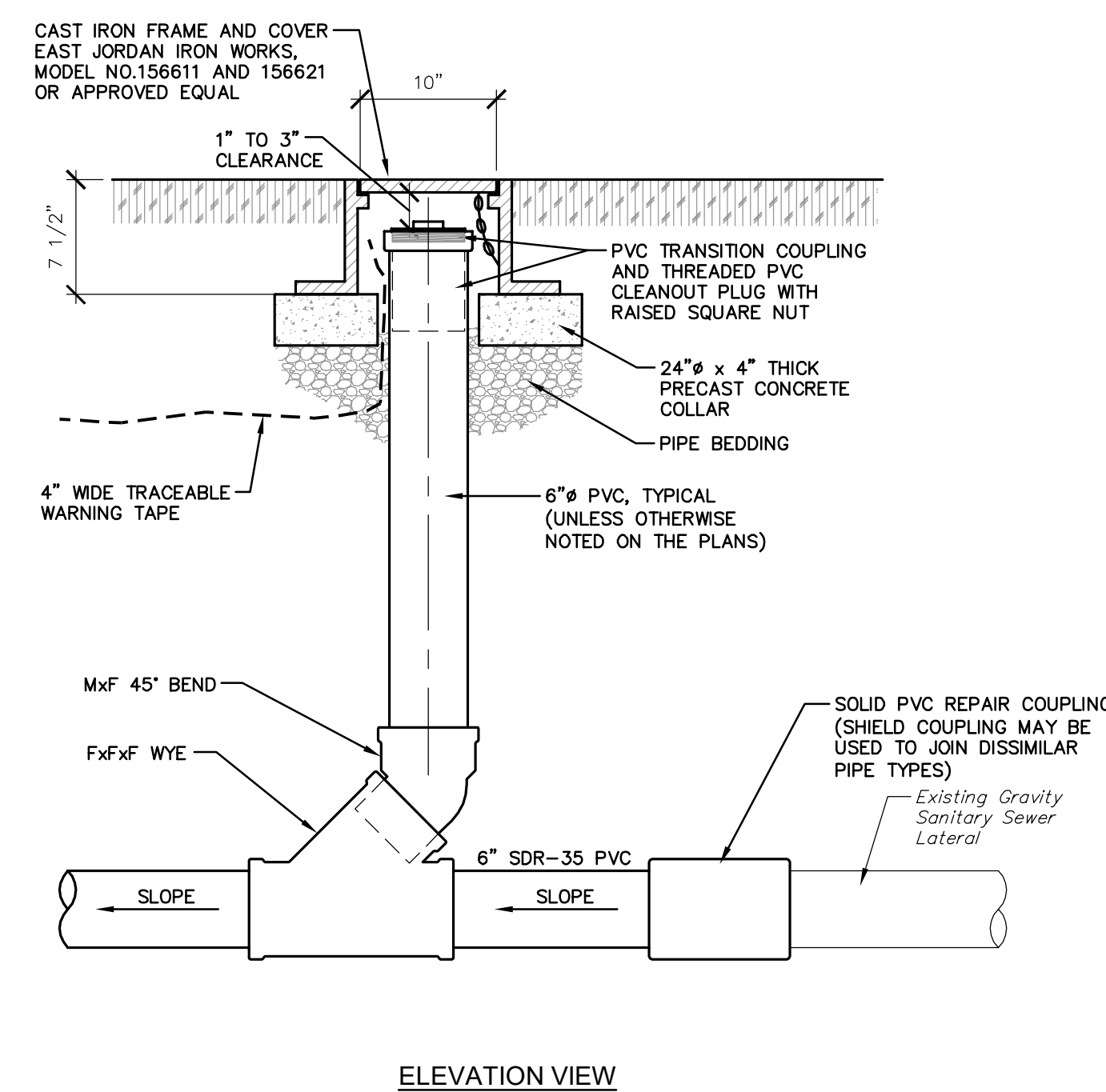
- 3 TYPICAL SANITARY SEWER FRAME AND COVER DETAIL**
NOT TO SCALE
- CAST IRON HEAVY DUTY MANHOLE FRAME AND SOLID COVER REFER TO PLANS FOR LOCATIONS, SOLID COVER - (THE WORD "SANITARY SEWER" CAST INTO IT) EAST JORDAN MODEL 104033 OR APPROVED EQUAL FRAME - 7" HIGH AND 2'-0" CLEAR OPENING EAST JORDAN MODEL 104511 OR APPROVED EQUAL



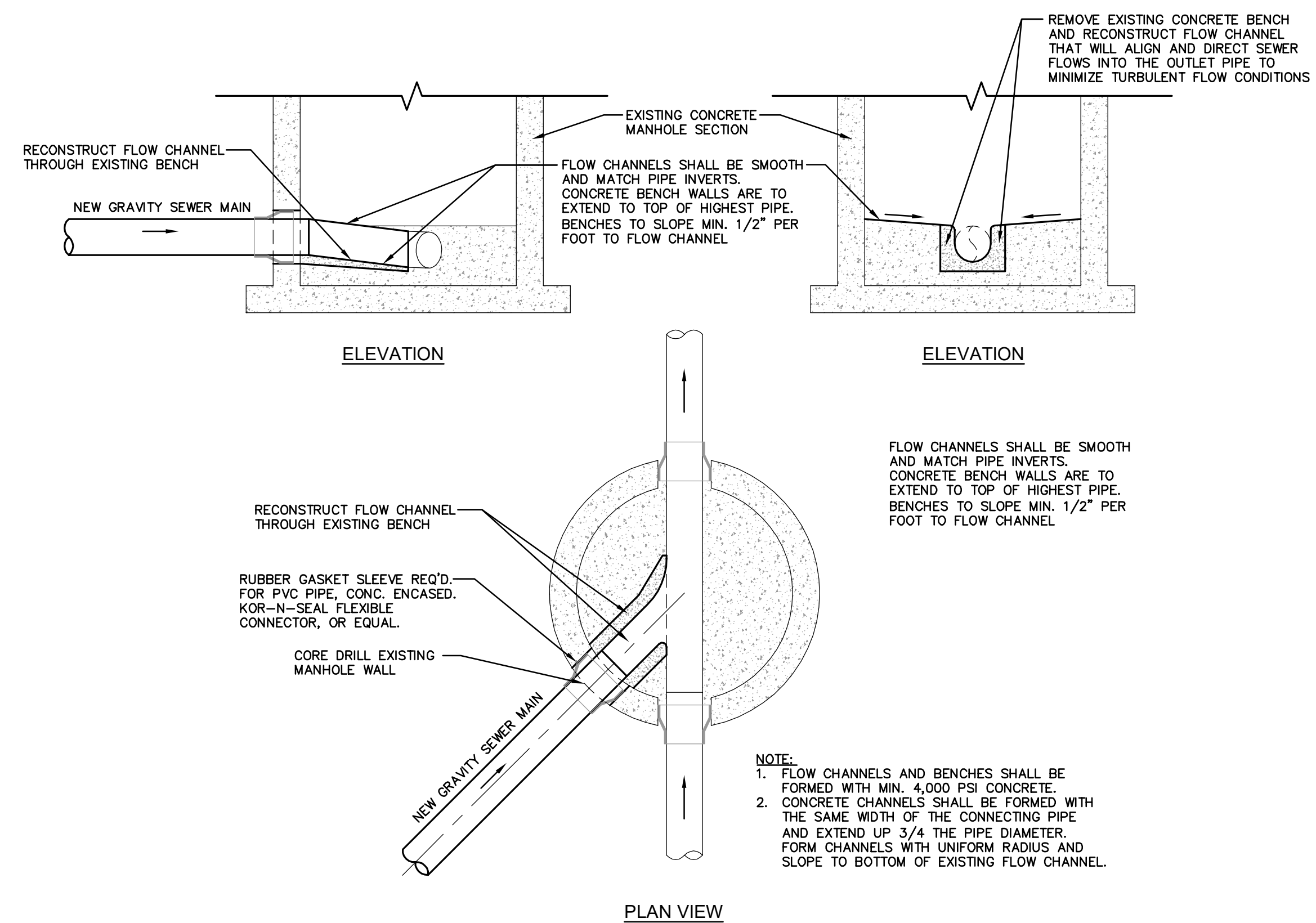
6 SANITARY SEWER CUT-IN MANHOLE DETAIL
NOT TO SCALE (FARWELL STREET)



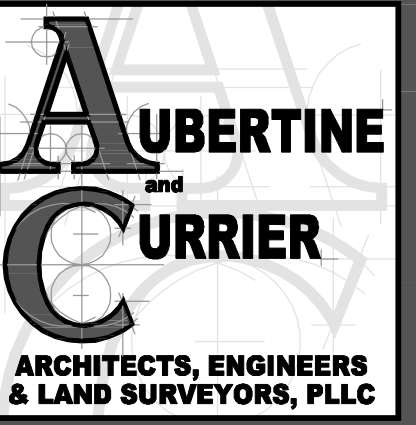
4 TYPICAL LATERAL CONNECTION AT SEWER MAIN DETAIL
NOT TO SCALE



5 TYPICAL CLEANOUT DETAIL
NOT TO SCALE



7 GRAVITY SANITARY SEWER PIPE CONNECTION AT EXISTING SANITARY MANHOLE DETAIL
NOT TO SCALE (ST. MARY STREET)



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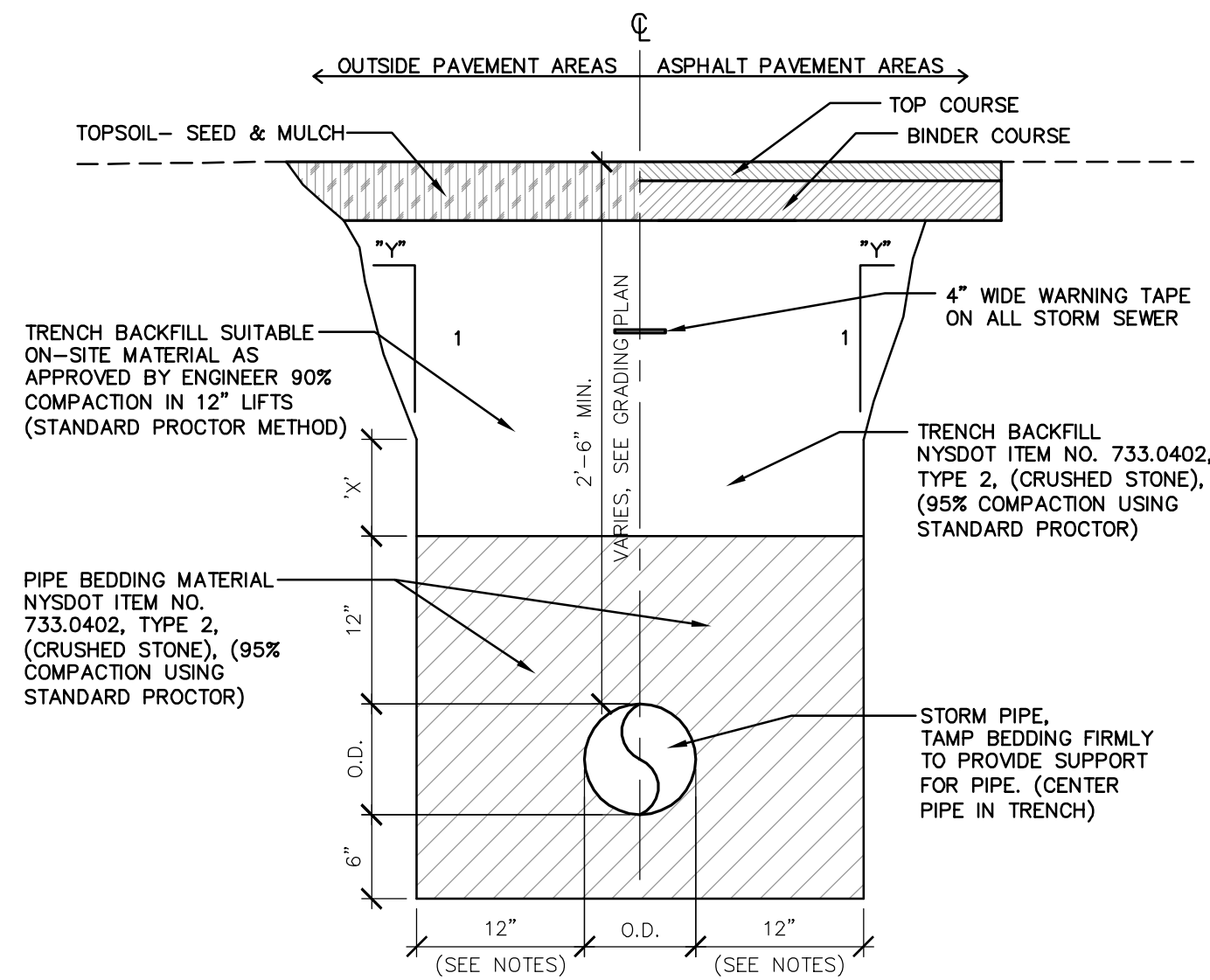


BURLINGTON STREET RECONSTRUCTION PROJECT
CITY OF WATERTOWN
ENGINEERING DEPARTMENT
JEFFERSON COUNTY, STATE OF NEW YORK

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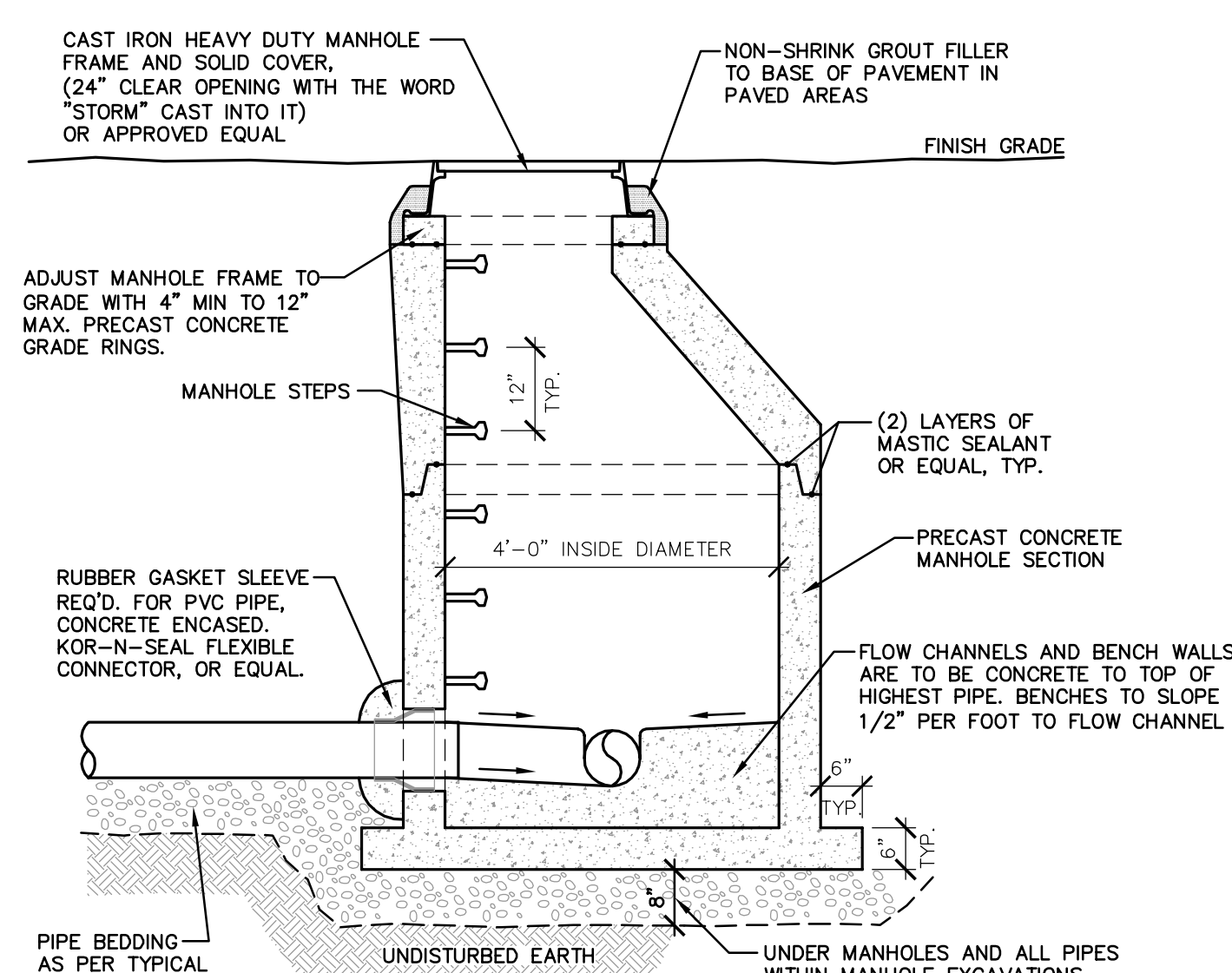
SANITARY SEWER DETAILS

CU-502



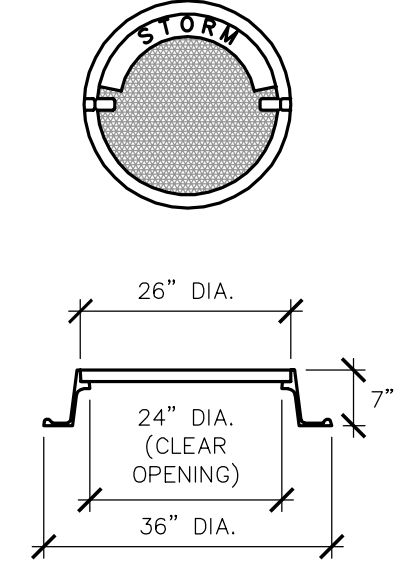
- NOTES:**
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 - TRENCHES LOCATED WITHIN 5' OF ROAD SHOULDERS SHALL BE TREATED THE SAME AS UNDER PAVEMENT.
 - PIPE TO TRENCH WALL DISTANCE MAY BE REDUCED WHEN INSTALLED IN SAWCUT ROCK TRENCH.

1 TYPICAL STORM SEWER TRENCH DETAIL
NOT TO SCALE



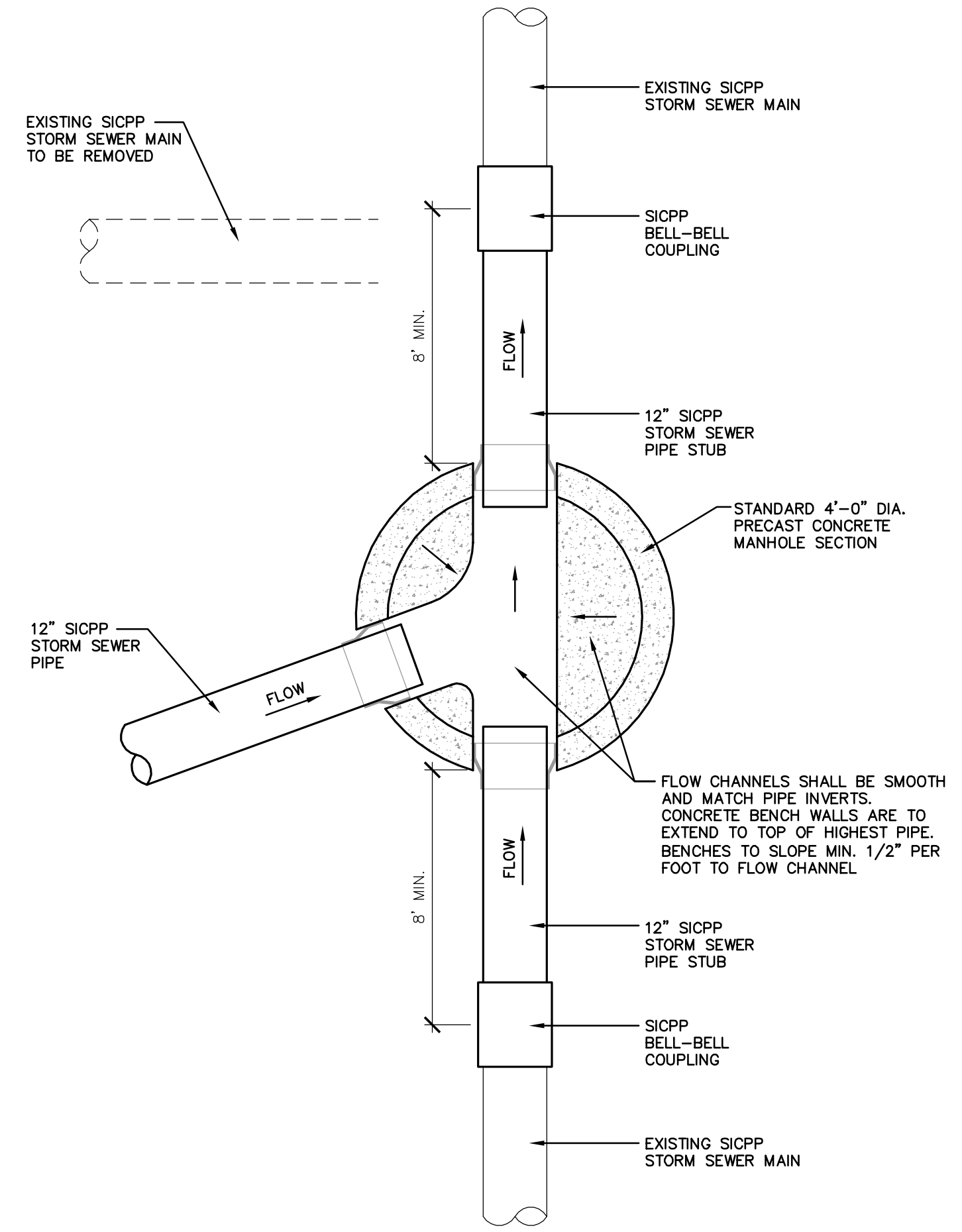
- TYPICAL PRECAST STORM SEWER MANHOLE NOTES:**
- CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 5,000 PSI AT 28 DAYS.
 - CONCRETE SHALL BE AIR ENTRAINED 5%-8%.
 - WALLS, FLOOR, TOP SLAB AND ACCESS COVERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM C890 (HS-20 LOAD).
 - REINFORCEMENT - ASTM A497/A615 GRADE 60.
 - ALL JOINTS SHALL BE SEALED SUCH THAT THE TANK IS WATERTIGHT WITH BUTYL SEALANT CS-102 ASTM C-990.
 - 2 COATS OF BITUMASTIC WATERPROOF COATING TO BE APPLIED TO OUTER SURFACE OF PRECAST CONCRETE STRUCTURES.
 - ALL MANHOLE COVERS SHALL HAVE THE WORDS "STORM" IN 2" LETTERS, CAST IN COVERS.
 - ALL MANHOLES SHALL BE 4'-0" I.D. UNLESS OTHERWISE NOTED.

2 TYPICAL STORM SEWER MANHOLE DETAIL
NOT TO SCALE

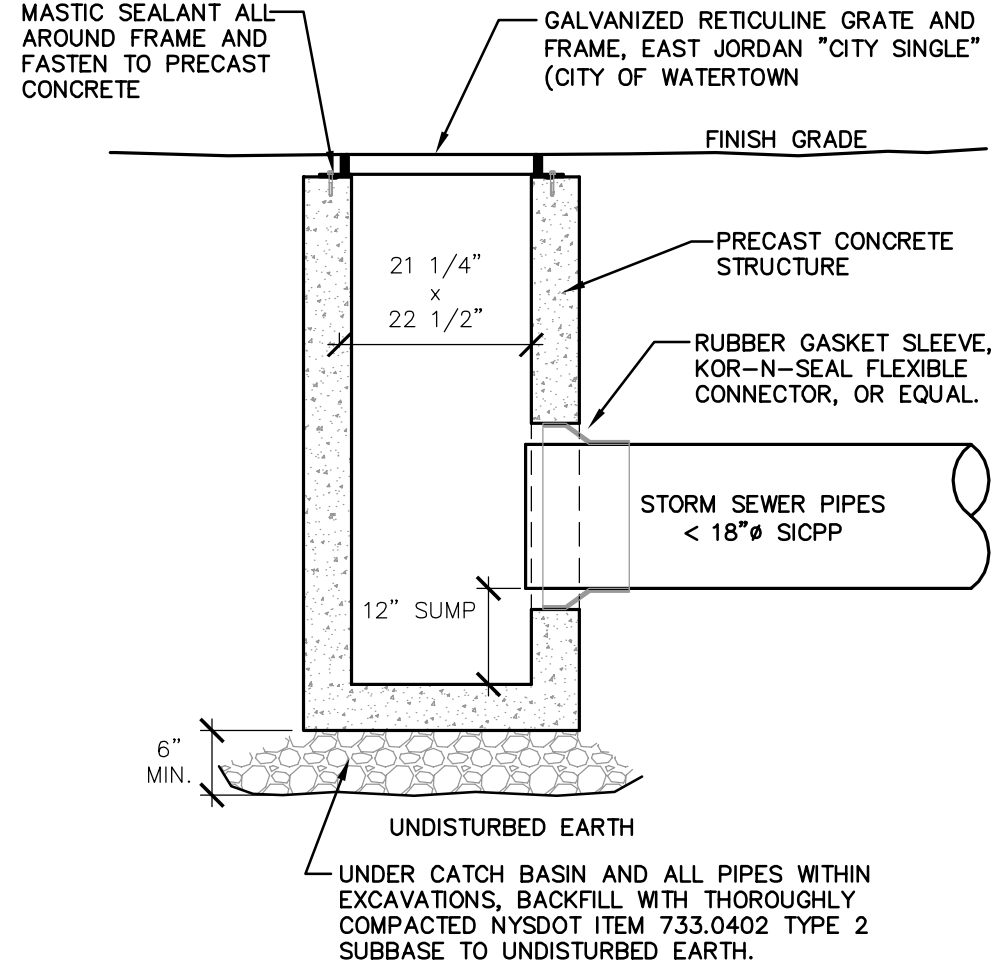


CAST IRON HEAVY DUTY MANHOLE FRAME AND SOLID COVER REFER TO PLANS FOR LOCATIONS, SOLID COVER (THE WORD "STORM" CAST INTO IT) EAST JORDAN MODEL 103525 OR APPROVED EQUAL FRAME: 7" HIGH AND 2'-0" CLEAR OPENING EAST JORDAN MODEL 104511 OR APPROVED EQUAL

3 TYPICAL STORM SEWER FRAME AND COVER DETAIL
NOT TO SCALE

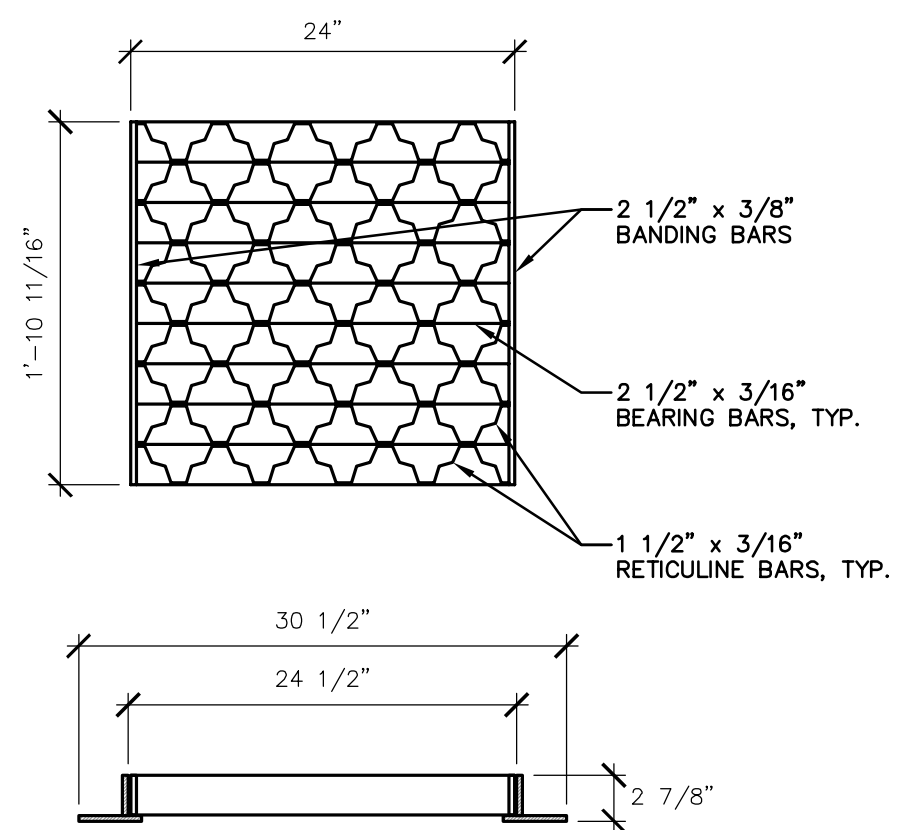


6 STORM SEWER CUT-IN MANHOLE DETAIL
NOT TO SCALE (ST. MARY STREET)

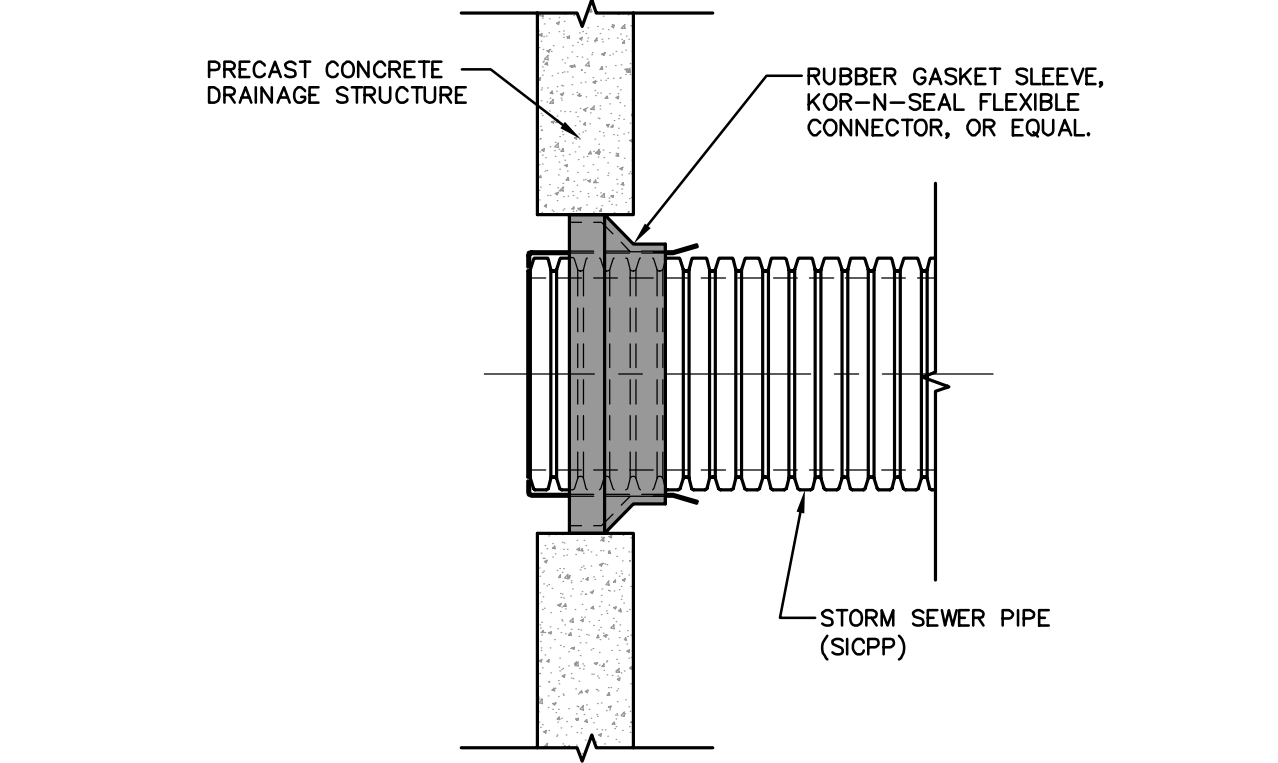


- TYPICAL PRECAST CATCH BASIN NOTES:**
- CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 5,000 PSI AT 28 DAYS.
 - CONCRETE SHALL BE AIR ENTRAINED 5%-8%.
 - WALLS, FLOOR, TOP SLAB AND ACCESS COVERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM C890 AND NYSDOT STANDARD SHEETS 604-5 TO 604-8 (HS-20 LOAD).
 - REINFORCEMENT - ASTM A497/A615 GRADE 60.
 - ALL JOINTS SHALL BE SEALED SUCH THAT THE TANK IS WATERTIGHT WITH BUTYL SEALANT CS-102 ASTM C-990.

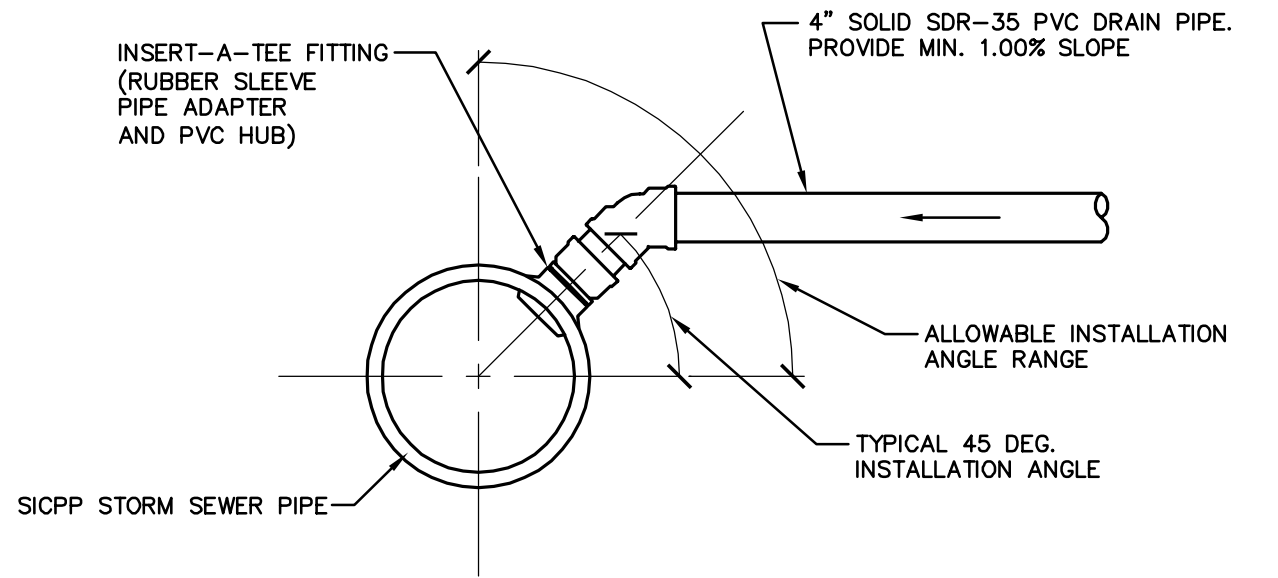
4 TYPICAL CITY SINGLE CATCH BASIN DETAIL
NOT TO SCALE



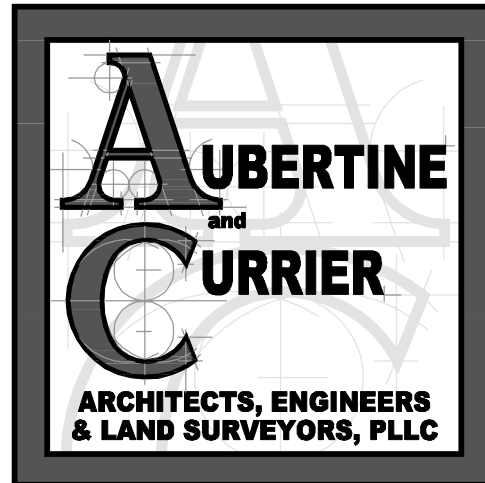
5 TYPICAL RETICULATE STORM SEWER "CITY SINGLE" FRAME AND GRATE DETAIL
NOT TO SCALE



7 TYPICAL STORM SEWER CONNECTION DETAIL
NOT TO SCALE



8 TYPICAL INSERT-A-TEE CONNECTION DETAIL
NOT TO SCALE

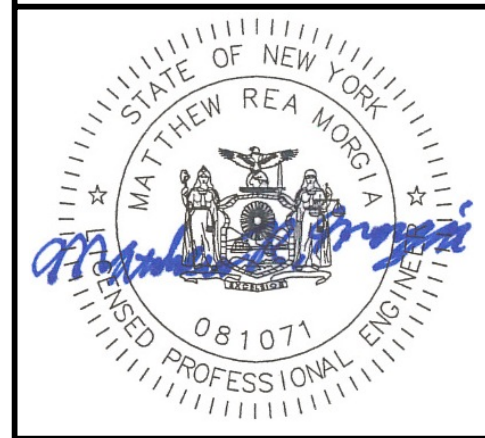


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CITY OF WATERTOWN
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JEFFERSON COUNTY, STATE OF NEW YORK

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ISSUE DATES:	02/23/2024 04/03/2024 12/13/2024

STORM SEWER DETAILS

CU-503

USC SEPTEMBER 01, 2022

FILE NAME = 619-010.dwg
DATE/TIME = 13-APR-2022 09:36
USER = oboastwright

GENERAL NOTES

- 1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
2. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
4. STANDARD SHEET 619-503 MAY BE USED FOR AN OFFSITE DETOUR SETUP FOR BOTH LONG TERM AND SHORT TERM WORK DURATIONS.
5. FOLLOW REGIONAL HIGH-VOLUME RESTRICTIONS. CONSULT WITH DOT ENGINEER IF EXCEPTION NEEDED.
6. PLAN AHEAD TO AVOID CONFLICTING WORK ZONES. CHECK FOR CONSTRUCTION PROJECTS, CLOSURES, & RESTRICTIONS AT WWW.511NY.ORG, WWW.DOT.NY.GOV/PROJECTS, AND WITH DOT ENGINEER.
7. DOCUMENT AND REPORT WORK ZONE INCIDENTS USING EITHER THE DEPARTMENT'S WORK ZONE INCIDENT FORM, OR THE CONSTRUCTION INCIDENT REPORTING SYSTEM, AS APPROPRIATE.
8. CONSIDER CLOSURE WIDTH & CLEAR WIDTH FOR WIDE VEHICLES ON WIDE LOAD ROUTES.

ACTIVITY AREA

- 1. A 500' MINIMUM LONGITUDINAL DISTANCE SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

- 1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
2. FOR LONG TERM WORK DURATIONS, ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET OR OMITTED WITH THE APPROVAL OF THE DOT ENGINEER. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
5. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
6. NYR9-12 SHALL BE USED IN PLACE OF NYR9-11 WHEN A REDUCED REGULATORY SPEED LIMIT SIGN IS AUTHORIZED.
7. RIGID AND FLEXIBLE "ROLL-UP" SIGNS MAY BE USED FOR MOBILE, SHORT DURATION AND SHORT-TERM STATIONARY WORK. RIGID SIGNS MUST BE MOUNTED AT LEAST 5 FEET ABOVE GRADE (7 FEET WHERE THERE ARE PEDESTRIANS OR PARKED CARS). FLEXIBLE SIGNS SHALL BE MOUNTED AT LEAST ONE FOOT ABOVE GRADE. MESH SIGNS SHALL NOT BE USED. USE RETRO REFLECTORIZED RIGID SIGNS FOR NIGHTTIME WORK.

CHANNELIZING DEVICES

- 1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

- 1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

- 1. LANE CLOSURES SHALL BE LOCATED TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.
3. ARROW PANELS SHALL BE LEGIBLE CONTINUOUSLY FROM ANY POINT WITHIN THE ROADWAY (INCLUSIVE OF SHOULDERS) FROM 1,500 FEET IN ADVANCE OF THE LANE CLOSURE TAPER TO THE BEGINNING OF THE LANE CLOSURE TAPER.

LANE WIDTHS

- 1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.
2. A WRITTEN NOTE SHALL BE PROVIDED TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.

PROTECTIVE VEHICLES

- 1. A PROTECTIVE VEHICLE IS A LARGE DUMP TRUCK, A LARGE RACK TRUCK OR OTHER VEHICLE HAVING A GROSS WEIGHT OF AT LEAST 24,000 POUNDS. IF THE PROTECTIVE VEHICLE ENCLOSES INTO THE TRAVEL LANE, OR IF IT REMAINS ENTIRELY ON THE SHOULDER OF ANY HIGH SPEED ROAD (45 MPH OR HIGHER), IT SHALL BE EQUIPPED WITH A DEPLOYED TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA), SEE TABLE 011-01 ON SHEET 619-11). PROTECTIVE VEHICLES MAY BE LOADED WITH SAND, GRAVEL, OR FINE AGGREGATE AS BALLAST TO ENHANCE THE VEHICLE'S GROSS WEIGHT. ANY BALLAST ADDED TO ENHANCE THE VEHICLE'S GROSS WEIGHT SHALL BE SECURED AS NOT TO BECOME DISLODGED IF IMPACTED.
2. A PROTECTIVE VEHICLE USED IN A MOVING OPERATION IS REFERRED TO AS A SHADOW VEHICLE.
3. A PROTECTIVE VEHICLE USED IN A STATIONARY OPERATION IS REFERRED TO AS A BARRIER VEHICLE.
4. IN A MOVING OPERATION OR A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR, THE OPERATOR SHALL REMAIN IN THE PROTECTIVE VEHICLE WITH THE SAFETY BELT AND HEADREST PROPERLY ADJUSTED, MAINTAIN VEHICLE SPACING, AND KEEP THE WHEELS ALIGNED WITH THE LANE STRIPING. TWO-WAY RADIOS SHOULD BE USED TO COMMUNICATE BETWEEN THE OPERATOR AND THE WORK CREW.
5. IN A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR, THE PROTECTIVE VEHICLE HAS BEEN APPROPRIATELY PLACED, IT SHOULD BE UNOCCUPIED. UNOCCUPIED VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK.
6. WHEN A PROTECTIVE VEHICLE IS USED IN ADVANCE OF EITHER MOVING OR STATIONARY OPERATIONS TO DISPLAY SIGN MESSAGES, IT IS REFERRED TO AS AN ADVANCE WARNING VEHICLE. ADVANCED WARNING VEHICLES MAY BE OCCUPIED OR UNOCCUPIED. WHEN SIGNS ARE MOUNTED ON AN ADVANCED WARNING VEHICLE, THEY SHALL NOT OBSTRUCT VISIBILITY OF ANY LIGHTS (TAILLIGHTS OR WARNING LIGHTS) OR SIDE-VIEW MIRRORS ON THE VEHICLE.
7. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE PROTECTIVE VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
8. PROTECTIVE VEHICLES MAY BE REQUIRED IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL, FOR CAPITOL CONSTRUCTION PROJECTS.
9. DIRECT VERBAL COMMUNICATION BETWEEN THE PROTECTIVE VEHICLES AND THE WORK VEHICLE(S) / EQUIPMENT SHALL BE UTILIZED WHERE AVAILABLE.

WORK DURATION DEFINITIONS

- 1. THERE ARE MAINLY FIVE WORK DURATIONS:
A. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
B. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
C. SHORT-TERM IS STATIONARY DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
D. SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N10 NOTES ON NIGHTTIME WORK.
E. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES. IT IS USED FOR VEHICLE BASED OPERATIONS AND DOES NOT INVOLVE WORKERS ON FOOT. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N10 NOTES ON NIGHTTIME WORK.
2. SPECIAL OPERATIONS ARE WORK OPERATIONS THAT DO NOT FIT INTO ONE OF THE ABOVE FIVE CATEGORIES. SPECIAL OPERATIONS INCLUDE:
A. STOP AND GO OPERATIONS - WORK THAT COMPLETES WITHIN 5 MINUTES AND ALLOWS WORKERS ON FOOT.
B. OTHER OPERATIONS INCLUDING MOWING, MULCHING/HERBICIDE OPERATIONS, TEMPORARY ROAD/INTERSECTION CLOSURES, ETC.

ROADWAY TYPE DEFINITIONS

- 1. FREEWAY:
A. INTERSTATE: INTERREGIONAL HIGH-SPEED, HIGH-VOLUME, DIVIDED FACILITIES WITH COMPLETE CONTROL OF ACCESS.
B. PARKWAY: DIVIDED HIGHWAYS FOR NON-COMMERCIAL TRAFFIC WITH FULL CONTROL OF ACCESS, GRADE PARKWAY SEPARATIONS, INTERCHANGES, AND OCCASIONAL AT-GRADE INTERSECTIONS. PARKWAYS ARE DESIGNATED BY LAW.

ROADWAY TYPE DEFINITIONS (CONTINUED)

- 2. EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS. ALL FREEWAY STANDARD SHEETS ARE APPLICABLE TO EXPRESSWAY.
3. NON-FREEWAY:
A. MULTILANE DIVIDED HIGHWAY
B. MULTILANE UNDIVIDED HIGHWAY
C. TWO-LANE TWO-WAY ROADWAY

ALL NON-FREEWAYS CAN BE EITHER URBAN OR RURAL:

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA)

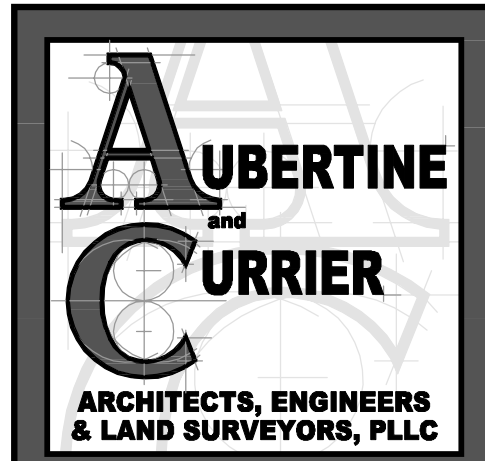
- HIGH DENSITY DEVELOPMENT
•ON-STREET PARKING
•VARIED BUILDING SETBACKS
•MULTI-STORY AND LOW-TO MEDIUM-RISE STRUCTURES FOR RESIDENTIAL, COMMERCIAL, AND EDUCATIONAL USES, STRUCTURES THAT ACCOMMODATE MIXED USES: COMMERCIAL, RESIDENTIAL, AND PARKING
•LIGHT INDUSTRIAL, AND SOMETIMES HEAVY INDUSTRIAL, LAND USE
•PROMINENT DESTINATIONS WITH SPECIALIZED STRUCTURES, E.G., LARGE THEATERS, SPORTS FACILITIES OR CONFERENCE CENTERS
•HIGH LEVELS OF PEDESTRIAN AND BICYCLIST ACTIVITY, WITH NEARLY CONTINUOUS SIDEWALKS AND MARKED CROSSWALKS
•HIGHER DENSITY OF TRANSIT STOPS AND ROUTES
•DRIVEWAY DENSITIES GREATER THAN 25 DRIVEWAYS/MILE ON EACH SIDE OF THE ROAD
•MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS/MILE OR GREATER
•MAJOR COMMERCIAL DRIVEWAYS
•HIGH DENSITY OF CROSS STREETS

RURAL: DOES NOT MEET MORE THAN ONE OF THE ABOVE CRITERIA.

NOTES FOR NIGHTTIME OPERATIONS:

- N1. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
N2. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
N3. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.
N4. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.
N5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
N6. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
N7. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
N8. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
N9. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER.
N10. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.
N11. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.

NEW YORK STATE OF OPPORTUNITY Department of Transportation
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL GENERAL NOTES
APPROVED APRIL 8, 2022 ISSUED UNDER EI 22-008
Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM 619-010

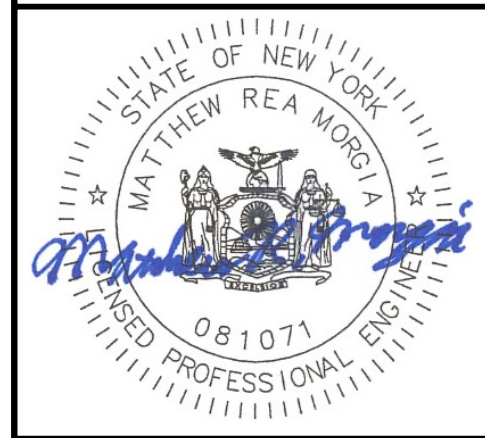


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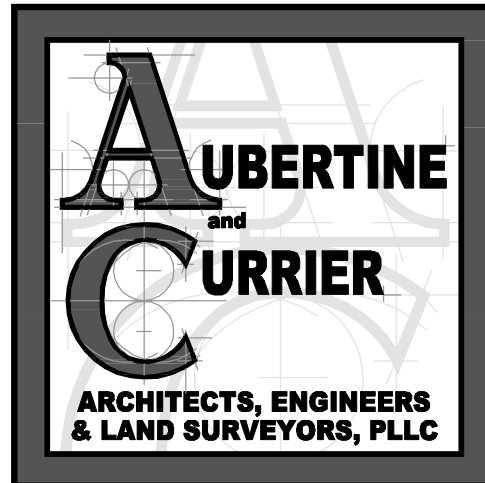


BURLINGTON STREET RECONSTRUCTION PROJECT CITY OF WATERTOWN ENGINEERING DEPARTMENT JEFFERSON COUNTY, STATE OF NEW YORK

Table with project details: PROJECT NO: 2023-052, SCALE: AS NOTED, DRAWN BY: TFT, CHECKED BY: MRM, ISSUE DATES: 02/23/2024, 04/03/2024, 12/13/2024

WORK ZONE TRAFFIC CONTROL SHEETS

CT-500

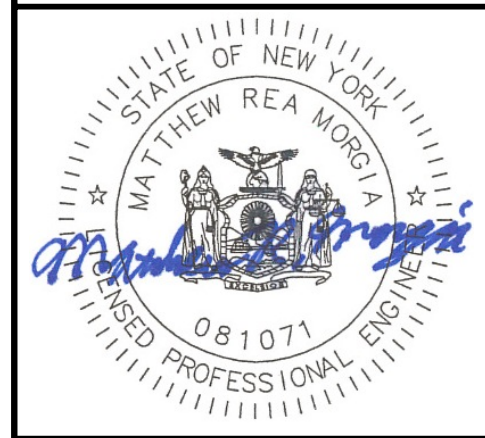


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**BURLINGTON STREET
RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

TABLE 011-01: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	DURATION	MOBILE (WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY, WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES) INCLUDES "STOP AND GO" OPERATIONS OF 5 MINUTES OR LESS				SHORT DURATION (WORK THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR)				SHORT TERM (DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD)				INTERMEDIATE TERM (WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS OR NIGHT TIME WORK LASTING MORE THAN 1 HOUR)				LONG TERM (WORK THAT OCCUPIES A LOCATION FOR MORE THAN 3 CONSECUTIVE DAYS)			
		ROAD TYPE & SPEED	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY			FREEWAY	NON-FREEWAY		
			≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P, TMIA	P	P, TMIA	P, TMIA	P, TMIA	P	P, TMIA	P, TMIA	P, TMIA	P	P, TMIA	P, TMIA	P, TMIA	SEE NOTE 2	P, TMIA	P, TMIA	P, TMIA	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	NA	NA	NA	NA	P, TMIA	P, TMIA	P	P	P, TMIA	P, TMIA	P	SEE NOTE 2	P, TMIA	P, TMIA	SEE NOTE 2	SEE NOTE 2	P, TMIA	P, TMIA	SEE NOTE 2	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P	P	P, TMIA	P, TMIA	P	P	P, TMIA	P, TMIA	P	P	P, TMIA	P, TMIA	SEE NOTE 2	SEE NOTE 2	P, TMIA	P, TMIA	SEE NOTE 2	SEE NOTE 2
	OTHER HAZARDS NO WORKERS EXPOSED	NA	NA	NA	NA	P, TMIA	P, TMIA	P	P	P, TMIA	P, TMIA	P	SEE NOTE 2	P, TMIA	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2	P, TMIA	SEE NOTE 2	SEE NOTE 2	SEE NOTE 2

LEGEND
 P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY.
 TMIA: TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA) REQUIRED
 NA: NOT APPLICABLE

NOTES
 1. THE EXPOSURE CONDITIONS ASSUMES THERE IS NO POSITIVE PROTECTION PRESENT
 2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

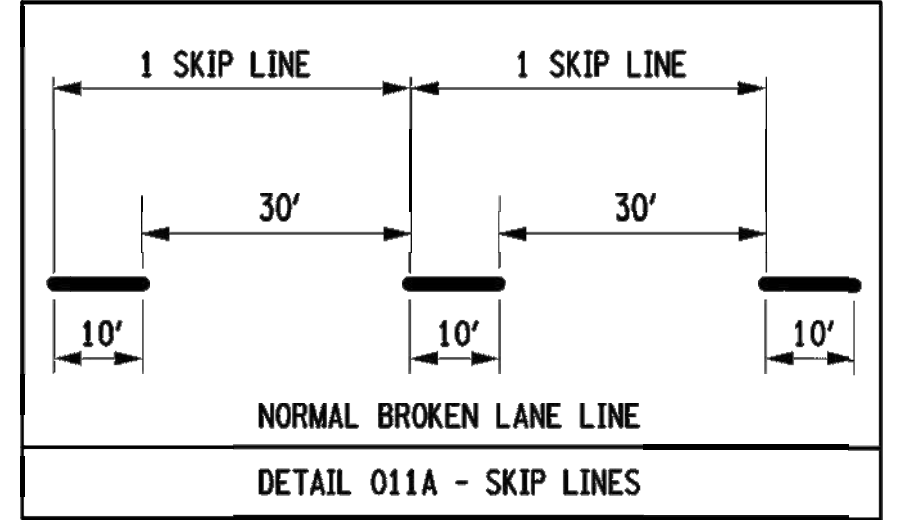


TABLE 011-02: TAPER LENGTHS & NUMBER OF CONES CHART

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	TAPER LENGTH L (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES												SHOULDER TAPER LENGTH: L/3 (FT.) / * OF SKIP LINES / * OF CHANNELIZING DEVICES		
	LATERAL SHIFT OF TRAFFIC FLOW PATH (FT.)												FOR SHOULDER WIDTH		
	4	5	6	7	8	9	10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.			
25	40/1/2	80/2/3	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2			
30	80/2/3	80/2/3	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2			
35	80/2/3	120/3/4	120/3/4	160/4/5	160/4/5	200/5/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3			
40	120/3/4	160/4/5	160/4/5	200/5/6	240/6/7	240/6/7	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3			
45	200/5/6	240/6/7	280/7/8	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4			
50	200/5/6	240/6/7	320/8/9	360/9/10	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5			
55	240/6/7	280/7/8	320/8/9	400/10/11	440/11/12	520/13/14	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5			
60	240/6/7	320/8/9	360/9/10	440/11/12	480/12/13	560/14/15	600/15/16	680/17/18	720/18/19	80/2/3	120/3/4	160/4/5			
65	280/7/8	320/8/9	400/10/11	480/12/13	520/13/14	600/15/16	640/16/17	720/18/19	800/19/20	80/2/3	160/4/5	200/5/6			

* THIS TABLE WAS CREATED WITH REFERENCE TO MUTCD TABLE 6H-4.
 ** THE NUMBER OF CHANNELIZING DEVICES SHOWN IS CALCULATED BASED ON A 40FT DEVICE SPACING. THE NUMBER OF CHANNELIZING DEVICES CAN BE ADJUSTED AS NECESSARY.

WORK ZONE TRAFFIC CONTROL LEGEND

SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL TRAILER OR SUPPORT
	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CONE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	AUTOMATED FLAGGER ASSISTANCE DEVICE WITH OPERATOR
	FLAGGER
	FLAG TREE
	LUMINAIRE
	MOWER
	PARKWAY GRASS SHOULDER
	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
	PORTABLE VARIABLE MESSAGE SIGN
	ADVANCE WARNING SIGN WITH ORANGE FLAGS
	TRAILER FOR ARROW PANEL OR PORTABLE VARIABLE MESSAGE SIGN (PVMS)

WORK ZONE TRAFFIC CONTROL LEGEND

SYMBOL	DESCRIPTION
	SIGN, TEMPORARY
	SPOTTER
	TEMPORARY POSITIVE BARRIER
	TEMPORARY POSITIVE BARRIER WITH WARNING LIGHTS
	TEMPORARY TRAFFIC SIGNAL HEAD
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK AREA
	WORK VEHICLE
	WORK VEHICLE (MULCHING/HERBICIDE OPERATION)
	WORK VEHICLE (PAVEMENT MARKING)
	WORK VEHICLE (SIGNAL WORK)
	24,000LB PROTECTIVE VEHICLE WITH TRUCK/TRAILER MOUNTED ATTENUATOR (TMIA)
	24,000LB PROTECTIVE VEHICLE WITHOUT TRUCK/TRAILER MOUNTED ATTENUATOR

TABLE 011-03: LONGITUDINAL BUFFER SPACE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE (FT.) / * OF SKIP LINES
25	155 / 4
30	200 / 5
35	250 / 6
40	305 / 8
45	360 / 9
50	425 / 11
55	495 / 13
65	645 / 16

* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-2.

TABLE 011-04: ROLL AHEAD DISTANCE FOR PROTECTIVE VEHICLES

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	ROLL AHEAD DISTANCE (FT.) / * OF SKIP LINES FOR VEHICLES			
	MOVING OPERATION (BASED ON PROTECTIVE VEHICLE SPEED OF 15 MPH)		STATIONARY OPERATION	
	MIN	MAX	MIN	MAX
≥ 55	200/5	280/7	120/3	200/5
45 - 50	160/4	240/6	80/2	160/4
≤ 40	120/3	200/5	40/1	120/3

* THIS TABLE WAS CREATED WITH REFERENCE TO MUTCD TABLES NY-2 AND NY2-B.

TABLE 011-06: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.
FREEWAY	1000	1500	2640	1 MILE	1/2 MILE

* PRECONSTRUCTION POSTED SPEED LIMIT.
 ** THIS TABLE IS THE SAME AS MUTCD TABLE NY6H-3.

TABLE 011-05: FLARE RATES FOR POSITIVE BARRIER

TYPE OF POSITIVE BARRIER	POSTED SPEED LIMIT				
	30 MPH	40 MPH	50 MPH	55 MPH	65 MPH
TEMPORARY POSITIVE BARRIER	8:1	11:1	14:1	16:1	20:1
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	12:1	15:1

TABLE 011-07: TAPER LENGTH FOR TEMPORARY TRAFFIC CONTROL ZONES

TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	50 FT. MIN -100 FT. MAX
DOWNSIDE TAPER	50 FT. MIN -100 FT. MAX

* THIS TABLE IS THE SAME AS MUTCD TABLE 6C-3.



U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL GENERAL TABLES AND LEGEND

APPROVED DECEMBER 2, 2021 ISSUED UNDER EI 21-028
 Robert Limoges
 ROBERT LIMOGES, P.E.
 DIRECTOR, OTSM
 619-011

PROJECT NO:	2023-052
SCALE:	AS NOTED
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/03/2024 12/13/2024

WORK ZONE TRAFFIC CONTROL SHEETS

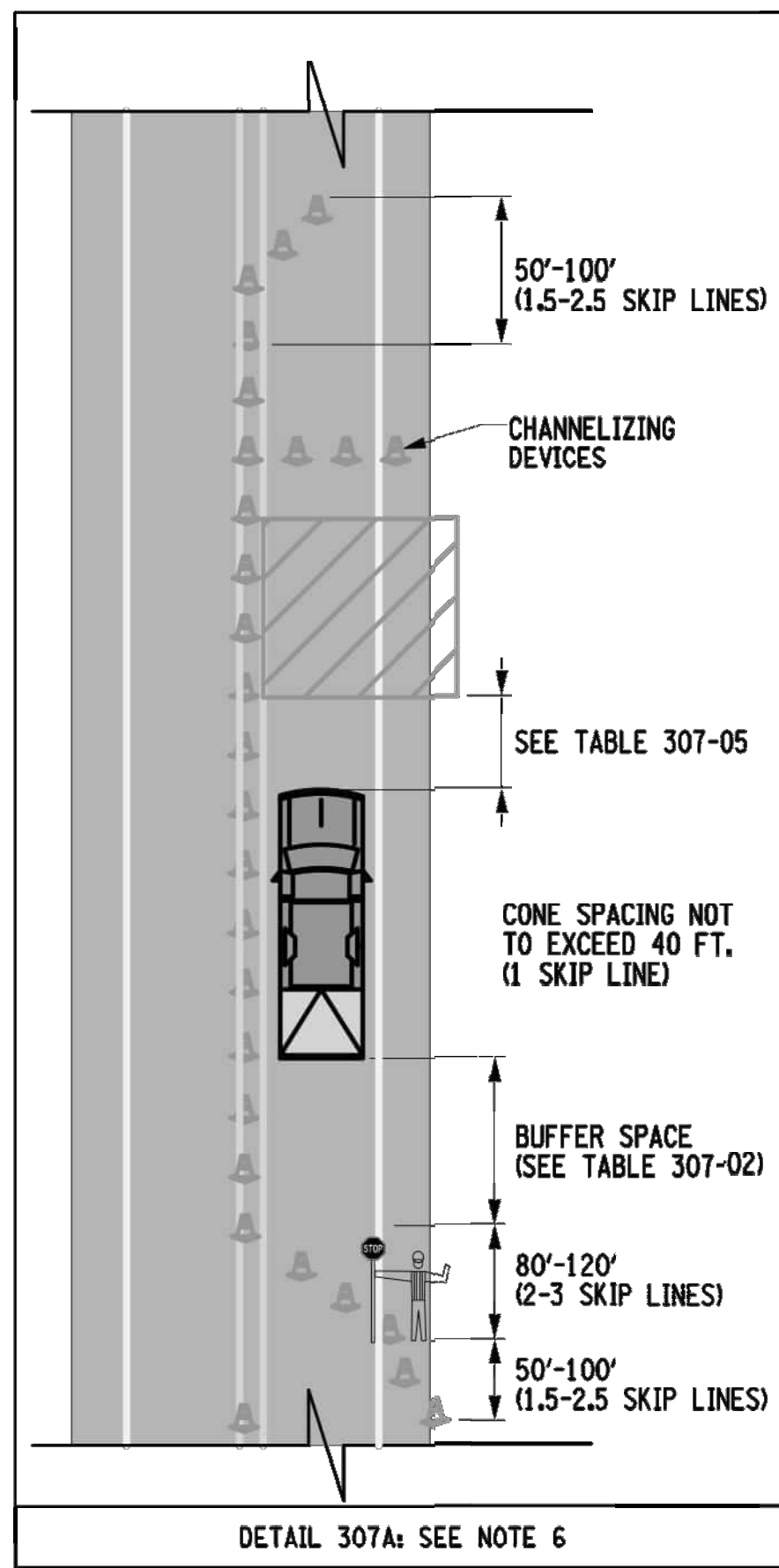
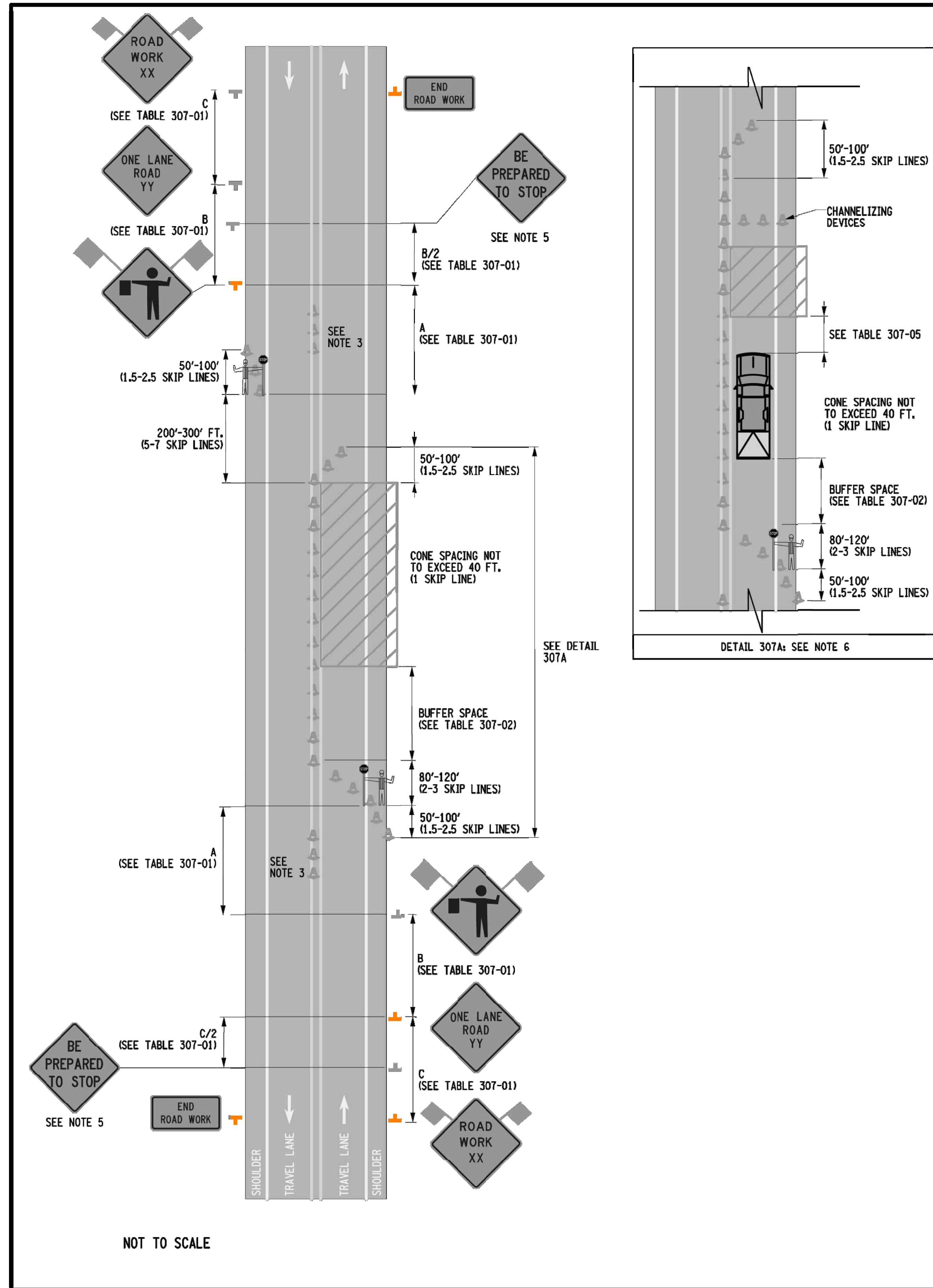
CT-501

USC SEPTEMBER 01, 2022

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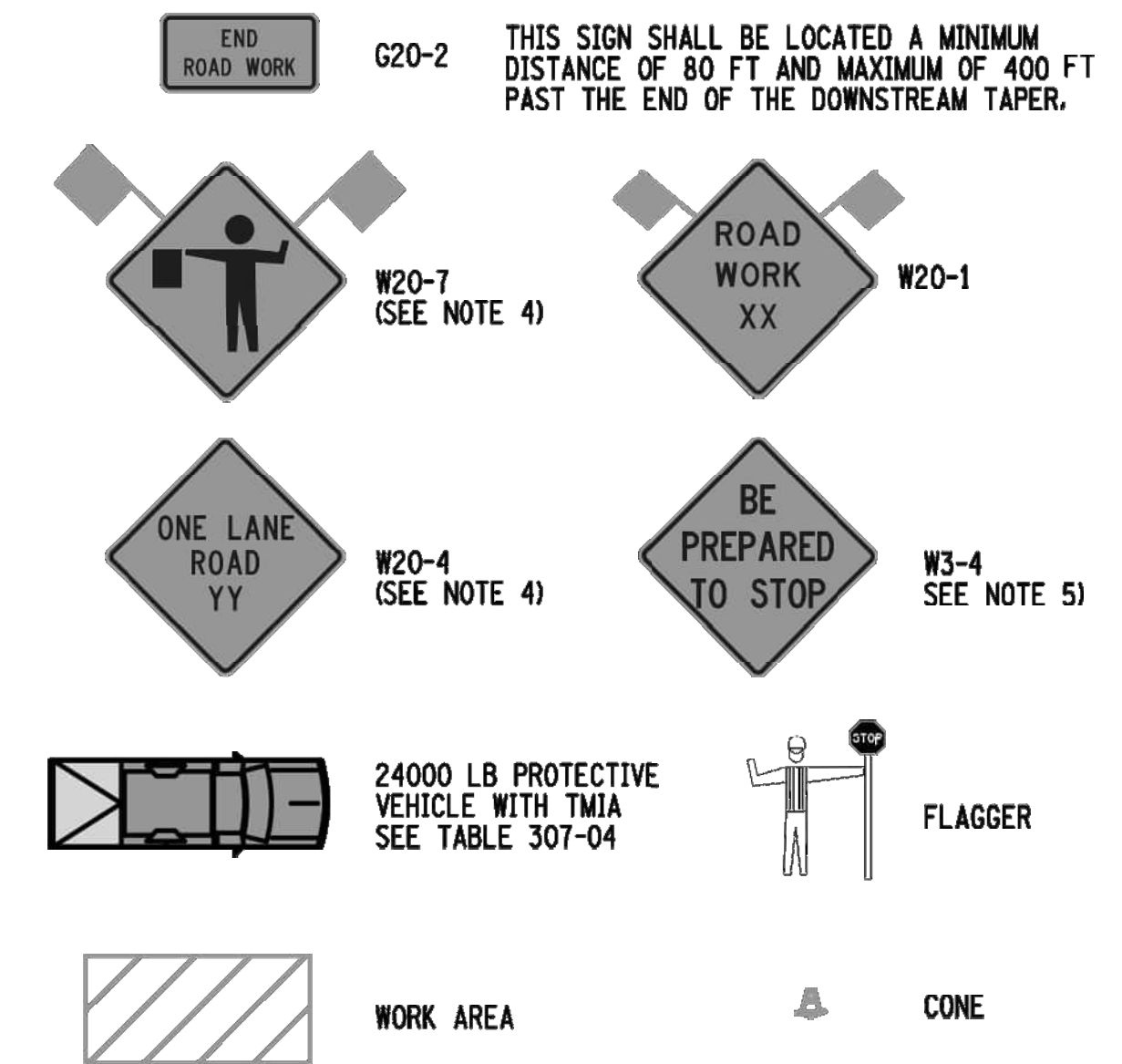
USC SEPTEMBER 01, 2022

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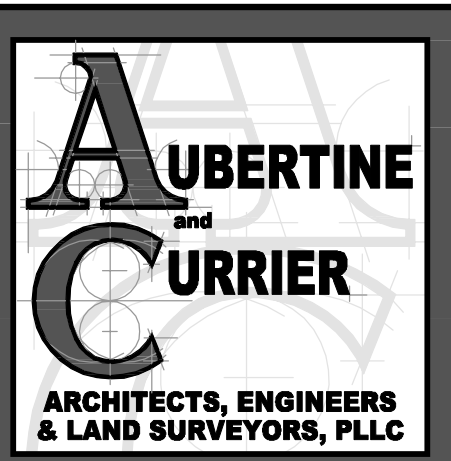
NOTES:

1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAY PERIOD.
2. IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.
3. CENTERLINE CONES MAY BE ADDED TO ENHANCE THE VISIBILITY OF THE FLAGGER STATION. IF CONES ARE USED, PLACE THEM 100 FT. (MINIMUM) FROM FLAGGER.
4. FLAGGER SYMBOL SIGN (W20-7) AND "ONE LANE ROAD AHEAD" SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
5. IF THE TRAFFIC IS EXPECTED TO QUEUE PAST THE W20-4 SIGN, A W3-4 SIGN SHOULD BE ADDED.
6. IF CONDITIONS WARRANT, PROTECTIVE VEHICLE WITH APPROPRIATE ROLL AHEAD DISTANCE MAY BE USED IN ADVANCE OF THE WORK AREA. TO USE PROTECTIVE VEHICLE, BUFFER SPACE SHALL BE PROVIDED ACCORDINGLY.
7. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
8. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
9. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
10. ALL FLAGGERS SHALL USE 24" MIN. OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6' STAFF. THE PADDLE IS THE PREFERRED DEVICE, BUT THE FLAG MAY BE USED AT INTERSECTIONS WHERE THE STOP/SLOW PADDLE WOULD OFFER CONTRADICTING INFORMATION TO DRIVERS TRAVELING IN OPPOSITE DIRECTIONS/LEGS OF THE INTERSECTION OR DURING INCIDENT MANAGEMENT SITUATIONS.



REFER TO SHEET 2 OF 2 FOR ALL TABLES

		Department of Transportation
U.S. CUSTOMARY STANDARD SHEET		
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY LANE CLOSURE WITH FLAGGERS SHORT TERM OPERATION (SHEET 1 OF 2)		
APPROVED DECEMBER 2, 2021 Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM	ISSUED UNDER EI 21-028 619-307	

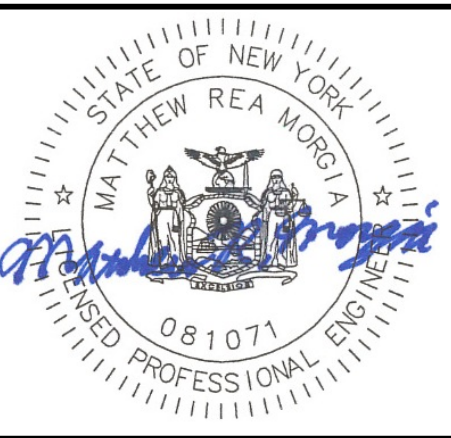


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**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO:	2023-052
SCALE:	AS NOTED
DRAWN BY:	TFT
CHECKED BY:	MRM
ISSUE DATES:	02/23/2024 04/30/2024 12/13/2024

WORK ZONE TRAFFIC CONTROL SHEETS

CT-502

USC SEPTEMBER 01, 2022

FILE NAME = 619-307-2.dgn
 DATE/TIME = 06-DEC-2021 16:01
 USER = oboastwright

TABLE 307-01: ADVANCE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT.)	B (FT.)	C (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	350	1000 FT.	AHEAD
RURAL	500	500	500	1500 FT.	1000 FT.

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 307-02: LONGITUDINAL BUFFER SPACE

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.)/* OF SKIP LINES
25	155/4
30	200/5
35	250/6
40	305/8
45	360/9
50	425/11
55	495/13

TABLE 307-03: REQUIRED SIGN SIZES*

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
W3-4	36x36	48x48
W20-1	36x36	48x48
W20-4	36x36	48x48
W20-7	36x36	48x48
WARNING FLAG	18x18	18x18

*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

TABLE 307-04: PROTECTIVE VEHICLE REQUIREMENTS

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2

LEGEND

P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER, IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY


TMIA: TMIA REQUIRED

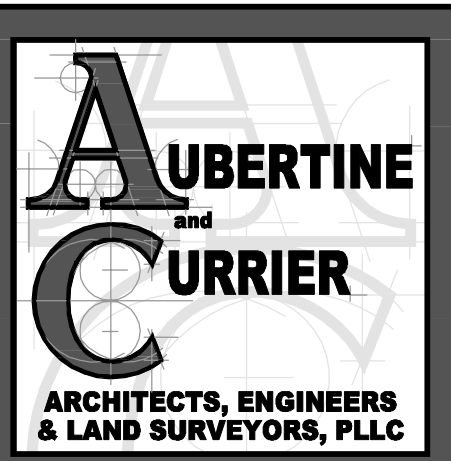
NOTES:

- THE EXPOSURE CONDITIONS ASSUMES THERE IS NO POSITIVE PROTECTION PRESENT
- EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

TABLE 307-05: ROLL AHEAD DISTANCE

ROLL AHEAD DISTANCE (FT.)/* OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

	NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
U.S. CUSTOMARY STANDARD SHEET		
WORK ZONE TRAFFIC CONTROL TWO-LANE TWO-WAY ROADWAY LANE CLOSURE WITH FLAGGERS SHORT TERM OPERATION (SHEET 2 OF 2)		
APPROVED DECEMBER 2, 2021	ISSUED UNDER EI 21-028	
<i>Robert Limoges</i> ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-307	



NYS WBE/DBE Certified
 SBA Woman Owned
 Small Business (WOSB)

aubertinecurrier.com

522 Bradley Street
 Watertown, New York 13601
 Phone: (315)782-2005

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**BURLINGTON STREET
 RECONSTRUCTION PROJECT**
 CITY OF WATERTOWN
 ENGINEERING DEPARTMENT
 JEFFERSON COUNTY, STATE OF NEW YORK

PROJECT NO: 2023-052
 SCALE: AS NOTED
 DRAWN BY: TFT
 CHECKED BY: MRM
 ISSUE DATES:
 02/23/2024
 04/30/2024
 12/13/2024

WORK ZONE TRAFFIC CONTROL SHEETS

CT-503